

Indiana Department of Transportation

County Jackson

Route: East CR 300 South

Des. No. 1703020

**FHWA-Indiana Environmental Document
CATEGORICAL EXCLUSION / ENVIRONMENTAL ASSESSMENT FORM
GENERAL PROJECT INFORMATION**

| | |
|-------------------------------------|---|
| Road No./County: | Jackson County, East CR 300 South |
| Designation Number: | 1703020 |
| Project Description/Termini: | Bridge Improvement Project Jackson County Bridge No. 154 (NBI #3600099) The project is located approximately 0.82 mile east of CR 840 E. Project improvements will extend 150 ft. west and 150 ft. east along East CR 300 South for a total project length of 0.081 mile. |

After completing this form, I conclude that this project qualifies for the following type of Categorical Exclusion (FHWA must review/approve if Level 4 CE):

| | |
|----------|---|
| | Categorical Exclusion, Level 2 – The proposed action meets the criteria for Categorical Exclusion Manual Level 2 - table 1, CE Level Thresholds. Required Signatories: ESM (Environmental Scoping Manager) |
| | Categorical Exclusion, Level 3 – The proposed action meets the criteria for Categorical Exclusion Manual Level 3 - table 1, CE Level Thresholds. Required Signatories: ESM, ES (Environmental Services Division) |
| X | Categorical Exclusion, Level 4 – The proposed action meets the criteria for Categorical Exclusion Manual Level 4 - table 1, CE Level Thresholds. Required Signatories: ESM, ES, FHWA |
| | Environmental Assessment (EA) – EAs require a separate FONSI. Additional research and documentation is necessary to determine the effects on the environment. Required Signatories: ES, FHWA |

Note: For documents prepared by or for Environmental Services Division, it is not necessary for the ESM of the district in which the project is located to release for public involvement or sign for approval.

Approval

| | | | |
|----------------|-------|--------------|-------|
| _____ | _____ | _____ | _____ |
| ESM Signature | Date | ES Signature | Date |
| _____ | _____ | | |
| FHWA Signature | Date | | |

Release for Public Involvement

| | | | |
|--------------|-------|-----------------------|------------------|
| <u>N/A</u> | _____ | <u>BDM</u> <u>REB</u> | <u>11/3/2021</u> |
| ESM Initials | Date | ES Initials | Date |

Certification of Public Involvement _____
INDOT District ESM Date

Note: Do not approve until after Section 106 public involvement and all other environmental requirements have been satisfied.

INDOT ES/District Env. Reviewer Signature: _____ Date: _____

Name and Organization of CE/EA Preparer: Elayna Stoner, Metric Environmental

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Part I - PUBLIC INVOLVEMENT

Every Federal action requires some level of public involvement, providing for early and continuous opportunities throughout the project development process. **The level of public involvement should be commensurate with the proposed action.**

| | | |
|---|-------------------------------------|--------------------------|
| | Yes | No |
| Does the project have a historic bridge processed under the Historic Bridges PA*? If No, then: | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| Opportunity for a Public Hearing Required? | <input type="checkbox"/> | <input type="checkbox"/> |

**A public hearing is required for all historic bridges processed under the Historic Bridges Programmatic Agreement between INDOT, FHWA, SHPO, and the ACHP.*

Discuss what public involvement activities (legal notices, letters to affected property owners and residents (i.e. notice of entry), meetings, special purpose meetings, newspaper articles, etc.) have occurred for this project.

Remarks: Notice of Entry letters were mailed to potentially affected property owners near the project area on April 13, 2020, notifying them about the project and that individuals responsible for land surveying and field activities might be seen in the area. A sample copy of the Notice of Entry letter and a list of property owners is provided in Appendix G, pages G-1 to G-2.

On November 15, 2019 a legal notice to interested parties for proposals for the rehabilitation and reuse, or the storage and future reuse of the bridge was published in the *Seymour Tribune* newspaper and the advertisement was also included on the Indiana Department of Transportation (INDOT) Historic Bridges Marketing Program website. Signs were posted at the bridge site on November 19, 2019. An advertisement was also placed in the *Indianapolis Star* on May 1, 2020. To date no interested parties have come forward to take ownership of Jackson County Bridge 154. The marketing period will end when the public hearing comment period ends. The text of the legal notices and the affidavits of publication are provided in Appendix D, pages D-44 to D-49.

To meet the public involvement requirements of Section 106, a legal notice of Federal Highway Administration-Indiana Division's (FHWA's) finding of "No Historic Properties Affected" was published in *The Tribune* on May 30, 2020 offering the public an opportunity to submit comment pursuant to 36 CFR 800.2(d), 800.3(e), and 800.6(a)(4). The public comment period closed 30 days later on June 29, 2020. No comments or responses were received. The text of the legal notice and the affidavit of publication are provided in Appendix D, pages D-52 to D-56.

Pursuant to the Historic Bridge Programmatic Agreement (Historic Bridges PA), a public hearing is required. A legal notice will appear in a local publication contingent upon the release of this document for public involvement. This document will be revised after the public involvement requirements are fulfilled.

Public Controversy on Environmental Grounds

| | | |
|--|--------------------------|-------------------------------------|
| Will the project involve substantial controversy concerning community and/or natural resource impacts? | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|--|--------------------------|-------------------------------------|

Remarks: At this time, there is no substantial public controversy concerning impacts to the community or to natural resources.

Part II - General Project Identification, Description, and Design Information

Sponsor of the Project: Jackson County INDOT District: Seymour
 Local Name of the Facility: East CR 300 South

Funding Source (mark all that apply): Federal State Local Other*

*If other is selected, please identify the funding source: _____

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PURPOSE AND NEED:

Describe the transportation problem that the project will address. The solution to the traffic problem should NOT be discussed in this section. (Refer to the CE Manual, Section IV.B.2. Purpose and Need)

The purpose of this project is to restore service to E CR 300S over Rider Ditch by providing a structure with a capacity of 15 tons minimum.

The need for this project is based on the poor physical condition of the existing bridge. The bridge has been closed since March 19, 2019. The project has a specific structural need due to deficiencies observed at the lower panel points on the truss that drastically reduce the load carrying capacity of the structure. The plates connecting the intermediate vertical posts and floor beams have severe corrosion and advanced section loss (up to 85%). At one location the plate is severed through. The bridge was closed immediately upon observing the failure during the most recent bridge inspection. Prior to the bridge closing in March 2019, the bridge was posted at a 5 ton load limit. The minimum required load carrying capacity for the bridge is 15 tons per the Indiana Design Manual (IDM).

PROJECT DESCRIPTION (PREFERRED ALTERNATIVE):

County: Jackson County Municipality: Washington Township

Limits of Proposed Work: The project limits will extend approximately 150 ft. west and 150 ft. east along East CR 300 South for a total project length of 401 ft. (0.081 mile)

Total Work Length: 0.081 Mile Total Work Area: 0.5 acre

Is an Interchange Modification Study / Interchange Justification Study (IMS/IJS) required? **Yes¹** **No**
 If yes, when did the FHWA grant a conditional approval for this project? Date: _____

¹If an IMS or IJS is required; a copy of the approved CE/EA document must be submitted to the FHWA with a request for final approval of the IMS/IJS.

Jackson County with oversight from INDOT and partial funding from the Federal Highway Administration (FHWA) for a portion of the cost of the preferred alternative, which involves replacing the existing bridge that carries East CR 300 South over Rider Ditch in Jackson County, Indiana.

Project Location

The project is located on East CR 300 South in southeast Jackson County. Specifically, the project is located approximately 0.82 mile east of CR 840 E in Sections 28 and 33, Township 5 North, Range 6 East, in Washington Township, as illustrated on the *Crothersville, Indiana 7.5-minute United States Geological Survey (USGS) topographic quadrangle (Appendix B, page B-2).*

Existing Roadway

East CR 300 South is a two-lane, east/west, gravel roadway, classified as a Low-Volume local rural road. Low Volume Roads are generally classified as rural roadways that have less than 400 vehicles per day. The existing cross-section provides one 9 ft. travel lane in each direction bordered by 2-3 ft. shoulders. The approach roadway width at the bridge is 15 ft. and narrows to 11 ft. to the east and west of the crossing. There are no guardrails, curbs or sidewalks. There is no posted speed on East CR 300 South. The design speed for the project is 35 miles per hour (mph).

Land use in the vicinity of the project is a combination of agriculture and wooded riparian land adjacent to the surrounding waterways. Along Rider Ditch there is a narrow riparian corridor consisting of various native trees and underbrush. East CR 300 South is currently closed at the bridge crossing.

Existing Bridge

Jackson County Bridge 154 (NBI #3600099) is a single-span, Steel Pratt Pony Truss that is 90 ft. in length with a wooden deck. The bridge was built originally built in 1910 and rehabilitated in 1992 and 2008. The out-to-out deck width is 16 ft. and the clear roadway width is 15 ft.-4 inches. The bridge is a single-lane structure; however, the one-lane bridge signs are missing at either end of the crossing. The approach roadway width is 15 ft. The substructure consists of steel caissons filled with concrete and a concrete retaining wall. The substructure is in fair condition with minor section loss. There are no guardrails on the bridge or along the roadway approaches. The bridge is eligible for the National Register of Historic Places (NRHP) and is designated as a Non-Select Bridge in the *Indiana Historic Bridge Inventory List (Mead and Hunt 2010).*

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The bridge was determined eligible under Criterion C, as an example of a bridge built during the initial period of development or application of standards for its type in Indiana; thus, represents an important phase in construction. Further, the bridge also displays exceptional main span length for its type, representing an innovative design and/or construction method.

Alternatives Analysis Process

Per the terms of the “Programmatic Agreement Regarding Management and Preservation of Indiana’s Historic Bridges” (Historic Bridges Programmatic Agreement (HBPA)), the FHWA will satisfy its Section 106 responsibilities involving “Select” and “Non-Select” bridges through the Project Development Process (PDP) of the Historic Bridges PA (Stipulation III).

Jackson County Bridge 154, a historic property, has been classified as a Non-Select Bridge by the INDOT Historic Bridge Inventory, and thus, the procedures outlined in Stipulation III.B of the Historic Bridges PA has been followed to determine the preferred alternative that meets the purpose and need of the project. The various alternatives shall be evaluated based on whether the alternative is feasible and prudent. Prudence of projects involving Non-Select bridges on low-volume roads should be assessed based on cost-effectiveness and other criteria as noted in the Indiana Design Manual (IDM 412-5.04(02)). If the bridge rehabilitation cost is > 40% of the replacement cost, then replacement is warranted.

An initial Historic Bridge Alternatives Analysis (HBAA, 5/28/2020) was developed and approved. After approval of the initial HBAA, modifications were made to the proposed project design including the widening the proposed bridge structure to provide a 24 ft. clear roadway width, providing a larger 50 ft. middle span length to reduce debris collection, adjusting the horizontal alignment of the bridge to correct the angle point located to the east of the bridge and additional channel protection along the Rider Ditch embankments. As a result of these changes, an HBAA Addendum (6/24/2021) was prepared to re-evaluate the alternatives and include scour protection and roadway approach work that are typically included as part of bridge rehabilitation analysis. The HBAA Addendum is provided in Appendix D, pages D-57 to D-177.

Preferred Alternative

Alternative E: Relocation of Existing Bridge and New Bridge on Current Alignment

The preferred alternative will provide a new bridge structure across Rider Ditch on the existing roadway alignment. The new, three-span continuous reinforced concrete slab bridge will be approximately 111 ft.-6 inches in length with an out-to-out deck width of 27 ft. and a clear roadway width of 24 ft. New, 20 ft.-6 inch reinforced, concrete bridge approaches that are 24 ft. in width will be installed at either end of the new bridge. The deck will be constructed with a 2 percent cross slope. The bridge railing will consist of standard, 2 ft.-9 inch concrete bridge rails and new, steel crash rated approach guardrail will be installed along both sides of the east and west approaches. riprap will be added north and south of the structure where bank erosion is present and significant. Along the west embankment, the channel will be cleared to allow a greater waterway opening through the structure during normal flow conditions. Approximately 100 linear feet of permanent impacts to Rider Ditch will result from the construction of the new bridge piers and placement of riprap at the toe of slope for erosion control.

Alternate E is prudent and feasible and provides an opportunity to preserve the bridge. If no organization or private parties come forward to fund the relocation and rehabilitation, Alternative F will become the preferred feasible and prudent alternative. If an organization or private party comes forward to fund the relocation and rehabilitation, this document will be updated to cover the impacts to the site where the bridge will be moved.

The limits of the preferred alternate will extend approximately 150 ft. west and 150 ft. east along East CR 300 South for a total project length of 401 ft. (0.081 mile) including the length of the new bridge. The preferred alternative will meet the purpose and need of the project by restoring service to E CR 300S over Rider Ditch by providing a structure with a capacity of 15 tons minimum. The project termini are logical because they encompass only the area necessary to install the new bridge and tie the improvements into the existing roadway for a smooth transition. Design plans are provided in Appendix B, pages B-7 to B-13.

East CR 300 South, east and west of the bridge has been closed since March 2019 and the community has been using local detours since that time. The official detour during construction will utilize CR 825 East and SR 250. Additional details are discussed in the *Maintenance of Traffic* Section of this CE document. The project letting is scheduled for February 2023 and construction is anticipated to extend into November 2023. The estimated cost of the preferred alternative is approximately \$1,025,000.00 and takes into account the cost of dismantling and relocating the bridge by a private party. If Alternative F (Bridge Replacement) is chosen as the preferred alternative, the cost would be approximately \$1,090,000.00. which includes the cost to construct the new bridge and demolish the existing bridge.

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OTHER ALTERNATIVES CONSIDERED:

Describe all discarded alternatives, including the Do-Nothing Alternative and an explanation of why each discarded alternative was not selected.

Additional details regarding each alternative can be found in the HBAA Addendum located in Appendix D, pages D-57 to D-177.

Alternative A: Do Nothing Alternative

This alternative means that no federal funds would be expended, and no action would be taken to correct the current deficiencies. The road would remain closed, and the bridge would continue to deteriorate. This alternative would not meet the purpose and need for the project. For this reason, this alternative was discarded from further consideration.

Alternative B-1: Rehabilitation in Accordance with the Secretary of the Interior's Standards

Alternative B-1 involves rehabilitating the existing bridge to a standard that meets the Secretary of Interior's Standards (SOIS) for Rehabilitation. This alternative would maintain the structure's historic integrity and restore the structure's original design capacity. To achieve this objective, all replacement procedures would maintain and/or restore the historic elements of the structure as closely as possible. The expected service life after rehabilitation would be 25 years but, the bridge would still have a deficient load rating. This alternative does not provide the minimum structural capacity of 15 tons as required by the purpose and need. For this reason, this alternative was discarded from further consideration.

Alternative B-2: Rehabilitation Not in Accordance with the Secretary of Interior's Standards

This alternative would rehabilitate the existing bridge in a manner not compliant with the SOIS for Rehabilitation. The bridge's historic features would not be considered in the repairs. The primary goal of this alternative would be to correct the existing structural deficiencies and strengthen the bridge to handle a minimum H15 loading rating. The expected service life after rehabilitation would be 25 years but the rehabilitation would significantly modify the original structure by replacing the tension eye-bars, diagonals, and stringers with new steel members of differing size, shape, and type in order to provide the required capacity. Guardrail and end treatments would be installed to prevent vehicular strikes to the pony trusses and errant vehicle entry into Rider Ditch. The estimated cost for this alternate was determined to be 42% higher than the replacement cost of Alternative F. The bridge would continue to have insufficient width and load capacity and would not meet the purpose and need. For these reasons, this alternative was discarded from further consideration.

Alternative C-1: Rehabilitation in Accordance with the Secretary of Interior's Standards (One-way Pair)

This alternative would rehabilitate the existing bridge in the same fashion as Alternative B-1 except that a new bridge would be built adjacent to the existing for use as a one-way pair. The new bridge would be a continuous three-span, reinforced concrete slab bridge. This alternative would also include building an additional approach roadway. In addition to the rehabilitation costs described in Alternative B-1, this option includes additional costs associated with a new bridge, increased right-of-way purchasing, and road approach modification. This alternate would be constructed on an existing one-lane, two-way road which is not intended to be upgraded from this configuration. As noted previously in the discussion of Alternative B-1, for Alternative C-1 the structure would be rehabilitated to the SOIS and, the expected service life after rehabilitation would be 25 years. As identical to Alternative B-1, the rehabilitated truss would have substandard load capacity. Although Alternative C-1 is feasible, the estimated cost of this alternate is 26% higher than the replacement cost of Alternative F. This alternate would not improve the load carrying capacity of the existing bridge to the minimum of 15 tons. For these reasons, this alternative was discarded from further consideration.

Alternative C-2: Rehabilitation Not in Accordance with the Secretary of Interior's Standards (One-way Pair)

This alternative would rehabilitate the existing bridge in the same fashion as Alternative B-2 except that a new bridge would be built adjacent to the existing for use as a one-way pair. The new bridge would be a continuous three-span, reinforced concrete slab bridge. This alternative would also include building an additional approach roadway. For additional details related to rehabilitation of the existing structure see the description in Alternative B-2. As with Alternative B-2 the expected service life extension would be 25 years. In addition to the rehabilitation costs as described in Alternative B-2, this alternative includes additional costs associated with a new bridge, increased right-of-way purchasing, and road approach modification. This alternate would be constructed on an existing one-lane two-way road which is not intended to be upgraded from this configuration. Identical to Alternative B-2, the existing truss would be significantly modified to provide enough load capacity. Alternative C-2 would not rehabilitate the structure to the SOIS and would include the introduction of replacement elements. Although Alternative C-2 is feasible, the estimated cost for this alternative is 36% higher than the replacement cost of Alternative F. For these reasons, this alternative was discarded from further consideration.

Alternative D: Rehabilitation of the Existing Structure with a Two-Way Bypass

Alternative D would rehabilitate the existing bridge for non-vehicular use and build a bypass bridge adjacent to it. This alternative is effectively identical to Alternate C-1. This Alternative would rehabilitate the truss to the SOIS.

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The main difference is that following rehabilitation, the existing bridge will be closed to vehicular traffic and the adjacent approach will be graded for parking. Due to the severely deteriorated condition of the existing structure, it would need to be rehabilitated in an equivalent manner to the B-1 Alternative. Once rehabilitated and permanently closed to vehicle traffic, inspection requirements would no longer be applicable. This alternative would require a private party to assume ownership of the bridge and maintain the bridge for perpetuity. To date no interested parties have come forward to take ownership of Jackson County Bridge 154 as required for this alternate. Alternative D is feasible; however, the estimated cost for this alternative is 26% higher than the replacement cost of Alternative F. For these reasons, this alternative was discarded from further consideration.

Alternative E: Relocation of Existing Bridge and New Bridge on Current Alignment

Alternative E would relocate the historic bridge and build a new bridge on the current alignment. The existing bridge would not be destroyed; however, a responsible party must come forward and fund the relocation and rehabilitation of the bridge. The existing truss bridge would be replaced by a three-span, haunched concrete slab bridge on the existing alignment and would meet all applicable design criteria. The cost of this alternative is \$1,025,000.00 which is 94% of the demolition and replacement cost of Alternative F. The bridge has been advertised for the minimum six month marketing period but to date, no interested party has come forward. The opportunity to relocate and reuse the bridge will remain viable until the public hearing requirements for this project have concluded. As a result, this alternate is still feasible. Given that this alternate is cost effective and meets the purpose and need, this alternative is also prudent if a responsible party comes forward to take ownership of the bridge.

Alternative F: Bridge Replacement

Alternative F would demolish the existing truss bridge and a new, three-span bridge would be built on the same alignment. There would be no bridge relocation process included as part of this alternative. The existing truss bridge would be replaced by a three-span, haunched concrete slab bridge on essentially the same alignment and would meet all applicable design criteria. If Alternative F is chosen as the preferred alternative, the cost would be approximately \$1,090,000.00. which includes the cost to construct the new bridge and demolish the existing bridge. Impacts to the bridge would be mitigated through the stipulations outlined within the Historic Bridges Programmatic Agreement (HBPA) process. This alternative is both feasible and prudent and meets the purpose and need of the project by providing a cost effective and structurally sufficient bridge at the project site.

Alternate E is prudent and feasible and provides an opportunity to preserve the bridge. If no organization or private parties come forward to fund the relocation, Alternative F will become the preferred feasible and prudent alternative. The State Historic Preservation Office (SHPO) requested that photo documentation of the bridge be conducted consistent with the *Historic Bridges PA: Attachment B- Standard Treatment Approach for Historic Bridges*. This will apply regardless of whether Alternative E or F is the chosen as the preferred alternative.

The Do Nothing Alternative is not feasible, prudent or practicable because (Mark all that apply):

- It would not correct existing capacity deficiencies;
- It would not correct existing safety hazards;
- It would not correct the existing roadway geometric deficiencies;
- It would not correct existing deteriorated conditions and maintenance problems; or
- It would result in serious impacts to the motoring public and general welfare of the economy.
- Other (Describe)

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| |
| |
| X |
| |

ROADWAY CHARACTER:

East CR 300 South

| | | | |
|----------------------------|-----------------------------|----------------------|---------------------|
| Functional Classification: | Low-Volume Local Rural Road | | |
| Current ADT: | 30 | VPD (2013) | Design Year ADT: 40 |
| Design Hour Volume (DHV): | N/A* | Truck Percentage (%) | 5 |
| Designed Speed (mph): | 35 | Legal Speed (mph): | 35 (Not Posted) |

| | Existing | Proposed | |
|------------------|----------------------------------|-------------------------------------|--|
| Number of Lanes: | 2 | 2 | |
| Type of Lanes: | 15 ft bi-directional travel lane | 15-24 ft bi-directional travel lane | |
| Pavement Width: | 11-15 ft. | 15-24 ft. | |
| Shoulder Width: | N/A ft. | N/A ft. | |
| Median Width: | N/A ft. | N/A ft. | |
| Sidewalk Width: | N/A ft. | N/A ft. | |

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If the proposed action has multiple roadways, this section should be filled out for each roadway.

***Since East CR 300 South is a Low Volume Road, a traffic study was not conducted to determine the Design year DHV. This is the best available traffic data.**

Setting: Urban Suburban Rural
 Topography: Level Rolling Hilly

DESIGN CRITERIA FOR BRIDGES:

Structure/NBI Number(s): Jackson County Bridge No. 154 (NBI # 3600099) Sufficiency Rating: 22 out of 100
 Source: March 13, 2019 Bridge Inspection Report

| | Existing | Proposed |
|---------------------------|------------------------|--------------------------------|
| Bridge Type: | Steel Pratt Pony Truss | Three-span continuous concrete |
| Number of Spans: | 1 | 3 |
| Weight Restrictions: | 0 ton | 36 Ton/minimum |
| Height Restrictions: | N/A ft. | N/A ft. |
| Curb to Curb Width: | 15-4 ft./in. | 24 ft. |
| Outside to Outside Width: | 16 ft. | 27 ft. |
| Shoulder Width: | N/A ft. | N/A ft. |
| Length of Channel Work: | | 103 ft. |

Describe bridges and structures; provide specific location information for small structures.

Remarks: The preferred alternative will provide a new bridge structure across Rider Ditch on essentially the existing roadway alignment. The existing pony truss bridge will be replaced with a three-span, continuous haunched, concrete slab bridge with a 50 ft. middle span and longer end spans to match, wider approach work, wall piers in the channel instead of pile bents, and a slight bridge realignment to the north to relax a horizontal curve. Expanded scour protection measures will be installed to address the existing bank erosion upstream and downstream.

The new bridge will be approximately 111 ft.-6 inches in length with an out-to-out deck width of 27 ft. and a clear roadway width of 24 ft. New, 20 ft.-6 inch reinforced, concrete bridge approaches that are 24 ft. in width will be installed at either end. The deck will be constructed with a 2 percent cross slope. The bridge railing will consist of standard, 2 ft.-9 inch concrete bridge rails and new, steel crash rated approach guardrail will be installed along both sides of the east and west approaches. Riprap will be installed along the east and west banks of Rider Ditch (around Bent No. 2 and Bent No. 3) for scour protection. Approximately 100 linear feet of permanent impacts to Rider Ditch will result from the construction of the new wall piers and placement of riprap below the Ordinary High Water Mark (OHWM) at the toe of slope for erosion control. Channel shaping limits will extend beyond the riprap and above the OHWM by the approximate lengths (measured perpendicular to the channel): northeast corner: 19 ft, northwest corner: 35 ft, southeast corner: 16 ft, southwest corner: 33 ft. for a total of 103 ft. of channel clearing work. The project limits will extend approximately 150 ft. west and 150 ft. east along East CR 300 South for a total project length of 401 ft. (0.081 mile).

Temporary cofferdams will be provided around the proposed wall pier locations via a temporary sheet pile or sandbag cofferdam. These temporary enclosures will be roughly 14 ft. x 41 ft. (574 sq. ft.) No temporary crossings are anticipated; however, temporary construction access paths will be installed in the northeast and northwest quadrants of the bridge crossing. These temporary access paths will consist of stone overlaid geotextiles and will impact approximately 0.096 acre. The temporary access paths are anticipated to be in place for approximately 180 days. The banks of Rider Ditch will be restored to preconstruction conditions and seeded per INDOT Standard Specifications.

Will the structure be rehabilitated or replaced as part of the project? Yes No N/A

If the proposed action has multiple bridges or small structures, this section should be filled out for each structure.

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| MAINTENANCE OF TRAFFIC (MOT) DURING CONSTRUCTION: |
|--|

| | Yes | No |
|---|-------------------------------------|-------------------------------------|
| Is a temporary bridge proposed? | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| Is a temporary roadway proposed? | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| Will the project involve the use of a detour or require a ramp closure? (describe in remarks) | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| Provisions will be made for access by local traffic and so posted. | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| Provisions will be made for through-traffic dependent businesses. | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| Provisions will be made to accommodate any local special events or festivals. | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| Will the proposed MOT substantially change the environmental consequences of the action? | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| Is there substantial controversy associated with the proposed method for MOT? | <input type="checkbox"/> | <input checked="" type="checkbox"/> |

Remarks: East CR 300 South has been closed at the bridge crossing since March 2019; however, a detour will be posted as part of the construction process. The preferred detour route will utilize SR 250 and S. CR 825 E., S. CR 840 E., and E CR 300 S. The detour will result in three miles of additional travel.

The road closure will pose a temporary inconvenience to traveling motorists (including school buses and emergency services); however, no significant delays are anticipated, and all inconveniences will cease upon project completion. There are no adjacent residential or commercial businesses that will require special access considerations. There is no MOT sheet for this project since the bridge and the portions of East CR 300 South immediately adjacent to the east and west of the bridge have been closed since March 2019. The project letting is scheduled for February 2023 and construction is anticipated to extend into November 2023.

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| ESTIMATED PROJECT COST AND SCHEDULE: |
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Engineering: \$244,613,000 (2020) Right-of-Way: \$15,000.00 (2022) Construction: \$1,025,000.00 (2023)

Anticipated Start Date of Construction: Spring 2023

Date project incorporated into STIP: July 2, 2019

Is the project in an MPO Area? **Yes** **No**

If yes,

Name of MPO: N/A

Location of Project in TIP: N/A

Date of incorporation by reference into the STIP: N/A

The scheduled letting date for the project has been extended to February 2023 as opposed to 2022 as illustrated in the FY 2020-2024 STIP. Once the preferred alternative has been determined based on the outcome of the public hearing, the project description will be updated in the STIP, in addition to the estimated cost of construction and right-of-way, if required. Any necessary modifications to the STIP will be completed before the Request for Contract (RFC).

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|----------------------|
| RIGHT OF WAY: |
|----------------------|

| Land Use Impacts | Amount (acres) | |
|---------------------------------------|----------------|-------------|
| | Permanent | Temporary |
| Residential | 0.00 | 0.00 |
| Commercial | 0.00 | 0.00 |
| Agricultural | 0.21 | 0.00 |
| Wooded | 0.11 | 0.00 |
| Wetlands | 0.01 | 0.00 |
| Other: Riparian Land (southeast side) | 0.03 | 0.00 |
| Channel Shaping | 0.11 | 0.12 |
| TOTAL | 0.47 | 0.12 |

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Describe both Permanent and Temporary right-of-way and describe their current use. Typical and Maximum right-of-way widths (existing and proposed) should also be discussed. Any advance acquisition or reacquisition, either known or suspected, and there impacts on the environmental analysis should be discussed.

Remarks: Additional permanent right-of-way will be required to complete the project. The existing right-of-way limits along East CR 300 South extends approximately 8 ft. north and south of the centerline (edge of the existing roadway). The proposed permanent right-of-way limits will extend approximately 50 ft. north and 58 ft. south of the centerline of East CR 300 South. Permanent right-of-way limits only extend as far upstream and downstream as the proposed riprap and channel shaping limits.

The proposed permanent right-of-way limits constrict on the east and west end of the project to approximately 12 ft. from the centerline at the east end to approximately 20 ft. from the centerline at the west end. The proposed additional permanent right-of-way consists of 0.21 acre of agricultural land and 0.11 acre of lightly wooded land adjacent to the stream crossing. Approximately 0.01 acre of forested wetland will be impacted in the southeast quadrant in addition to 0.03 acre of riparian land and 0.11 acre of land within Rider Ditch for channel re-shaping.

Approximately 0.12 acre of temporary right-of-way will be necessary for channel shaping and construction access. The temporary right-of-way limits will extend from a minimum of approximately 48 ft. in the southwest quadrant and a maximum of 90 ft. in the northeast quadrant. Construction access is anticipated to be undertaken from the northeast and northwest quadrant of the crossing. Plan sheets are provided in Appendix B, pages B-7 to B-13.

If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately.

Part III – Identification and Evaluation of Impacts of the Proposed Action

SECTION A – ECOLOGICAL RESOURCES

| | <u>Presence</u> | <u>Impacts</u> | |
|---|-----------------|----------------|-----------|
| | | <u>Yes</u> | <u>No</u> |
| Streams, Rivers, Watercourses & Jurisdictional Ditches | X | X | |
| Federal Wild and Scenic Rivers | | | |
| State Natural, Scenic or Recreational Rivers | | | |
| Nationwide Rivers Inventory (NRI) listed | | | |
| Outstanding Rivers List for Indiana | | | |
| Navigable Waterways | | | |

Remarks: Based on a desktop review, a site visit on May 27, 2020 by Metric Environmental (Metric), a review of the 2016 aerial photograph (Appendix B, page B-3), and the water resources map in the Red Flag Investigation (RFI) Report (Appendix E, pages E-2 and E-8), there are twenty-five (25) stream/waterway segments mapped in the project area. There are no Federal, Wild and Scenic Rivers; State Natural, Scenic, and Recreational Rivers; Outstanding Rivers for Indiana; navigable waterways or National Rivers Inventory waterways present in the project area. The U.S. Army Corps of Engineers (USACE) makes all final determinations regarding jurisdiction.

A Waters of the U.S. Determination / Wetland Delineation Report was prepared by Metric on November 20, 2020. Please refer to Appendix F for the *Waters of the U.S. Determination / Wetland Delineation Report*. The field investigation confirmed that one (1) waterway is present within the project area. In addition, the Muscatatuck River is located approximately 175 ft. south, beyond the construction limits.

Rider Ditch flows north to south into the Muscatatuck River, which flows east into the East Fork White River, a Section 10 Traditional Navigable Waterway (TNW). Therefore, Rider Ditch should be considered a jurisdictional Water of the U.S. The waterway is approximately 168 linear feet within the survey limits. Rider Ditch is associated with a solid blue line on the USGS topographic map, indicating it is likely a perennial waterway. The OHWM was measured at approximately 43. ft. wide and approximately 5 ft. deep both upstream and downstream of the bridge. The dominant stream substrate was silt. Rider Ditch was classified as average quality. Vegetation observed along the streambanks included bitternut hickory (*Carya cordiformis*) and stinging nettle (*Urtica dioica*).

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Remarks:

Riprap will be installed along the east and west banks of Rider Ditch for scour protection. Approximately 100 linear feet of permanent impacts to Rider Ditch will result from the construction of the new bridge piers and placement of riprap for erosion control at the toe of slope. Stream mitigation will not likely be required as the permanent waterway impacts are less than 300 linear feet. Temporary cofferdams will be built around the proposed wall pier locations via temporary sheet pile or sandbag cofferdams. These temporary enclosures will be roughly 14 ft. x 41 ft. (574 sq. ft.).

No temporary stream crossings are anticipated; however, temporary construction access will be necessary to install the falsework for slab construction. Temporary access paths will be installed in the northeast and northwest quadrants of the bridge crossing. These temporary access paths will consist of stone overlaid geotextiles and will impact approximately 0.096 acre of land. These access paths will not extend into the channel or below the OHWM of Rider Ditch. The temporary access paths are anticipated to be in place for approximately 180 days. Once construction is complete the access paths will be removed, and the banks of Rider Ditch will be graded and restored to preconstruction conditions and seeded per INDOT Standard Specifications.

Early coordination letters were sent on May 19, 2020, to the Indiana Department of Natural Resources-Division of Fish and Wildlife (IDNR), the U.S. Army Corps of Engineers (USACE) and the U.S. Fish and Wildlife Service (USFWS) (Appendix C, pages C-1 to C-2). Metric Environmental also generated the automatic IDEM Proposed Roadway Construction Projects letter, in which IDEM recommended obtaining the appropriate 401/404 permits (Appendix C, pages C-33 to C-39). The IDNR responded on June 18, 2020 with recommendations to avoid or minimize impacts to waterways, including bank stabilization measures, methods for riprap placement, and the minimization of in-channel disturbance (Appendix C, pages C-4 to C-6). All applicable IDNR recommendations are provided in the *Environmental Commitments* section of this CE document.

The USFWS responded on June 10, 2020 and did not object to the project as proposed (Appendix C, pages C-40 to C-41). The USFWS recommended restricting below low-water work in streams to placement of culverts, piers, pilings and/or footings, shaping of the spill slopes around the bridge abutments, and placement of riprap, and restrict channel work and vegetation clearing to the minimum necessary to construct the project. The USFWS also recommended minimizing the extent of hard armor (riprap) in bank stabilization by using bioengineering techniques whenever possible. This project meets the conditions of the USFWS 2013 Interim Policy, dated 5/29/2013, and no further coordination is required at this time. Should new information arise pertaining to project plans, it will be necessary to reinitiate consultation. The USACE did not respond to the early coordination letter.

| Other Surface Waters | <u>Presence</u> | <u>Impacts</u> | |
|-----------------------------------|--------------------------|--------------------------|--------------------------|
| | | Yes | No |
| Reservoirs | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| Lakes | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| Farm Ponds | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| Detention Basins | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| Storm Water Management Facilities | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| Other: | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |

Remarks:

Based on a desktop review, a site visit on May 27, 2020 by Metric Environmental, a review of the 2016 aerial photograph of the project area (Appendix B, page B-3), and the water resources map in the RFI report (Appendix E, pages E-2 and E-8), there is one (1) lake located within the 0.5 mile search radius. No surface waters are present within the project area therefore, no impacts are expected.

A *Waters of the U.S. Determination / Wetland Delineation Report* was prepared by Metric on July 29, 2020. Please refer to Appendix F for the *Waters of the U.S. Determination / Wetland Delineation Report*. It was determined that there are no surface waters located within or adjacent to the project area.

The early coordination letter was sent to the IDNR, the USFWS and the USACE on May 19, 2020 (Appendix C, pages C-1 to C-2). The IDNR responded on June 18, 2020 but did not reply with recommendations specific to surface waters (Appendix C, pages C-4 to C-6). The USFWS response dated June 10, 2020 had no recommendations related to other surface waters (Appendix C, pages C-40 and C-41). The USACE did not respond to the early coordination letter.

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| | | |
|--|-------------------------------------|--|
| | <u>Presence</u> | <u>Impacts</u> |
| | Yes | Yes No |
| Wetlands | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> <input type="checkbox"/> |
| Total wetland area: <u>0.122</u> acre(s) | Total wetland area impacted: | <u>0.012</u> Acre |

(If a determination has not been made for non-isolated/isolated wetlands, fill in the total wetland area impacted above.)

| Wetland No. | Classification | Total Size (Acres) | Impacted Acres | Comments |
|-------------|----------------|--------------------|----------------|----------|
| Wetland A | PFO1A | 0.122 | 0.01 | |

| | | |
|---------------------------------------|-------------------------------------|---------------------------------|
| | <u>Documentation</u> | <u>ES Approval Dates</u> |
| Wetlands (Mark all that apply) | | |
| Wetland Determination | <input checked="" type="checkbox"/> | <input type="checkbox"/> N/A |
| Wetland Delineation | <input checked="" type="checkbox"/> | <input type="checkbox"/> N/A |
| USACE Isolated Waters Determination | <input type="checkbox"/> | <input type="checkbox"/> |
| Mitigation Plan | <input type="checkbox"/> | <input type="checkbox"/> |

Improvements that will not result in any wetland impacts are not practicable because such avoidance would result in (Mark all that apply and explain):

- Substantial adverse impacts to adjacent homes, business or other improved properties;
 - Substantially increased project costs;
 - Unique engineering, traffic, maintenance, or safety problems;
 - Substantial adverse social, economic, or environmental impacts, or
 - The project not meeting the identified needs.
- | |
|----------|
| |
| |
| |
| |
| X |

Measures to avoid, minimize, and mitigate wetland impacts need to be discussed in the remarks box.

Remarks: Based on a review of the National Wetlands Inventory (NWI) online mapper (<https://www.fws.gov/wetlands/data/Mapper.html>), a site visit on May 27, 2019 by Metric Environmental, a review of the USGS topographic map (Appendix B page B-2), and the RFI report (Appendix E, page E-2) there are (11) wetlands within the 0.5 mile search radius.

A *Waters of the U.S. Determination / Wetland Delineation Report* was prepared by Metric on November 20, 2020. Please refer to Appendix F for the *Waters of the U.S. Determination / Wetland Delineation Report*. It was determined that one (1) Palustrine, Forested, Broad-Leaved Deciduous, Temporarily Flooded (PFO1A) wetland is present within and adjacent to the project area (Appendix F, page F-14).

Wetland A (0.122 acre)
 Wetland A is classified as a PFO1A wetland. This wetland is located in the southeast quadrant of Rider Ditch and East CR 300 South. Approximately 0.122 acre of Wetland A was delineated within the survey limits. The wetland continues south and east beyond the survey limits. The northern boundary of Wetland A was delineated by lack of wetland vegetation and increased elevation. Due to its location within a floodplain, Wetland A likely receives flood waters on a consistent basis during rain events. Based on topography, it can be deduced that water drains south in Rider Ditch, which flows into the Muscatatuck River, which flows into the East Fork White River, a Section 10 TNW. Therefore, Wetland A should be considered a jurisdictional Water of the U.S. The wetland is associated with a Palustrine, Forested, Broad-leaved Deciduous, Temporarily Flooded NWI polygon and was formed within a hydric mapped soil unit. The wetland exhibited decent plant diversity. These factors contribute to the conclusion the wetland can support an average amount of wildlife or aquatic habitat, and therefore should be considered average quality.

The dominant vegetation within Wetland A, located on the eastern bank of Rider Ditch, south of East CR 300 South was bitternut hickory (*Carya cordiformis*) and American beech (*Fagus grandifolia*) in the tree stratum, boxelder maple (*Acer negundo*) and common pawpaw (*Asimina triloba*) in the sapling/shrub stratum, stinging nettle (*Urtica dioica*) in the herb stratum, and common greenbrier (*Smilax rotundifolia*) in the woody vine stratum.

There will be approximately 0.01 acre of permanent impacts to Wetland A due to bridge and roadway construction activities. Construction access to build the new bridge piers and foundation units will take place from the northwest and northeast quadrants; however, fencing and "Do not Disturb" signs will be installed along the construction

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Remarks: boundaries in the southeast quadrant to avoid additional and unnecessary impacts to this wetland. This wetland area is marked with "Do Not Disturb" call out stamps on the plan sheets with instructions to the contractor to maintain all work in this area within the established construction limits. These additional avoidance and minimization measures to protect this wetland have been included as a firm commitment in the *Environmental Commitments* section of this CE document. Wetland mitigation will not likely be required as the permanent impacts are less than 0.10 acre. The disturbed area of Wetland A will be seeded according to INDOT standard specifications.

Early coordination letters were sent on May 19, 2020 to IDNR, the USFWS and the USACE (Appendix C, pages C-1 to C-2). The IDNR responded on June 18, 2020 and had no recommendations specific to wetlands (Appendix C, pages C-4 to C-6). The USACE did not respond and the USFWS did not have any recommendations specifically related to wetlands (Appendix C, pages C-40 to C-41).

| | <u>Presence</u> | <u>Impacts</u> | |
|--------------------------------|-----------------|----------------|----|
| Terrestrial Habitat | | Yes | No |
| Unique or High Quality Habitat | X | X | |

Remarks: *Use the remarks box to identify each type of habitat and the acres impacted (i.e. forested, grassland, farmland, lawn, etc).*

Based on a desktop review, a site visit on May 27, 2019 by Metric Environmental and a review of the 2016 aerial photograph of the project area (Appendix B, page B-3), the predominant land use in the project area consists of cultivated agricultural land.

Approximately 0.47 acre of terrestrial habitat is anticipated to be impacted to facilitate the proposed project. Approximately 0.03 acre of trees and understory brush will be removed in the northwest, southwest and southeast quadrants. The northeast quadrant is clear of trees and underbrush and there will be no need to conduct any clearing in this area. All efforts to minimize terrestrial impacts were considered during the design phase of the project. The construction limits have been reduced to the extent that is practical to build the project while limiting terrestrial disturbance. No terrestrial habitat restoration or mitigation will not likely be necessary.

The IDNR responded on June 18, 2020, with recommendations to minimize terrestrial impacts including revegetating all bare and disturbed areas with a mixture of grasses (excluding all varieties of tall fescue) and legumes as soon as possible upon project completion. The IDNR also recommended that appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from leavening the construction area and maintaining these measures until construction is complete and all disturbed areas are stabilized (Appendix C, pages C-4 to C-6). All applicable IDNR recommendations are included in the *Environmental Commitments* section of this CE document.

The automatic IDEM Proposed Roadway Construction Projects letter recommended that all solid waste generated by the project or removed from the project site be taken to an approved solid waste disposal facility (Appendix C, pages C-33 to C-39). The USFWS responded on June 10, 2020 and recommended implementing temporary erosion and sediment control methods within areas of disturbed soil. All disturbed soil areas upon project completion will be vegetated following INDOT's standard specifications. The USFWS also recommended avoiding clearing trees or understory vegetation outside the construction zone boundaries (Appendix C, pages C-40 to C-41).

If there are high incidences of animal movements observed in the project area, or if bridges and other areas appear to be the sole corridor for animal movement, consideration of utilizing wildlife crossings should be taken.

| | Yes | No |
|--|------------|-----------|
| Karst | | |
| Is the proposed project located within or adjacent to the potential Karst Area of Indiana? | | X |
| Are karst features located within or adjacent to the footprint of the proposed project? | | X |
| If yes, will the project impact any of these karst features? | | |

Use the remarks box to identify any karst features within the project area. (Karst investigation must comply with the Karst MOU, dated October 13, 1993)

Remarks: Based on a desktop review, the project is located outside the designated karst region of Indiana as outlined in the October 13, 1993 Memorandum of Understanding (MOU). According to the topo map of the project area (Appendix B, page B-2) and the RFI report (Appendix E, page E-2), there no karst features identified within or adjacent to the project area. The Indiana Geological Survey (IGS) responded to early coordination efforts on May 19, 2020 and did not indicate that karst features exist in the project area (Appendix C, page C-31 to C-32).

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Remarks: There is a high liquification potential for the project area and a 1 percent annual chance of flood hazard. There is a moderate potential for bedrock resources and a low potential for sand and gravel resources. No documented active or abandoned mineral resource extraction sites are documented within the search radius. The response from IGS has been communicated with the designer on May 19, 2020. No impacts are expected.

| Threatened or Endangered Species | <u>Presence</u> | <u>Impacts</u> | |
|--|-------------------------------------|-------------------------------------|-------------------------------------|
| | | Yes | No |
| Within the known range of any federal species | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| Any critical habitat identified within project area | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| Federal species found in project area (based upon informal consultation) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| State species found in project area (based upon consultation with IDNR) | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |

Is Section 7 formal consultation required for this action? Yes No

Remarks: Based on a desktop review and the RFI report (Appendix E) completed by Metric Environmental on October 10, 2019, the IDNR Jackson County Endangered, Threatened and Rare (ETR) Species List has been checked and is included in Appendix E, pages E-10 to E-12. The highlighted species on the list reflect the federal and state identified ETR species located within the county. According to the IDNR early coordination response letter dated June 18, 2020 (Appendix C, pages C-4 to C-6), the Natural Heritage Program’s Database has been checked and the Little Spectaclecase (*Villosa lienosa*), a state species of special concern, has been documented in Rider Ditch within the project area. The IDNR recommended that standard erosion control measures be implemented, and in-channel disturbance be minimized as much as possible, to avoid any impacts to Little Spectaclecase. These recommendations have been included as a firm commitment in the *Environmental Commitments* section of this CE document. No other plant or animal species listed as state or federally endangered, threatened and/or rare have been reported to occur in the project vicinity.

Project information was submitted through the USFWS’s Information for Planning and Consultation (IPaC) portal, and an official species list was generated (Appendix C, pages C-22 to C-27). The project is within range of the federally endangered Indiana bat (*Myotis sodalis*) and the federally threatened northern long-eared bat (NLEB) (*Myotis septentrionalis*). No additional species were found within or adjacent to the project area. The project qualifies for the USFWS 2013 Interim Policy, dated 5/29/2013. No additional coordination is required at this time; however, should new project plans or species be identified, it will be necessary to reinstate coordination.

The project qualifies for the *Range-wide Programmatic Informal Consultation for the Indiana bat and northern long-eared bat (NLEB)*, dated May 2016 (revised February 2018), between FHWA, Federal Railroad Administration (FRA), Federal Transit Administration (FTA), and USFWS. An effect determination key was completed on February 10, 2020 and based on the responses provided, the project was found to “May Affect/Not Likely to Adversely Affect” finding for the Indiana bat and the NLEB (Appendix C, pages C-7 to C-21).

INDOT reviewed and verified the effect finding on February 10, 2020 and requested USFWS’s review of the finding on July 6, 2020 (Appendix C, page C-28). No response was received from USFWS within the 14-day review period; therefore, it was concluded they concur with the finding. Avoidance and Mitigation Measures (AMMs) are included as firm commitments in the *Environmental Commitments* section of this CE document. This precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act, as amended. If new information on endangered species at the site becomes available, or if project plans are changed, USFWS will be contacted for consultation.

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SECTION B – OTHER RESOURCES

Drinking Water Resources

- Wellhead Protection Area
- Public Water System(s)
- Residential Well(s)
- Source Water Protection Area(s)
- Sole Source Aquifer (SSA)

| | <u>Presence</u> | <u>Impacts</u> | |
|---------------------------------|--------------------------|--------------------------|--------------------------|
| | | <u>Yes</u> | <u>No</u> |
| Wellhead Protection Area | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| Public Water System(s) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| Residential Well(s) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| Source Water Protection Area(s) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| Sole Source Aquifer (SSA) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |

If a SSA is present, answer the following:

- Is the Project in the St. Joseph Aquifer System?
- Is the FHWA/EPA SSA MOU Applicable?
- Initial Groundwater Assessment Required?
- Detailed Groundwater Assessment Required?

| | <u>Yes</u> | <u>No</u> |
|--|--------------------------|--------------------------|
| Is the Project in the St. Joseph Aquifer System? | <input type="checkbox"/> | <input type="checkbox"/> |
| Is the FHWA/EPA SSA MOU Applicable? | <input type="checkbox"/> | <input type="checkbox"/> |
| Initial Groundwater Assessment Required? | <input type="checkbox"/> | <input type="checkbox"/> |
| Detailed Groundwater Assessment Required? | <input type="checkbox"/> | <input type="checkbox"/> |

Remarks:

The project is located in Jackson County, which is not located within the area of the St. Joseph Sole Source Aquifer, the only legally designated sole source aquifer in the state of Indiana. Therefore, the FHWA/EPA Sole Source Aquifer Memorandum of Understanding (MOU) is not applicable to this project. Therefore, a detailed groundwater assessment is not needed, and no impacts are expected.

The Indiana Department of Environmental Management’s Wellhead Proximity Determinator website (<http://www.in.gov/idem/cleanwater/pages/wellhead/>) was accessed on May 4, 2020 by Metric Environmental. This project is not located within a Wellhead Protection Area or Source Water Area. No impacts are expected.

The Indiana Department of Natural Resources Water Well Record Database website (<https://www.in.gov/dnr/water/3595.htm>) was accessed on May 4, 2020 by Metric Environmental. No wells are located near the project site. Therefore, no impacts are expected.

Based on a desktop review of the INDOT MS4 website (<https://entapps.indot.in.gov/MS4/>) by Metric Environmental on May 4, 2020, and the RFI report this project is not located in an Urban Area Boundary location. No impacts are expected. Based on a desktop review, a site visit on May 27, 2020 by Metric Environmental, a review of the 2016 aerial photograph of the project area (Appendix B, page B-3) no public water systems were identified. Therefore, no impacts are expected.

Flood Plains

- Longitudinal Encroachment
- Transverse Encroachment
- Project located within a regulated floodplain
- Homes located in floodplain within 1000’ up/downstream from project

| | <u>Presence</u> | <u>Impacts</u> | |
|---|-------------------------------------|-------------------------------------|--------------------------|
| | | <u>Yes</u> | <u>No</u> |
| Longitudinal Encroachment | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| Transverse Encroachment | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| Project located within a regulated floodplain | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| Homes located in floodplain within 1000’ up/downstream from project | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |

Remarks:

Discuss impacts according to classification system described in the “Procedural Manual for Preparing Environmental Studies”.

Based on a desktop review of The Indiana Department of Natural Resources Indiana Floodway Information Portal website (<http://dnrmmaps.dnr.in.gov/appsphp/fdms/>) by Metric Environmental on May 4, 2020 and the RFI report; this project is located within a regulatory floodplain as determined from approved IDNR floodplain maps (Appendix F, page F-12).

Early coordination letters were sent on May 19, 2020 to the IDNR, the USACE and the Jackson County Floodplain Administrator (Appendix C, pages C-1 to C-2). The IDNR responded on June 18, 2020 that this project will require the formal approval of their agency for construction in a floodway pursuant to the Flood Control Act (IC 14-28-1) (Appendix C, pages C-4 to C-6). The project will require a construction in a Floodway Permit. The USACE and the Jackson County Floodplain Administrator did not respond within the 30-day time frame.

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Remarks: This project qualifies as a Category 4 per the current *INDOT CE Manual*, for projects involving replacement of existing drainage structures on essentially the same alignment which states the modifications to drainage structures included in this project will result in an insubstantial change in their capacity to carry flood water. There are no homes located within the base floodplain within 1,000 ft. upstream or downstream. The closest structure appears to be a large barn structure that is located approximately 1,600 ft. southwest of the project site. The proposed structure will have an effective capacity such that backwater surface elevations are not expected to substantially increase. As a result, there will be no substantial adverse impacts on natural and beneficial floodplain values; there will be no substantial change in flood risks; and there will be no substantial increase in potential for interruption or termination of emergency service or emergency evacuation routes; therefore, it has been determined that this encroachment is not substantial.

A hydraulic design study that addresses various structure size alternatives will be completed during the preliminary design phase. The designer will provide a summary of this study and a summary of the resolution of floodplain impacts for the project file.

| Farmland | <u>Presence</u> | <u>Impacts</u> | | | | |
|---------------------------|---|----------------|---|---|---|--|
| | | Yes | No | | | |
| Agricultural Lands | <table border="1" style="margin: auto;"><tr><td style="text-align: center;">X</td></tr></table> | X | <table border="1" style="margin: auto;"><tr><td style="text-align: center;">X</td></tr></table> | X | <table border="1" style="margin: auto;"><tr><td style="width: 20px; height: 15px;"></td></tr></table> | |
| X | | | | | | |
| X | | | | | | |
| | | | | | | |
| Prime Farmland (per NRCS) | <table border="1" style="margin: auto;"><tr><td style="text-align: center;">X</td></tr></table> | X | <table border="1" style="margin: auto;"><tr><td style="text-align: center;">X</td></tr></table> | X | <table border="1" style="margin: auto;"><tr><td style="width: 20px; height: 15px;"></td></tr></table> | |
| X | | | | | | |
| X | | | | | | |
| | | | | | | |

Total Points (from Section VII of CPA-106/AD-1006* 150
**If 160 or greater, see CE Manual for guidance.*

See CE Manual for guidance to determine which NRCS form is appropriate for your project.

Remarks: Based on a desktop review, a site visit on May 27, 2020 by Metric Environmental, the 2016 aerial photograph of the project area (Appendix B, page B-3), the project will convert 0.10 acre of farmland as defined by the Farmland Protection Policy Act. An early coordination letter was sent on May 19, 2020 to Natural Resources Conservation Services (NRCS). Coordination with NRCS resulted in a score of 150 on the NRCS-AD 1006 Form (Appendix C, pages C-29 to C-30). NRCS's threshold score for significant impacts to farmland that result in the consideration of alternatives is 160. Since this project score is less than the threshold, no significant loss of prime, unique, statewide, or local important farmland will result from this project. No alternatives other than those previously discussed in this document will be investigated without reevaluating impacts to prime farmland.

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SECTION C – CULTURAL RESOURCES

| | Category | Type | INDOT Approval Date | N/A |
|-----------------------------|--------------------------|--------------------------|--------------------------|-------------------------------------|
| Minor Projects PA Clearance | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |

Results of Research

| | <u>Eligible and/or Listed Resource Present</u> |
|------------------------|--|
| Archaeology | <input type="checkbox"/> |
| NRHP Buildings/Site(s) | <input type="checkbox"/> |
| NRHP District(s) | <input type="checkbox"/> |
| NRHP Bridge(s) | <input checked="" type="checkbox"/> |

Project Effect

| | | | | | |
|---------------------------------|-------------------------------------|-------------------|--------------------------|----------------|--------------------------|
| No Historic Properties Affected | <input checked="" type="checkbox"/> | No Adverse Effect | <input type="checkbox"/> | Adverse Effect | <input type="checkbox"/> |
|---------------------------------|-------------------------------------|-------------------|--------------------------|----------------|--------------------------|

Documentation Prepared

| | | <u>ES/FHWA Approval Date(s)</u> | <u>SHPO Approval Date(s)</u> |
|--|-------------------------------------|---------------------------------|------------------------------|
| Documentation (mark all that apply) | | | |
| Historic Properties Short Report | <input checked="" type="checkbox"/> | February 25, 2020 | March 30, 2020 |
| Historic Property Report | <input type="checkbox"/> | | |
| Archaeological Records Check/ Review | <input checked="" type="checkbox"/> | February 21, 2020 | March 30, 2020 |
| Archaeological Phase Ia Survey Report | <input checked="" type="checkbox"/> | February 21, 2020 | March 30, 2020 |
| Archaeological Phase Ic Survey Report | <input type="checkbox"/> | | |
| Archaeological Phase II Investigation Report | <input type="checkbox"/> | | |
| Archaeological Phase III Data Recovery | <input type="checkbox"/> | | |
| APE, Eligibility and Effect Determination | <input checked="" type="checkbox"/> | May 26, 2020 | June 8, 2020 |
| 800.11 Documentation | <input checked="" type="checkbox"/> | May 26, 2020 | June 8, 2020 |

Memorandum of Agreement (MOA)

| | | |
|--|--------------------------|---|
| | <input type="checkbox"/> | MOA Signature Dates (List all signatories) |
| | | |

Describe all efforts to document cultural resources, including a detailed summary of the Section 106 process, using the categories outlined in the remarks box. The completion of the Section 106 process requires that a Legal Notice be published in local newspapers. Please indicate the publication date, name of paper(s) and the comment period deadline. Likewise include any further Section 106 work which must be completed at a later date, such as mitigation or deep trenching.

Remarks: Per the terms of the “Programmatic Agreement Regarding Management and Preservation of Indiana’s Historic Bridges” (Historic Bridges PA), the Federal Highway Administration-Indiana Division (FHWA) will satisfy its Section 106 responsibilities involving “Select” and “Non-Select” bridges through the Project Development Process (PDP) of the Historic Bridges PA (Stipulation III).

Jackson County Bridge 154 has been classified as a Non-Select Bridge by the INDOT Historic Bridge Inventory, and thus, the procedures outlined in Stipulation III.B of the Historic Bridges PA will be followed to fulfill FHWA’s Section 106 responsibilities for the bridge. Therefore, the finding for this project only applies to other resources located within the APE and not Jackson County Bridge 154. This document will satisfy the Section 106 responsibilities for other resources located in the APE.

Area of Potential Effect:
 Qualified professionals working for Metric and meeting the Secretary of the Interior’s Professional Qualifications Standards defined an Area of Potential Effect. The Area of Potential Effects (APE) is “the geographic area or areas within which an undertaking may directly or indirectly cause alterations in the character or use of historic properties, if any such properties exist. The area of potential effects is influenced by the scale and nature of an undertaking and may be different for different kinds of effects caused by the undertaking” [36 CFR § 800.16(d)].

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Remarks: The APE for aboveground resources was drawn sufficiently large to encompass potential impacts including visual, physical, and traffic-related impacts that may result from the undertaking, whichever alternative is selected. The established APE consists of a 0.25 (1,350 ft.) mile radius around the bridge (Appendix D, page D-8).

Coordination with Consulting Parties:

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. In accordance with 36 CFR 800.2(c), individuals and groups with a demonstrated interest in the undertaking were invited to participate in efforts to identify historic properties potentially affected by the undertaking, assess its effects, and seek ways to avoid, minimize or mitigate any adverse effects on historic properties.

On November 12, 2019, the following individuals and groups listed in the table below were sent an email on behalf of INDOT requesting them to act as a consulting party for the undertaking. They were also advised that the Early Coordination Letter was available for review at the INDOT's Section 106 Consultation and Outreach Portal Enterprise, known as INSCOPE. The invitees were requested to respond within 30 days indicating whether the agency agreed or did not agree to participate as a consulting party. Also, on November 12, 2019 the INDOT Cultural Resources Office emailed the Native American Tribes listed in the table to invite them to be consulting parties, and to direct them to the documents available for review on INSCOPE. It was noted in the email correspondence that if no response was received, the individual or group would not be considered a consulting party and would not receive further information about the undertaking unless the scope changed.

| Invited Organization | Reply Received |
|--|-------------------|
| Indiana Landmarks (Southern Regional Office) | None Received |
| Jackson County Historical Center | None Received |
| Jackson County Historian | None Received |
| Jackson County Commissioners | None Received |
| Jackson County Parks and Recreation Board | None Received |
| Dr. James L. Cooper | None Received |
| Historicbridges.com | None Received |
| Historic Spans Taskforce | None Received |
| Historic Hoosier Bridges | None Received |
| Historic Bridge Foundation | None Received |
| Eastern Shawnee Tribe of Oklahoma | None Received |
| Miami Tribe of Oklahoma | December 10, 2019 |
| Delaware Tribe of Indians, Oklahoma | None Received |
| Peoria Tribe of Indians of Oklahoma | None Received |
| Pokagon Band of Potawatomi Indians | None Received |

The Indiana Department of Natural Resources, Division of Archaeology and Historic Preservation (SHPO) is automatically considered a consulting party for federally funded transportation projects due to its mandated or designated role as specified in 36 C.F.R. § 800.2. The SHPO was also emailed a copy of this early coordination letter and sent a paper copy of the documentation for review and comment on November 12, 2019. On December 16, 2019 SHPO replied that they were not aware of any additional parties who should be invited to participate in the Section 106 consultation (Appendix D, pages D-31 to D-32). An affirmative response to join in consultation was received from the Miami Tribe of Oklahoma on December 10, 2019 (Appendix D, page D-30).

The letter from the Miami Tribe of Oklahoma offered no objection to the project but stated that "if any human remains or Native American cultural items falling under the Native American Graves Protection and Repatriation Act (NAGPRA) or archaeological evidence is discovered during any phase of this project, the Miami Tribe requests immediate consultation with the entity of jurisdiction for the location of discovery." No other responses were received from invited tribes.

In January 2020, a representative of the Jackson County Parks and Recreation made a telephone inquiry to SHPO staff regarding truss bridges in Jackson County that may be proposed for replacement. The inquiry was forwarded to INDOT Cultural Resources Office staff, who responded to the representative via email on January 10, 2020.

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Remarks: On the same date, SHPO staff also encouraged the representative via email to let INDOT know if she wanted to become a consulting party for the project. No response was received from the representative of the Jackson County Parks and Recreation.

Archaeology:

Pursuant to 36 CFR § 800.4(b), a Qualified Professional Archaeologist with Metric conducted an archaeological records check on January 2, 2020 which involved review the State Historical Architectural and Archaeological Research Database (SHAARD), site maps on file with the IDNR-Division of Historic Preservation and Archaeology, cultural resource management reports, cemetery records, and historical data. A Phase Ia Archaeological Survey was conducted on January 7, 2020. No archaeological resources were identified in the project area. On January 31, 2019, an Archaeology Short Report (ASR) (Snell 1/31/2019) was prepared and recommended the project proceed as planned. On February 24, 2020 INDOT-Cultural Resources Office (CRO) approved the report and the document was uploaded to INSCOPE on February 24, 2020. On February 24, 2020 INDOT-CRO notified the Miami Tribe of Oklahoma of the approval of the ASR. No reply was received. The SHPO concurred with the ASR in a letter dated March 30, 2020 (Appendix D, pages D-40 to D-41). Excerpts of the ASR are provided in Appendix D, pages D-21 to D-23.

Historic Properties:

Pursuant to 36 CFR § 800.4(b), personnel with Metric Environmental (Metric), who meet the Secretary of the Interior's Professional Qualification Standards as per 36 CFR Part 61, reviewed the NRHP, the Indiana Historic Sites and Structures Inventory (IHSSI), the Indiana State Historical Architectural and Archaeological Research Database (SHAARD) and the Indiana Historic Buildings, Bridges, and Cemeteries Map, the Indiana Historic Bridge Inventory, the Indiana Historical Bureau's Historical Markers Database, and the 1988 Jackson County Interim Historic Sites and Structures Inventory (IHSSI) for previously-identified properties. Primary and secondary documentary research included numerous published county and local histories, historical and current atlases and maps, and online resources. Additionally, in January 2020 a field survey was conducted by walking all the areas within the APE and taking photographs in an effort to identify and evaluate any historic resources present. A Historic Property Short Report (HPSR) (Becher Gilliam, 2/21/2020) was developed and provided recommendations concerning the historic significance of the properties within the APE. As a result of identification and evaluation efforts for this project, no properties within the project APE were recommended eligible for listing in the NRHP.

The HPSR was submitted to the CRO for review on January 31, 2020, and on February 24, 2020 their office released the document for consulting party review. On February 25, 2020, the Miami Tribe of Oklahoma and the SHPO; consulting parties who had accepted the invitation to participate were sent a letter to notify them that the HPSR and the Archaeology Report were available for review on INSCOPE. The SHPO concurred with the limits of the established APE and with the findings of the HPSR in a letter dated March 30, 2020 (Appendix D, pages D-40 to D-41). No other comments from the consulting parties were received. Excerpts of the HPSR are provided in Appendix D, pages D-18 to D-20.

Documentation, Findings:

Per the terms of the "Programmatic Agreement Regarding Management and Preservation of Indiana's Historic Bridges" (Historic Bridges PA), the Federal Highway Administration-Indiana Division (FHWA) will satisfy its Section 106 responsibilities involving "Select" and "Non-Select" bridges through the Project Development Process (PDP) of the Historic Bridges PA (Stipulation III). Jackson County Bridge No. 154 (NBI #3600099) is classified as a "Non-Select" bridge by the *Indiana Historic Bridge Inventory* and thus, the procedures outlined in Stipulation III. of the Historic Bridges PA will be followed to fulfill FHWA's Section 106 responsibilities. Photo documentation of the bridge will be conducted consistent with the *Historic Bridges PA: Attachment B- Standard Treatment Approach for Historic Bridges*. This will apply regardless of whether Alternative E or F is the chosen as the preferred alternative.

Per the terms of the Historic Bridge PA, the finding for this project only applies to other resources located within the APE and not Jackson County Bridge No. 154. Regarding other resources in the project area, INDOT, on behalf of the FHWA, has determined a "No Historic Properties Affected" finding is appropriate because no other properties listed in or eligible for listing in the National Register are present within the APE.

On May 26, 2020, the INDOT-CRO, on behalf of the FHWA approved the APE and issued a "No Historic Properties Affected" finding for this project (Appendix D, Page D-1 to D-7). Following this finding, the effect documentation was provided to the Indiana SHPO for a 30-day review and comment period.

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Remarks: On June 8, 2020, the Indiana SHPO responded and concurred with the “No Historic Properties Affected” finding (Appendix D, Page D-50 to D-51). No additional responses were received.

Public Involvement:

In accordance with 36 CFR 800.2(d), 800.3(e), and 800.6(a)(4), the views of the public were sought regarding the effect of the proposed project. A legal notice was published in the *Seymour Tribune* on May 30, 2020 with a 30-day comment period. The 30-day deadline for comments was June 29, 2020. No comments were received by the 30-day deadline. A copy of the legal notice and the publisher’s affidavit are provided in Appendix D, pages D-52 to D-56.

The HBAA Addendum (6/24/2021) was sent out to CPs on July 13, 2021. An e-mail was sent to the tribal consulting parties inviting them to review and comment on the HBAA (Appendix D, page D-178). No responses were received from any of the tribal consulting parties. The SHPO responded with their concurrence of the HBAA on July 26, 2021 (Appendix D, Pages D-179 to D-180).

In accordance with the HBPA, Stipulation III.B.2, on November 15, 2019 a legal notice to interested parties for proposals for the rehabilitation and reuse, or the storage and future reuse of the bridge was published in the *Seymour Tribune* newspaper and the advertisement was also included on the INDOT Historic Bridges Marketing Program website. Signs were posted at the bridge site on November 19, 2019 (Appendix B, page B-6). A secondary advertisement was also placed in the *Indianapolis Star* on May 1, 2020. The bridge must be marketed at for at least six months prior to the required public hearing. The six-month marketing period for the May 1, 2020 publication, ended on October 31, 2020. To date no interested parties have come forward to take ownership of Jackson County Bridge 154. The text of the legal notices and the affidavits of publication are provided in Appendix D, pages D-44 to D-49.

Pursuant to the Historic Bridge Programmatic Agreement (PA), a public hearing for the project is required. A legal notice for the public hearing will appear in a local publication contingent upon the release of this document for public involvement. The Section 106 process will be complete after the public hearing is held and the *Public Involvement* section of this document is updated per the outcome of that public hearing.

SHPO has determined that photo documentation of the bridge is required consistent with the *Historic Bridges PA: Attachment B- Standard Treatment Approach for Historic Bridges* (Appendix D, Pages D-179 to D-180). The documentation shall be produced in keeping with the applicable photographic standards of the *Indiana DNR–Division of Historic Preservation and Archaeology Minimum Architectural Documentation*. One CD or DVD of the documentation shall be provided to the Indiana State Archives and one CD or DVD shall be provided to at least one local public or not-for-profit organization that agrees to retain the CD or DVD permanently and make it available to the public.

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SECTION D – SECTION 4(f) RESOURCES/ SECTION 6(f) RESOURCES

Section 4(f) Involvement (mark all that apply)

Parks & Other Recreational Land

| | <u>Presence</u> |
|--|--------------------------|
| Publicly owned park | <input type="checkbox"/> |
| Publicly owned recreation area | <input type="checkbox"/> |
| Other (school, state/national forest, bikeway, etc.) | <input type="checkbox"/> |

| <u>Use</u> | |
|--------------------------|--------------------------|
| Yes | No |
| <input type="checkbox"/> | <input type="checkbox"/> |
| <input type="checkbox"/> | <input type="checkbox"/> |
| <input type="checkbox"/> | <input type="checkbox"/> |

Evaluations Prepared

| | |
|----------------------------|--------------------------|
| Programmatic Section 4(f)* | <input type="checkbox"/> |
| “De minimis” Impact* | <input type="checkbox"/> |
| Individual Section 4(f) | <input type="checkbox"/> |

FHWA Approval date

Wildlife & Waterfowl Refuges

| | <u>Presence</u> |
|---------------------------|--------------------------|
| National Wildlife Refuge | <input type="checkbox"/> |
| National Natural Landmark | <input type="checkbox"/> |
| State Wildlife Area | <input type="checkbox"/> |
| State Nature Preserve | <input type="checkbox"/> |

| <u>Use</u> | |
|--------------------------|--------------------------|
| Yes | No |
| <input type="checkbox"/> | <input type="checkbox"/> |
| <input type="checkbox"/> | <input type="checkbox"/> |
| <input type="checkbox"/> | <input type="checkbox"/> |

Evaluations Prepared

| | |
|----------------------------|--------------------------|
| Programmatic Section 4(f)* | <input type="checkbox"/> |
| “De minimis” Impact* | <input type="checkbox"/> |
| Individual Section 4(f) | <input type="checkbox"/> |

FHWA Approval date

Historic Properties

| | <u>Presence</u> |
|--|-------------------------------------|
| Sites eligible and/or listed on the NRHP | <input checked="" type="checkbox"/> |

| <u>Use</u> | |
|-------------------------------------|--------------------------|
| Yes | No |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> |

Evaluations Prepared

| | |
|----------------------------|-------------------------------------|
| Programmatic Section 4(f)* | <input checked="" type="checkbox"/> |
| “De minimis” Impact* | <input type="checkbox"/> |
| Individual Section 4(f) | <input type="checkbox"/> |

FHWA Approval date

**FHWA approval of the environmental document also serves as approval of any Section 4(f) Programmatic and/or De minimis evaluation(s) discussed below.*

Discuss Programmatic Section 4(f) and “de minimis” Section 4(f) impacts in the remarks box below. Individual Section 4(f) documentation must be separate Draft and Final documents. For further discussions on Programmatic, “de minimis” and Individual Section 4(f) evaluations please refer to the “Procedural Manual for the Preparation of Environmental Studies”. Discuss proposed alternatives that satisfy the requirements of Section 4(f).

Remarks: Section 4(f) of the U.S. Department of Transportation Act of 1966 prohibits the use of certain public and historic lands for federally funded transportation facilities unless there is no feasible and prudent alternative. The law applies to significant publicly owned parks, recreation areas, wildlife / waterfowl refuges, and NRHP eligible or listed historic properties regardless of ownership. Lands subject to this law are considered Section 4(f) resources.

Based on a desktop review, a site visit on May 27, 2020 by Metric Environmental, the 2016 aerial photograph of the project area (Appendix B, page B-3), and the RFI report (Appendix E, page E-2) there are no 4(f) resources located within the 0.5 mile search radius other than Jackson County Bridge 154. Jackson County Bridge 154 is afforded protection under Section 4(f) as a historic site that is eligible for listing on the NRHP.

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Remarks:

The Section 4(f) statute places restrictions on the use of land from historic sites for highway improvements but makes no mention of historic bridges or highways that are already serving as transportation facilities. FHWA therefore, determined that Section 4(f) will only apply when a historic bridge is demolished, or if the historic quality for which the facility was determined eligible for the NRHP is substantially affected by the proposed improvements. This resource is used for transportation purposes. Jackson County Bridge 154 will be evaluated through the *Programmatic Section 4(f) Evaluation and Approval for FHWA Projects that Necessitate the Use of Historic Bridges*. The proposed bridge project qualifies for the programmatic Section 4(f) evaluation and approval for FHWA projects that necessitate the use of a historic bridge when the project meets the following criteria:

1. The bridge is to be replaced or rehabilitated with Federal funds.
2. The project will require the use of a historic bridge structure which is on or is eligible for listing on the NRHP.
3. The bridge is not a National Historic Landmark.
4. The FHWA Division Administrator determines that the facts of the project match those set forth by the investigation of the appropriate Alternatives, Findings, and Mitigation.
5. Agreement among the FHWA, the SHPO, and the ACHP has been reached through procedures pursuant to Section 106 of the NHPA.

The Jackson County Bridge 154 bridge project meets these criteria. To apply the Historic Bridge Programmatic Section 4(f) Evaluation, three alternatives that avoid any use of the historic bridge must be examined: do nothing, build a new structure at a different location without affecting the historic integrity of the historic bridge, and rehabilitate the historic bridge without affecting the historic integrity of the structure. The Indiana Historic Bridges PA requires a more extensive alternatives analysis evaluating additional alternatives. Per the terms Historic Bridges PA, FHWA will satisfy its Section 106 responsibilities involving "Select" and "Non-Select" bridges through the PDP of the Historic Bridges PA (Stipulation III).

Jackson County Bridge 154 has been classified as a Non-Select Bridge by the INDOT Historic Bridge Inventory, and thus, the procedures outlined in Stipulation III.B of the Historic Bridges PA will be followed to fulfill FHWA's Section 106 responsibilities for the bridge. The alternatives described in this document are based on the guidance for writing a historic bridge Section 4(f) alternatives analysis, produced by Janssen & Spaans Engineering Inc. Per the guidance, alternatives A, B1, B2, C1, C2, D1, D2, E, and F must be analyzed in consecutive order until a feasible and prudent alternative has been determined which also results in the least amount of harm to the protected resource. A feasible alternative is one that is possible to engineer, design, and build, and a prudent alternative is one that does not present significantly unique or unusual factors (e.g. cost; social, economic, or environmental impacts; community disruption). Once a feasible and prudent alternative has been determined, the remaining alternatives do not need to be analyzed.

As previously explained, an initial HBAA was developed and approved in May 2020. After approval of the initial HBAA, modifications were made to the proposed project design and a HBAA Addendum was prepared to re-evaluate the alternatives. The HBAA Addendum is provided in Appendix D, pages D-57 to D-177.

Alternative A: Do Nothing Alternative

This alternative means that no federal funds would be expended, and no action would be taken to correct the current deficiencies. The road would remain closed, and the bridge would continue to deteriorate. This alternative would not meet the purpose and need for the project. For this reason, this alternative was discarded from further consideration.

Alternative B-1: Rehabilitation in Accordance with the Secretary of the Interior's Standards

Alternative B-1 involves rehabilitating the existing bridge to a standard that meets the Secretary of Interior's Standards (SOIS) for Rehabilitation. This alternative would maintain the structure's historic integrity and restore the structure's original design capacity. To achieve this objective, all replacement procedures would maintain and/or restore the historic elements of the structure as closely as possible. The expected service life after rehabilitation would be 25 years but, the bridge would still have a deficient load rating. This alternative does not provide the minimum structural capacity of 15 tons as required by the purpose and need. For this reason, this alternative was discarded from further consideration.

Alternative B-2: Rehabilitation Not in Accordance with the Secretary of Interior's Standards

This alternative would rehabilitate the existing bridge in a manner not compliant with the SOIS for Rehabilitation. The bridge's historic features would not be considered in the repairs.

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Remarks:

The primary goal of this alternative would be to correct the existing structural deficiencies and strengthen the bridge to handle a minimum H15 loading rating. The expected service life after rehabilitation would be 25 years but the rehabilitation would significantly modify the original structure by replacing the tension eye-bars, diagonals, and stringers with new steel members of differing size, shape, and type in order to provide the required capacity. Guardrail and end treatments would be installed to prevent vehicular strikes to the pony trusses and errant vehicle entry into Rider Ditch. The estimated cost for this alternate was determined to be 42% higher than the replacement cost of Alternative F. The bridge would continue to have insufficient width and load capacity and would not meet the purpose and need. For these reasons, this alternative was discarded from further consideration.

Alternative C-1: Rehabilitation in Accordance with the Secretary of Interior's Standards (One-way Pair)

This alternative would rehabilitate the existing bridge in the same fashion as Alternative B-1 except that a new bridge would be built adjacent to the existing for use as a one-way pair. The new bridge would be a continuous three-span, reinforced concrete slab bridge. This alternative would also include building an additional approach roadway. In addition to the rehabilitation costs described in Alternative B-1, this option includes additional costs associated with a new bridge, increased right-of-way purchasing, and road approach modification. This alternate would be constructed on an existing one-lane, two-way road which is not intended to be upgraded from this configuration. As noted previously in the discussion of Alternative B-1, for Alternative C-1 the structure would be rehabilitated to the SOIS and, the expected service life after rehabilitation would be 25 years. As identical to Alternative B-1, the rehabilitated truss would have substandard load capacity. Although Alternative C-1 is feasible, the estimated cost of this alternate is 26% higher than the replacement cost of Alternative F. This alternate would not improve the load carrying capacity of the existing bridge to the minimum of 15 tons. For these reasons, this alternative was discarded from further consideration.

Alternative C-2: Rehabilitation Not in Accordance with the Secretary of Interior's Standards (One-way Pair)

This alternative would rehabilitate the existing bridge in the same fashion as Alternative B-2 except that a new bridge would be built adjacent to the existing for use as a one-way pair. The new bridge would be a continuous three-span, reinforced concrete slab bridge. This alternative would also include building an additional approach roadway. For additional details related to rehabilitation of the existing structure see the description in Alternative B-2. As with Alternative B-2 the expected service life extension would be 25 years. In addition to the rehabilitation costs as described in Alternative B-2, this alternative includes additional costs associated with a new bridge, increased right-of-way purchasing, and road approach modification. This alternate would be constructed on an existing one-lane two-way road which is not intended to be upgraded from this configuration. Identical to Alternative B-2, the existing truss would be significantly modified to provide enough load capacity. Alternative C-2 would not rehabilitate the structure to the SOIS and would include the introduction of replacement elements. Although Alternative C-2 is feasible, the estimated cost for this alternative is 36% higher than the replacement cost of Alternative F. For these reasons, this alternative was discarded from further consideration.

Alternative D: Rehabilitation of the Existing Structure with a Two-Way Bypass

Alternative D would rehabilitate the existing bridge for non-vehicular use and build a bypass bridge adjacent to it. This alternative is effectively identical to Alternate C-1. This Alternative would rehabilitate the truss to the SOIS standards. The main difference is that following rehabilitation, the existing bridge will be closed to vehicular traffic and the adjacent approach will be graded for parking. Due to the severely deteriorated condition of the existing structure, it would need to be rehabilitated in an equivalent manner to the B-1 Alternative. Once rehabilitated and permanently closed to vehicle traffic, inspection requirements would no longer be applicable. This alternative would require a private party to assume ownership of the bridge and maintain the bridge for perpetuity. To date no interested parties have come forward to take ownership of Jackson County Bridge 154 as required for this alternate. Alternative D is feasible; however, the estimated cost for this alternative is 26% higher than the replacement cost of Alternative F. For these reasons, this alternative will likely be discarded from further consideration.

Alternative E: Relocation of Existing Bridge and New Bridge on Current Alignment

Alternative E would relocate the historic bridge and build a new bridge on the current alignment. The existing bridge would not be destroyed; however, a responsible party must come forward and fund the relocation and rehabilitation of the bridge. The existing truss bridge would be replaced by a three-span, haunched concrete slab bridge on the existing alignment and would meet all applicable design criteria. The cost of this alternative is \$1,025,000.00 which is 94% of the demolition and replacement Alternative F. This cost takes into account that a private party would fund the dismantling and removal of the existing bridge. The bridge has been advertised for the minimum six month marketing period but to date, no interested party has come forward.

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Remarks: The opportunity to relocate and reuse the bridge will remain viable until the public hearing requirements for this project have concluded. As a result, this alternate is still feasible. Given that this alternate is cost effective and meets the purpose and need, this alternative is also prudent if a responsible party comes forward to take ownership of the bridge.

Alternative F: Bridge Replacement

Alternative F would demolish the existing truss bridge and a new, three-span bridge would be built on the same alignment. There would be no bridge relocation process included as part of this alternative. The existing truss bridge would be replaced by a three-span, haunched concrete slab bridge on essentially the same alignment and would meet all applicable design criteria. If Alternative F is chosen as the preferred alternative, the cost would be approximately \$1,090,000.00, which includes the cost to construct the new bridge and demolish the existing bridge. Impacts to the bridge would be mitigated through the stipulations outlined within the Historic Bridges Programmatic Agreement (HBPA) process. This alternative is both feasible and prudent and meets the purpose and need of the project by providing a cost effective and structurally sufficient bridge at the project site.

The programmatic Section 4(f) evaluation and approval may be used only for projects where the FHWA Division Administrator, in accordance with this evaluation, ensures that the proposed action includes all possible planning to minimize harm. The project has considered all appropriate measures to minimize harm and mitigate for adverse impacts or effects on Jackson County Bridge 154, including development of the initial alternative analysis.

Alternative E would meet the project purpose and need; however, if a responsible party does not come forward to relocate the bridge, then Alternative F will become the preferred alternative. Alternative F would result in demolition of Jackson County Bridge 154; therefore, the required photo documentation as described below, will mitigate for the adverse effect to the bridge. If an interested party comes forward to acquire the bridge, it would be dismantled and relocated to a different site.

The HBAA Addendum was sent out to CPs on July 13, 2021. An e-mail was sent to the tribal consulting parties inviting them to review and comment on the HBAA (Appendix D, page D-178). No responses were received from any of the tribal consulting parties. The SHPO responded with their concurrence of the HBAA on July 26, 2021 (Appendix D, Pages D-179 to D-180). The SHPO letter stated "If no responsible party steps forward to fund the relocation of this bridge, we understand that demolition of the bridge will occur. As a result, pursuant to the Indiana Historic Bridges PA, we request that this bridge be photographically documented prior to commencement of the project by a qualified professional historian, architectural historian, or architect. Please provide overall views of the bridge and representative photographs of its deck, abutments, piers, along with any additional character defining features".

The documentation shall be produced in keeping with the applicable photographic standards of the *Indiana DNR–Division of Historic Preservation and Archaeology Minimum Architectural Documentation*. One CD or DVD of the documentation shall be provided to the Indiana State Archives and one CD or DVD shall be provided to at least one local public or not-for-profit organization that agrees to retain the CD or DVD permanently and make it available to the public. These are firm commitments and are discussed in the *Environmental Commitments* section of this document. No other consulting parties have commented on the HBAA. Pursuant to the Programmatic Section 4(f) Evaluation and Approval for FHWA projects that necessitate the use of historic bridges, the preferred alternative, Alternative F, will result in a use of the historic bridge. The FHWA signature of this Level 4 Categorical Exclusion will act as FHWA concurrence of this Programmatic Section 4(f) evaluation for Jackson County Bridge 154.

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Section 6(f) Involvement

Presence

Use

Yes

No

Section 6(f) Property

Remarks: *Discuss proposed alternatives that satisfy the requirements of Section 6(f). Discuss any Section 6(f) involvement.*
 The U.S. Land and Water Conservation Fund Act of 1965 established the Land and Water Conservation Fund (LWCF), which was created to preserve, develop, and assure accessibility to outdoor recreation resources. Section 6(f) of this Act prohibits conversion of lands purchased with LWCF monies to a non-recreation use. A list of 6(f) properties obtained from the IDNR-Division of Outdoor Recreation via the INDOT Environmental Policy Office website (<https://www.in.gov/indot/2523.htm>) revealed a total of six properties in Jackson County (Appendix I, page I-1). None of these sites are located within or adjacent to the project area. Therefore, no use is expected.

SECTION E – Air Quality

Air Quality

Conformity Status of the Project

Is the project in an air quality non-attainment or maintenance area?

Yes

No

If YES, then:

Is the project in the most current MPO TIP?

Is the project exempt from conformity?

If the project is NOT exempt from conformity, then:

Is the project in the Transportation Plan (TP)?

Is a hot spot analysis required (CO/PM)?

Level of MSAT Analysis required?

Level 1a Level 1b Level 2 Level 3 Level 4 Level 5

Remarks: This project is included in the Fiscal Year (FY) 2020-2024 Statewide Transportation Improvement Program (STIP) (Appendix H, page H-1). Modifications to the STIP may be necessary depending on the outcome of the preferred alternative that is selected after the public hearing. Jackson County is not located within a recognized MPO region.

This project is located in Jackson County, which is currently a maintenance area for Ozone per the IDEM County Nonattainment List located at http://www.in.gov/idem/airquality/files/nonattainment_county_list.pdf. under the 1999 Ozone 8-hour standard, which was revoked in 2015 but is being evaluated for conformity due to the February 16, 2018, South Coast Air Quality Management District V. Environmental Protection Agency, Et. Al. Decision. The project’s design concept and scope are accurately reflected in the State Implementation Plan (SIP). Therefore, the conformity requirements of 40 CFR 93 have been met.

This project is of a type qualifying as a categorical exclusion (Group 1) under 23 CFR 771.117(c) or exempt under the Clean Air Act conformity rule under 40 CFR 93.126, and as such, a Mobile Source Air Toxics analysis is not required.

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| |
|--------------------------|
| SECTION F – NOISE |
|--------------------------|

Noise **Yes** **No**
 Is a noise analysis required in accordance with FHWA regulations and INDOT’s traffic noise policy?

| | No | Yes/ Date |
|------------------------------------|--------------------------|--------------------------|
| ES Review of Noise Analysis | <input type="checkbox"/> | <input type="checkbox"/> |

Remarks: This project is a Type III project. In accordance with 23 CFR 772 and the current *Indiana Department of Transportation Traffic Noise Analysis Procedure*, this action does not require a formal noise analysis.

| |
|--------------------------------------|
| SECTION G – COMMUNITY IMPACTS |
|--------------------------------------|

| | Yes | No |
|--|-------------------------------------|-------------------------------------|
| Regional, Community & Neighborhood Factors | | |
| Will the proposed action comply with the local/regional development patterns for the area? | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| Will the proposed action result in substantial impacts to community cohesion? | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| Will the proposed action result in substantial impacts to local tax base or property values? | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| Will construction activities impact community events (festivals, fairs, etc.)? | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| Does the community have an approved transition plan? | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| If No, are steps being made to advance the community’s transition plan? | <input type="checkbox"/> | <input type="checkbox"/> |
| Does the project comply with the transition plan? (explain in the remarks box) | <input checked="" type="checkbox"/> | <input type="checkbox"/> |

Remarks:

On May 19, 2020, Metric sent an early coordination packet to the U.S. Department of Housing and Urban Development (HUD) requesting comments regarding any possible environmental effects associated with this project (Appendix C, pages C-1 to C-2). HUD did not respond to the early coordination letter. No permanent impacts to community cohesion, the local tax base, or property values will result from the proposed project. Furthermore, there will be no impact to local mobility, access, pedestrian or motorist safety or emergency services as a result of the project.

On May 4, 2020, Metric conducted an on-line review of the Indiana Festivals website (<http://www.indianafestivals.org>). There were no events identified within or near the project area that would be impacted during the construction schedule.

The Americans with Disabilities Act (ADA) requires a transition plan by local and state governments. Such a plan includes how the government will remove barriers to accessibility over time for persons with disabilities, such as installing curb ramps at intersections, making a web site accessible for persons with low vision, ensuring public meetings are fully accessible to persons with disabilities and other related issues. Jackson County has an approved ADA transition plan, and the project is in compliance with that plan because it will not create barriers to accessibility.

Indirect and Cumulative Impacts **Yes** **No**
 Will the proposed action result in substantial indirect or cumulative impacts?

Remarks:

Indirect impacts are effects which are caused by the action and are later in time or farther removed in distance but are still reasonably foreseeable. Indirect effects may include growth inducing effects and other effects related to induced changes in the pattern of land use, population density, or growth rate. Cumulative impacts affect the environment which result from the incremental impact of the action when added to other past, present, and reasonably foreseeable future actions regardless of what agency or person undertakes such actions.

This project will not result in indirect or cumulative impacts as it will improve an existing intersection within a residential area and will not change the general development patterns, population density, or growth rate of the project area. The project will be a positive improvement for the area and will not result in any negative direct or cumulative impacts.

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Public Facilities & Services

Will the proposed action result in substantial impacts on health and educational facilities, public and private utilities, emergency services, religious institutions, airports, public transportation or pedestrian and bicycle facilities?

| Yes | No |
|--------------------------|-------------------------------------|
| <input type="checkbox"/> | <input checked="" type="checkbox"/> |

Remarks: *Discuss how the maintenance of traffic will affect public facilities and serves.*

Based on a desktop review, a site visit on May 27, 2020 by Metric Environmental, the 2016 aerial photograph of the project area (Appendix B, page B-3) and the RFI report (Appendix E, page E-7) there are no public facilities within the 0.5 mile search radius. There are no public facilities within or adjacent to the project area. Utility coordination has been conducted and because there are no public or private utilities in the project area, there are no anticipated utility conflicts. Access to all properties will be maintained during construction. Therefore, no impacts are expected.

There will be no substantial impacts on health and educational facilities, public and private utilities, emergency services, religious institutions, airports, public transportation or pedestrian and bicycle facilities. The early coordination letter was sent to the INDOT Office of Aviation on May 19, 2020, and no response was received.

There are no public or private airports in the project area. It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access.

Environmental Justice (EJ) (Presidential EO 12898)

During the development of the project were EJ issues identified?

| Yes | No |
|--------------------------|-------------------------------------|
| <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| <input type="checkbox"/> | <input checked="" type="checkbox"/> |

Does the project require an EJ analysis?

If YES, then:

Are any EJ populations located within the project area?

Will the project result in adversely high or disproportionate impacts to EJ populations?

| | |
|--------------------------|--------------------------|
| <input type="checkbox"/> | <input type="checkbox"/> |
| <input type="checkbox"/> | <input type="checkbox"/> |

Remarks: Under FHWA Order 6640.23A, FHWA and Jackson County, as a recipient of funding from FHWA, are responsible to ensure that their programs, policies, and activities do not have a disproportionately high and adverse effect on minority or low-income populations. Per the current INDOT Categorical Exclusion Manual, an Environmental Justice (EJ) Analysis is required for any project that has two or more relocations or 0.5 acre of new, additional permanent right-of-way. This project will have no relocations and will require only 0.47 acre of new, additional permanent right-of-way; therefore, an EJ analysis is not required.

Relocation of People, Businesses or Farms

Will the proposed action result in the relocation of people, businesses or farms?

Is a Business Information Survey (BIS) required?

Is a Conceptual Stage Relocation Study (CSRS) required?

Has utility relocation coordination been initiated for this project?

| Yes | No |
|--------------------------|-------------------------------------|
| <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| <input type="checkbox"/> | <input checked="" type="checkbox"/> |

Number of relocations: Residences: 0 Businesses: 0 Farms: 0 Other: 0

If a BIS or CSRS is required, discuss the results in the remarks box.

Remarks: No relocations or displacements of people, businesses, farms or other facilities will be required as a result of this project. Therefore, a Business Information Survey and Conceptual Stage Relocation Study will not be required. There will be no utility relocation as part of this project. There are no public utilities located in the project area.

SECTION H – HAZARDOUS MATERIALS & REGULATED SUBSTANCES

Hazardous Materials & Regulated Substances (Mark all that apply)

Red Flag Investigation

Phase I Environmental Site Assessment (Phase I ESA)

Phase II Environmental Site Assessment (Phase II ESA)

Design/Specifications for Remediation required?

Documentation

| |
|-------------------------------------|
| <input checked="" type="checkbox"/> |
| <input type="checkbox"/> |
| <input type="checkbox"/> |
| <input type="checkbox"/> |

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| | No | Yes/ Date |
|------------------------------------|--------------------------|---------------------|
| ES Review of Investigations | <input type="checkbox"/> | Yes / June 30, 2020 |

Include a summary of findings for each investigation.

Remarks: Based on a review of GIS and available public records, and a Red Flag Investigation (RFI) completed by Metric on June 30, 2020 (Appendix E, pages E-1 to E-12), no sites with hazardous material concerns (hazmat sites) or sites involved with regulated substances were identified in or within 0.5 mile of the project area. Further investigation for hazardous material concerns or regulated substances is not required at this time.

SECTION I – PERMITS CHECKLIST

| Permits (mark all that apply) | <u>Likely Required</u> |
|---|-------------------------------------|
| Army Corps of Engineers (404/Section10 Permit) | |
| Individual Permit (IP) | <input type="checkbox"/> |
| Nationwide Permit (NWP) | <input checked="" type="checkbox"/> |
| Regional General Permit (RGP) | <input type="checkbox"/> |
| Pre-Construction Notification (PCN) | <input type="checkbox"/> |
| Other | <input type="checkbox"/> |
| Wetland Mitigation required | <input type="checkbox"/> |
| Stream Mitigation required | <input type="checkbox"/> |
| IDEM | |
| Section 401 WQC | <input checked="" type="checkbox"/> |
| Isolated Wetlands determination | <input type="checkbox"/> |
| Rule 5 | <input type="checkbox"/> |
| Other | <input type="checkbox"/> |
| Wetland Mitigation required | <input type="checkbox"/> |
| Stream Mitigation required | <input type="checkbox"/> |
| IDNR | |
| Construction in a Floodway | <input checked="" type="checkbox"/> |
| Navigable Waterway Permit | <input type="checkbox"/> |
| Lake Preservation Permit | <input type="checkbox"/> |
| Other | <input type="checkbox"/> |
| Mitigation Required | <input type="checkbox"/> |
| US Coast Guard Section 9 Bridge Permit | <input type="checkbox"/> |
| Others (Please discuss in the remarks box below) | <input type="checkbox"/> |

Remarks: The project will not require a Rule 5 permit as less than 1.0 acre of land will be disturbed. An IDEM 401 Water Quality Certification Regional General Permit and an USACE 404 Nationwide waterway permit will be necessary due to the permanent impacts to Rider Ditch and to Wetland A. In addition, an IDNR Construction in a Floodway permit will be required.

Applicable recommendations provided by the Indiana Department of Natural Resources are included in the *Environmental Commitments* section of this CE document. If permits are found to be necessary, the conditions of the permit will be requirements of the project and will supersede these recommendations. It is the responsibility of the project sponsor to identify and obtain all required permits.

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SECTION J- ENVIRONMENTAL COMMITMENTS

The following information should be provided below: List all commitments, name of agency/organization requesting the commitment(s), and indicating which are firm and which are for further consideration. The commitments should be numbered.

Remarks:

Firm:

1. If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately. (INDOT ESD)
2. It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access. (INDOT ESD)
3. General AMM 1: Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs. (USFWS)
4. Lighting AMM 1: Direct temporary lighting away from suitable habitat during the active season. (USFWS)
5. Tree Removal AMM 1: Modify all phases/aspects of the project (e.g., temporary work areas, alignments) to the extent practicable to avoid tree removal in excess of what is required to implement the project safely. (USFWS)
6. Tree Removal AMM 2: Apply time of year restrictions (April 1 through September 30) for tree removal when bats are not likely to be present, or limit tree removal to 10 or fewer trees per project at any time of year within 100 feet of existing road/ rail surface and outside of documented roosting/foraging habitat or travel corridors; visual emergence survey must be conducted with no bats observed. (USFWS and IDNR-DFW)
7. Tree Removal AMM 3: Ensure tree removal is limited to that specified in project plans. Install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits. Ensure that contractors understand clearing limits and how they are marked in the field. (USFWS)
8. Tree Removal AMM 4: Do not remove **documented** Indiana Bat or NLEB roosts (that are still suitable for roosting) or trees within 0.25 mile of roosts or **documented** foraging habitat at any time of the year. (USFWS)
9. Pursuant to the Indiana Historic Bridges PA, this bridge must be photographically documented prior to commencement of the project by a qualified professional historian, architectural historian, or architect. Provide overall views of the bridge and representative photographs of its deck, abutments, piers, along with any additional character defining features. The documentation shall be produced in keeping with the applicable photographic standards of the *Indiana DNR-Division of Historic Preservation and Archaeology Minimum Architectural Documentation*. One CD or DVD of the documentation shall be provided to the Indiana State Archives and one CD or DVD shall be provided to at least one local public or not-for-profit organization that agrees to retain the CD or DVD permanently and make it available to the public. (IDNR-SHPO)
10. The Little Spectaclecase (*Villosa lianosa*), a state mussel species of special concern, has been documented in Rider Ditch within the project area. The IDNR recommends standard erosion control measures be implemented, and in-channel disturbance minimized as much as possible. (IDNR-DFW)
11. Additional fencing and "Do Not Disturb" signs will be installed along the construction limits in the southeast quadrant to avoid unnecessary or additional impacts to Wetland A. This wetland area will also be marked as "Do not Disturb" on the plan sheets with instructions to the contractor to adhere to the established construction limits and avoid any activities beyond those limits. (INDOT ESD)

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Remarks:

For Further Consideration:

12. Do not work in the waterway from April 1 through June 30 without the prior written approval of the Division of Fish and Wildlife. (IDNR)
13. Do not excavate in the low flow area except for the placement of piers, foundations, and riprap, or removal of the old structure. (IDNR)
14. Do not construct any temporary runarounds, access bridges, causeways, cofferdams, diversions, or pump-arounds. (IDNR-DFW)
15. Use minimum average 6 inch graded riprap stone extended below the normal water level to provide habitat for aquatic organisms in the voids. (IDNR)
16. Impacts to non-wetland forest of one (1) acre or more should be mitigated at a minimum 2:1 ratio. If less than one acre of non-wetland forest is removed in a rural setting, replacement should be at a 1:1 ratio based on area. Impacts to non-wetland forest under one (1) acre in an urban setting should be mitigated by planting five trees, at least 2 inches in diameter-at-breast height (dbh), for each tree which is removed that is 10 inches dbh or greater (5:1 mitigation based on the number of large trees). (IDNR)
17. Riprap or other hard bank stabilization materials should only be used at the toe of slopes up to the ordinary high water mark (OHWM) with the exception of areas directly under bridges. The banks above the OHWM must be restored, stabilized, and revegetated using geotextiles and a mixture of grasses, sedges, wildflowers, shrubs, and trees native to Central Indiana and specifically for stream bank/floodway stabilization purposes as soon as possible upon completion. (IDNR)
18. The new, replacement, or rehabbed structure should not create conditions that are less favorable for wildlife passage under the structure compared to the current conditions. (IDNR)
19. Do not clear trees or understory vegetation outside the construction zone boundaries. (**This restriction is not related to the "tree clearing" restriction for potential Indiana Bat habitat.**) (USFWS)
20. Restrict below low-water work in streams to placement of culverts, piers, pilings and/or footings, shaping of the spill slopes around the bridge abutments, and placement of riprap. (USFWS)
21. Restrict channel work and vegetation clearing to the minimum necessary for installation of the stream crossing structure. (USFWS)
22. Minimize the extent of hard armor (riprap) in bank stabilization by using bioengineering techniques whenever possible. If riprap is utilized for bank stabilization, extend it below low-water elevation to provide aquatic habitat. (USFWS)
23. Avoid all work within the inundated part of the stream channel (in perennial streams and larger intermittent streams) during the fish spawning season (April 1 through June 30), except for work within sealed structures such as caissons or cofferdams that were installed prior to the spawning season. No equipment shall be operated below Ordinary High Water Mark during this time unless the machinery is within the caissons or on the cofferdams. (USFWS)
24. Evaluate wildlife crossings under bridge/culverts projects in appropriate situations. Suitable crossings include flat areas below bridge abutments with suitable ground cover, high water shelves in culverts, amphibian tunnels and diversion fencing. (USFWS)

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| |
|--------------------------------------|
| SECTION K- EARLY COORDINATION |
|--------------------------------------|

Please list the date coordination was sent and all agencies that were contacted as a part of the development of this Environmental Study. Also, include the date of their response or indicate that no response was received. INDOT and FHWA are automatically considered early coordination participants and should only be listed if a response is received.

Remarks:

| Resource Agencies | Date Sent | Date Response |
|--|--------------|---------------|
| National Park Service | May 19, 2020 | None Received |
| Federal Highway Administration | May 19, 2020 | None Received |
| INDOT Office of Public Hearings | May 19, 2020 | None Received |
| INDOT Seymour District Office | May 19, 2020 | None Received |
| USFWS IPaC Coordination | May 19, 2020 | July 6, 2020 |
| USFWS Bloomington Field Office | May 19, 2020 | June 10, 2020 |
| NRCS | May 19, 2020 | June 4, 2020 |
| USACE, Louisville District | May 19, 2020 | None Received |
| IGS | May 19, 2020 | May 19, 2020 |
| IDEM, Proposed Roadway Construction Projects | May 19, 2020 | Automatic |
| IDNR, DFW | May 19, 2020 | June 18, 2020 |
| INDOT, Office of Aviation | May 19, 2020 | None Received |
| U.S. Dpt. of Housing and Urban Development | May 19, 2020 | None Received |
| Jackson County Commissioners | May 19, 2020 | None Received |
| Jackson County Floodway Administrator | May 19, 2020 | None Received |
| Jackson County Emergency Management | May 19, 2020 | None Received |
| Jackson County Highway Department | May 19, 2020 | None Received |
| Jackson County Surveyor | May 19, 2020 | None Received |
| Hoosier National Forest | May 19, 2020 | May 19, 2020 |

APPENDICES

APPENDIX A: INDOT Supporting Documentation

- CE Threshold Chart A-1

APPENDIX B: Graphics

- Project Location Map B-1
- USGS Topographic Map B-2
- Aerial Photograph B-3
- Ground Level Photographs B-4
- Project Design Plans B-7

APPENDIX C: Early Coordination

- Sample Early Coordination Letter C-1
- Early Coordination Recipients List C-3
- IDNR-DFW Response C-4
- USFWS Concurrence Verification C-7
- USFWS Official Species List C-22
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- IDEM Proposed Roadway Construction Projects Letter C-33
- Hoosier National Forest Service C-39
- USFWS, Bloomington Field Office C-40

APPENDIX D: Section 106 of the National Historic Preservation Act

- No Historic Properties Effected Finding D-1
- Area of Potential Effect Map D-8
- Historic Properties Report Excerpts D-18
- Archaeological Short Report Excerpts D-21
- Early Coordination Letters/Emails to Consulting Parties D-24
- Miami Tribe of Oklahoma Response D-30
- SHPO Response to Early Coordination D-31
- SHPO Approval of HPR and Archaeological Report D-40
- Bridge Marketing Website Notice D-42
- Bridge Marketing Legal Notice/Publishers Claim D-44
- SHPO Approval of Effect Determination Finding D-50
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- Legal Notice of Effect Determination D-56
- HBAA Addendum D-57
- SHPO Approval of HBAA Addendum D-179

APPENDIX E: Red Flag and Hazardous Materials

- Red Flag Investigation, INDOT Approval E-4
- Red Flag Maps E-6
- IDNR List of Endangered, Threatened and Rare Species E-10

APPENDIX F: Water Resources

- Waters Determination Report F-1
- Exhibit 4 - NWI Wetland Inventory Map F-13
- Exhibit 5 - Waters Delineation Map F-14
- Site Photographs F-16
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APPENDIX G: Public Involvement

- Example Notice of Survey Letter G-1
- Property Owner List G-2
- Bridge Marketing Notice G-3
- Publishers Claim G-4

APPENDIX H: Air Quality

- FY 2020-2024 INDOT STIP Project List H-1

APPENDIX I:

- LWCF Listing for Jackson County I-1

APPENDIX A
INDOT Supporting Documentation

Categorical Exclusion Level Thresholds

| | PCE | Level 1 | Level 2 | Level 3 | Level 4 ¹ |
|--|--|--|-------------------------------------|------------------------------|--|
| Section 106 | Falls within guidelines of Minor Projects PA | “No Historic Properties Affected” | “No Adverse Effect” | - | “Adverse Effect” Or Historic Bridge involvement ² |
| Stream Impacts | No construction in waterways or water bodies | < 300 linear feet of stream impacts | ≥ 300 linear feet of stream impacts | - | Individual 404 Permit |
| Wetland Impacts | No adverse impacts to wetlands | < 0.1 acre | - | < 1 acre | ≥ 1 acre |
| Right-of-way³ | Property acquisition for preservation only or none | < 0.5 acre | ≥ 0.5 acre | - | - |
| Relocations | None | - | - | < 5 | ≥ 5 |
| Threatened/Endangered Species (Species Specific Programmatic for Indiana bat & northern long eared bat) | “No Effect”, “Not likely to Adversely Affect” (Without AMMs ⁴ or with AMMs required for all projects ⁵) | “Not likely to Adversely Affect” (With any other AMMs) | - | “Likely to Adversely Affect” | Project does not fall under Species Specific Programmatic |
| Threatened/Endangered Species (Any other species) | Falls within guidelines of USFWS 2013 Interim Policy | “No Effect”, “Not likely to Adversely Affect” | - | - | “Likely to Adversely Affect” |
| Environmental Justice | No disproportionately high and adverse impacts | - | - | - | Potential ⁶ |
| Sole Source Aquifer | Detailed Assessment Not Required | - | - | - | Detailed Assessment |
| Floodplain | No Substantial Impacts | - | - | - | Substantial Impacts |
| Coastal Zone Consistency | Consistent | - | - | - | Not Consistent |
| National Wild and Scenic River | Not Present | - | - | - | Present |
| New Alignment | None | - | - | - | Any |
| Section 4(f) Impacts | None | - | - | - | Any |
| Section 6(f) Impacts | None | - | - | - | Any |
| Added Through Lane | None | - | - | - | Any |
| Permanent Traffic Alteration | None | - | - | - | Any |
| Coast Guard Permit | None | - | - | - | Any |
| Noise Analysis Required | No | - | - | - | Yes |
| Air Quality Analysis Required | No | - | - | - | Yes ⁷ |
| Approval Level | Concurrence by INDOT District Environmental or Environmental Services | Yes | Yes | Yes | Yes |
| <ul style="list-style-type: none"> • District Env. Supervisor • Env. Services Division • FHWA | | | | Yes | Yes |

¹Coordinate with INDOT Environmental Services. INDOT will then coordinate with the appropriate FHWA Environmental Specialist.

²Any involvement with a bridge processed under the Historic Bridge Programmatic Agreement.

³Permanent and/or temporary right-of-way.

⁴AMMs = Avoidance and Mitigation Measures.

⁵AMMs determined by the IPAC decision key to be needed that are listed in the USFWS *User's Guide for the Range-wide Programmatic Consultation for Indiana bat and Northern long-eared bat* as “required for all projects”.

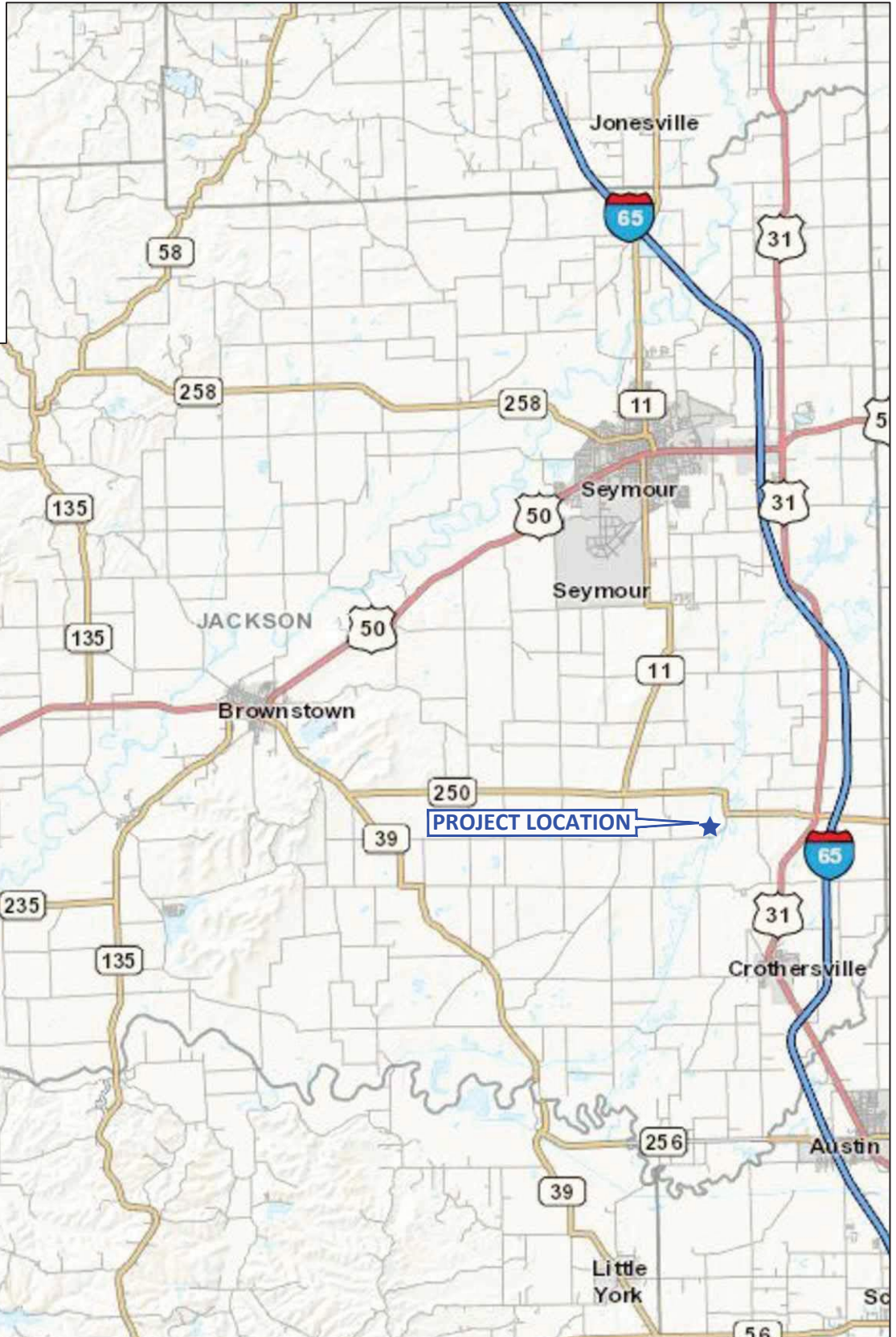
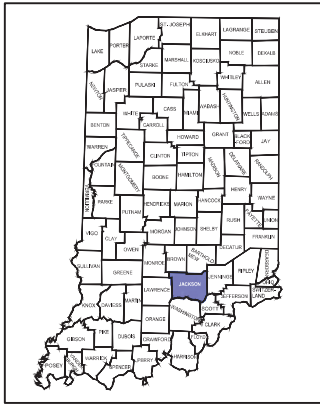
⁶Potential for causing a disproportionately high and adverse impact.

⁷Hot Spot Analysis and/or MSAT Quantitative Emission Analysis.

*Substantial public or agency controversy may require a higher-level NEPA document.



APPENDIX B

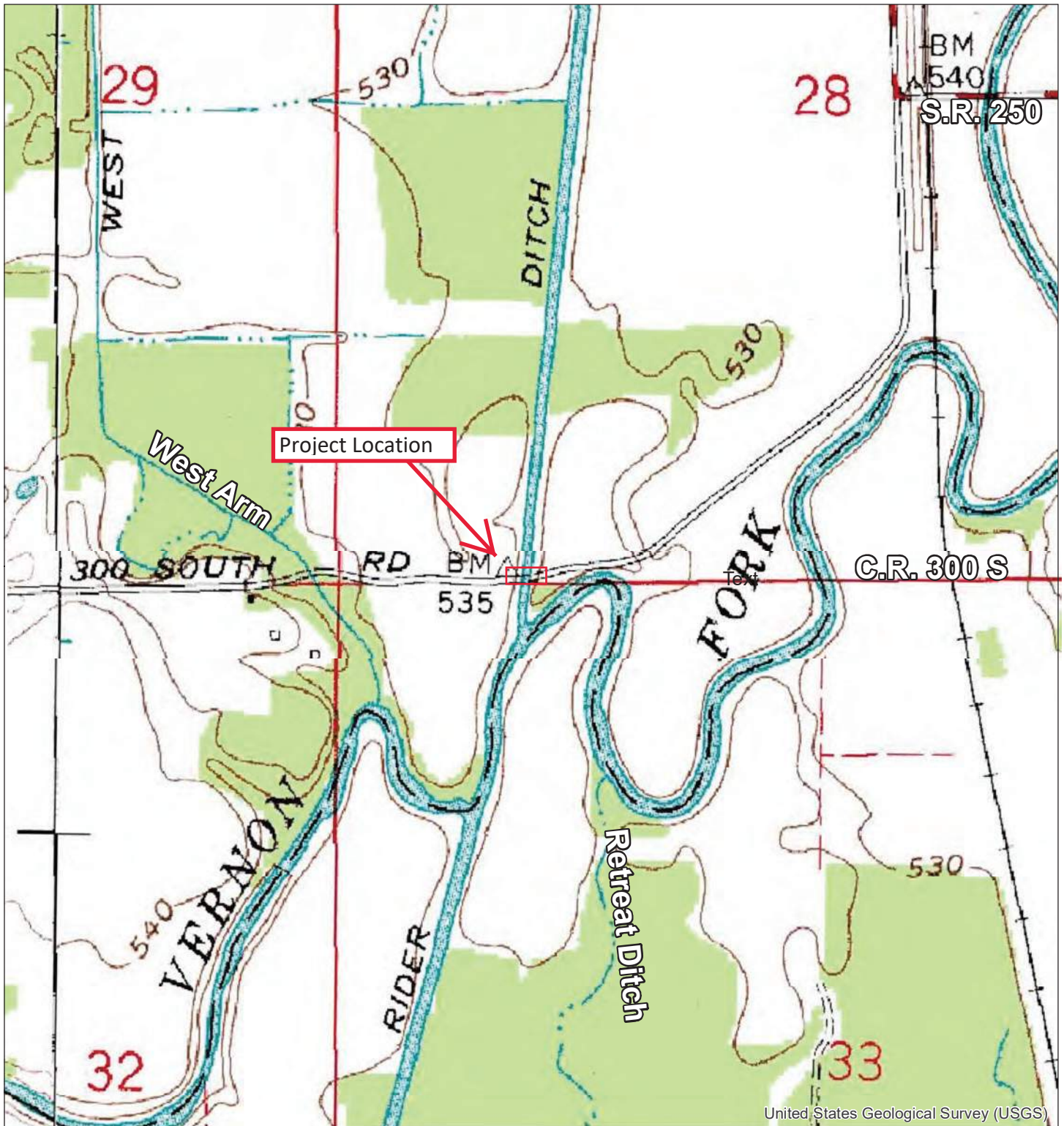
Project Maps and Graphics



P:\2019\19-0011 - SE-Jackson County - Bridge 154 Replacement CR 300S\5 - Deliverables\Exhibits\EC\Location Map.dwg

Source: <http://maps.indiana.edu/>

| | | |
|--|---|---|
| <p>Project Location Map</p> <p>Bridge Project Des. No. 1703020 East CR 300 South over Rider Ditch Washington Township, Jackson County, Indiana</p> | <p>All locations approximate</p> <div style="text-align: center;">  Not to Scale </div> |  |
|--|---|---|

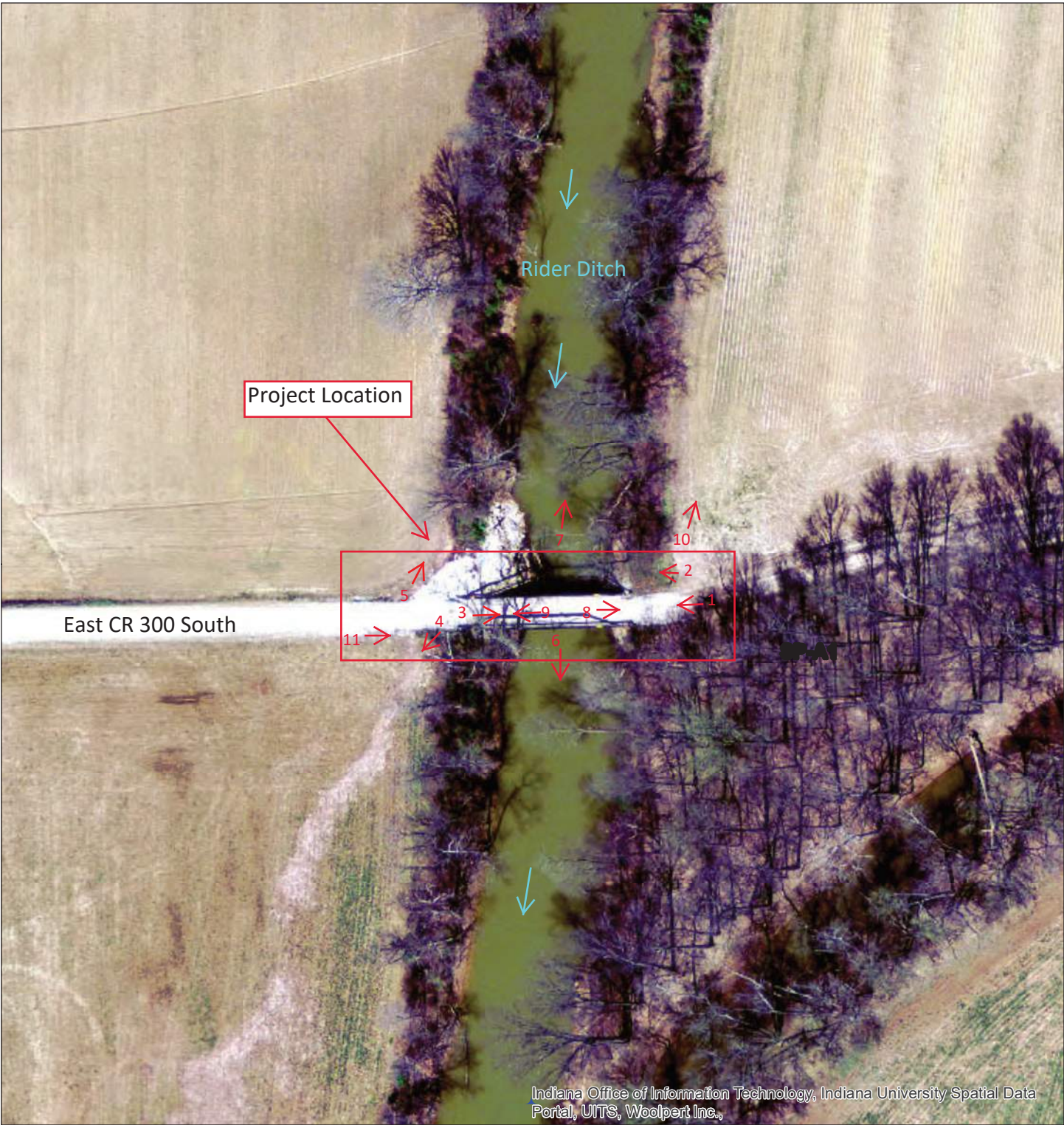


USGS Topographic Map

Bridge Project
 Des. No. 1703020
 East CR 300 South Over Rider Ditch
 Washington Township, Jackson County, IN

All locations approximate
 Source: Indiana Spatial Data Portal (1996)





Indiana Office of Information Technology, Indiana University Spatial Data Portal, UITS, Woolpert Inc.,

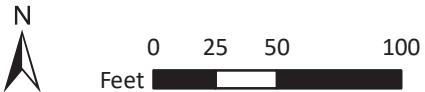
Photo Orientation Direction



2016 Aerial Photograph

Bridge Project
 Des. No.1703020
 East CR 300 South over Rider Ditch
 Washington Township, Jackson County, IN

All locations approximate
 Source: Indiana Spatial Data Portal (2016)





1. View looking west at Jackson County Bridge No. 154



2. View of the northeast quadrant of the Jackson County Bridge No. 154 crossing



3. View looking east at Jackson County Bridge No. 154



4. View looking south at the southwest quadrant of the Jackson County Bridge No. 154 crossing

SITE PHOTOGRAPHS 5/27/2020

Bridge Project

Des. No. 1703020

CR 300 South over Rider Ditch

Washington Township, Jackson County, Indiana





5. View of the northwest quadrant of the Jackson County Bridge No. 154 crossing



6. View of Rider Ditch looking south (downstream)



7. View of Rider Ditch looking north (upstream)



8. View of east embankment of Jackson County Bridge No. 154

SITE PHOTOGRAPHS 5/27/2020

Bridge Project

Des. No. 1703020

CR 300 South over Rider Ditch

Washington Township, Jackson County, Indiana





9. View of west embankment of Jackson County Bridge No. 154



10. View looking northeast at farm field located beyond the stream corridor



11. INDOT Public Notice for Historic Bridge Marketing

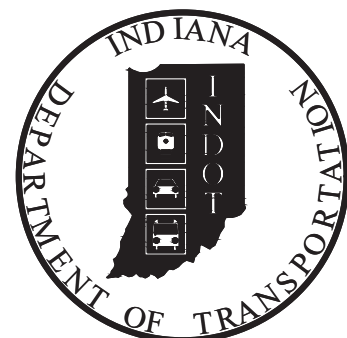
SITE PHOTOGRAPHS 5/27/2020

Bridge Project
Des. No. 1703020
CR 300 South over Rider Ditch
Washington Township, Jackson County, Indiana



| | |
|---------------------|------------------------|
| PROJECT 1703020 | DESIGNATION 1703020 |
| CONTRACT B-40895 | BRIDGE FILE |

INDIANA DEPARTMENT OF TRANSPORTATION



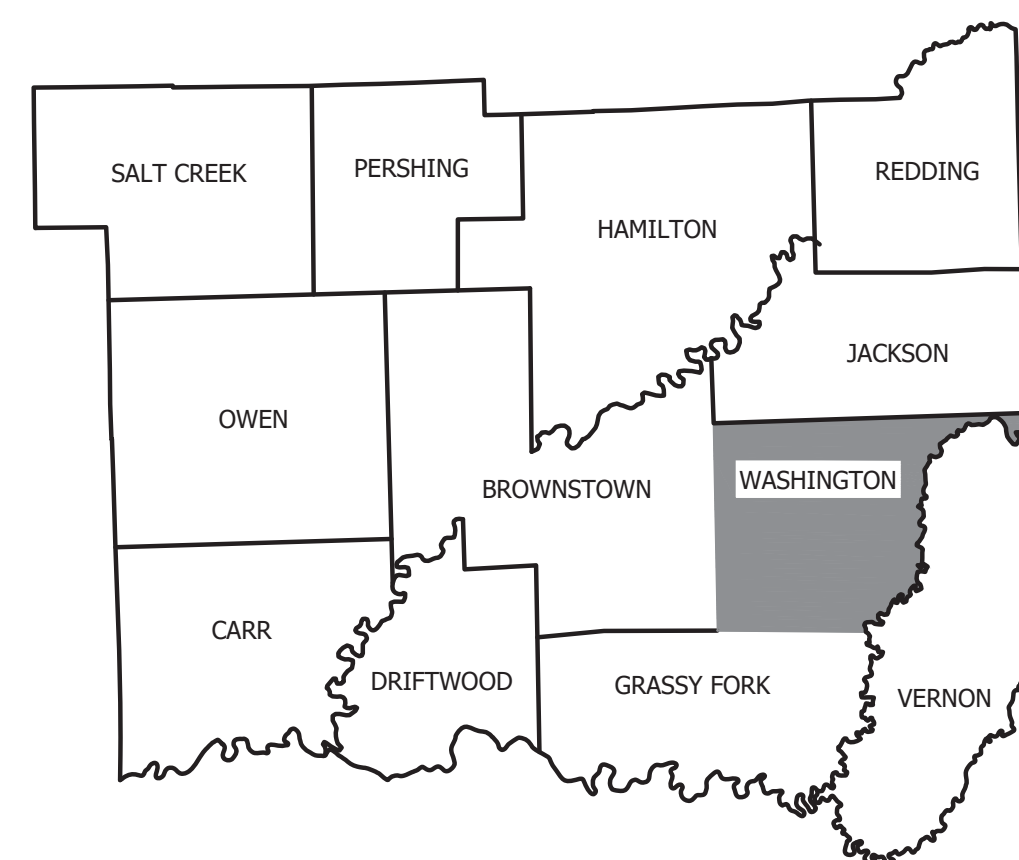
| | | |
|---------------------------|--|------------------|
| TRAFFIC DATA | | E CR300 S |
| A.A.D.T. (2020) | | 30 V.P.D. |
| A.A.D.T. (2040) | | 40 V.P.D. |
| D.H.V. () | | V.P.H. |
| DIRECTIONAL DISTRIBUTION | | 50% - 50% |
| TRUCKS | | 5.00% A.A.D.T. |
| DESIGN DATA | | |
| DESIGN SPEED | | 30 M.P.H. |
| PROJECT DESIGN CRITERIA | | (3R) NON-FREEWAY |
| FUNCTIONAL CLASSIFICATION | | LOCAL RURAL |
| RURAL/URBAN | | RURAL |
| TERRAIN | | LEVEL |
| ACCESS CONTROL | | NONE |

| STRUCTURE | DESIGNATION | TYPE | SPAN AND SKEW | OVER | STATION |
|---------------------------|-------------|---|--|-------------|-------------------|
| Jackson County Bridge 154 | 1703020 | Continuous Haunched Reinforced Concrete Slab Bridge | 3 Spans, 30'-0", 50'-0" & 30'-0" 5° Skew | Rider Ditch | 7+60.7Line "PR-A" |

BRIDGE PLANS

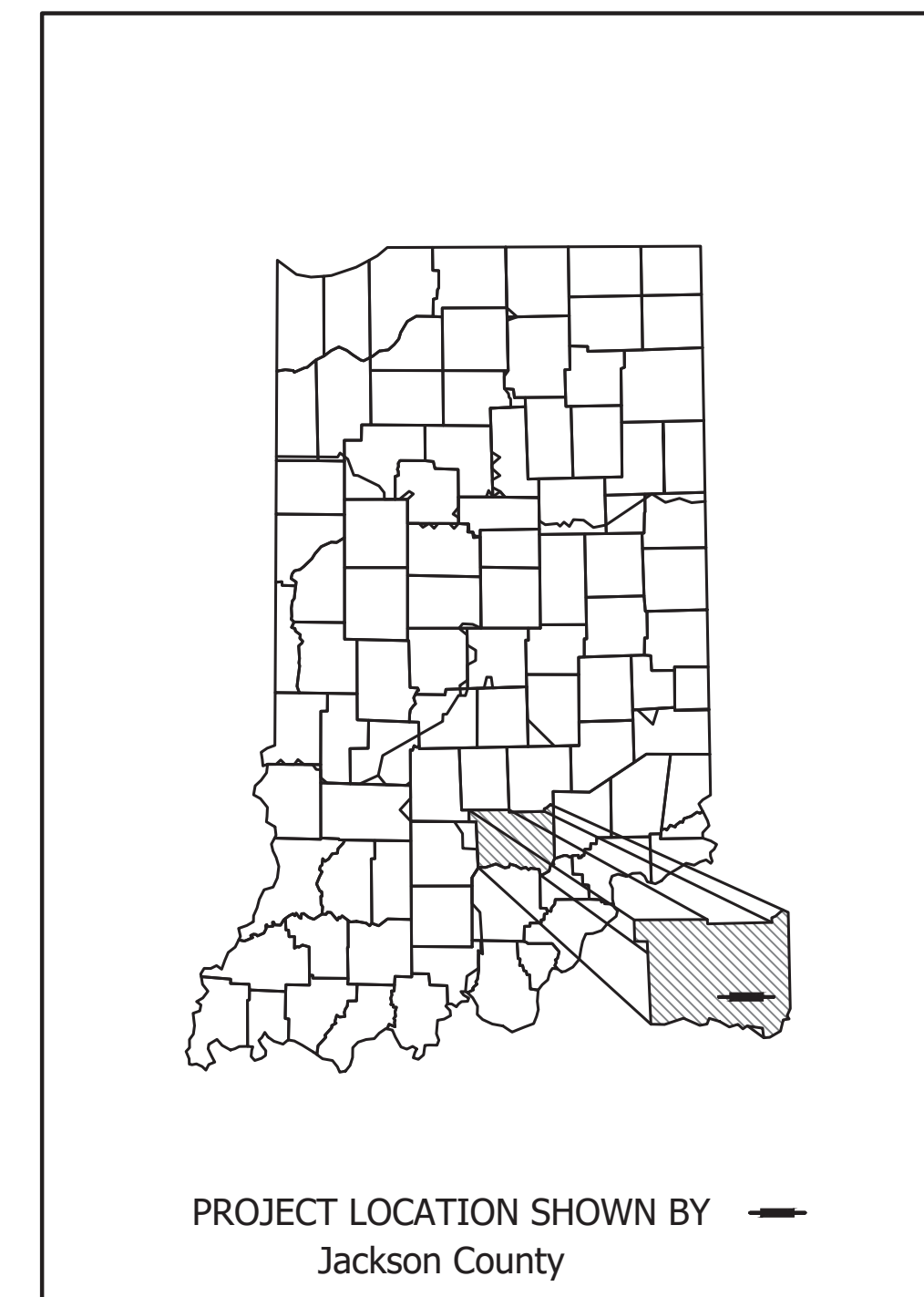
FOR SPANS OVER 20 FEET

ROUTE: E COUNTY ROAD 300 S
PROJECT NO. 1703020 P.E.
1703020 R/W
1703020 CONST.



TOWNSHIP MAP

Bridge Replacement of Structure No. 154 over Rider Ditch, located along County Road 300S and is located 0.8 miles E of CR 840 E in Section 28, R-6-E, T-5-N, Washington Township, Jackson County, Indiana



PROJECT LOCATION SHOWN BY Jackson County

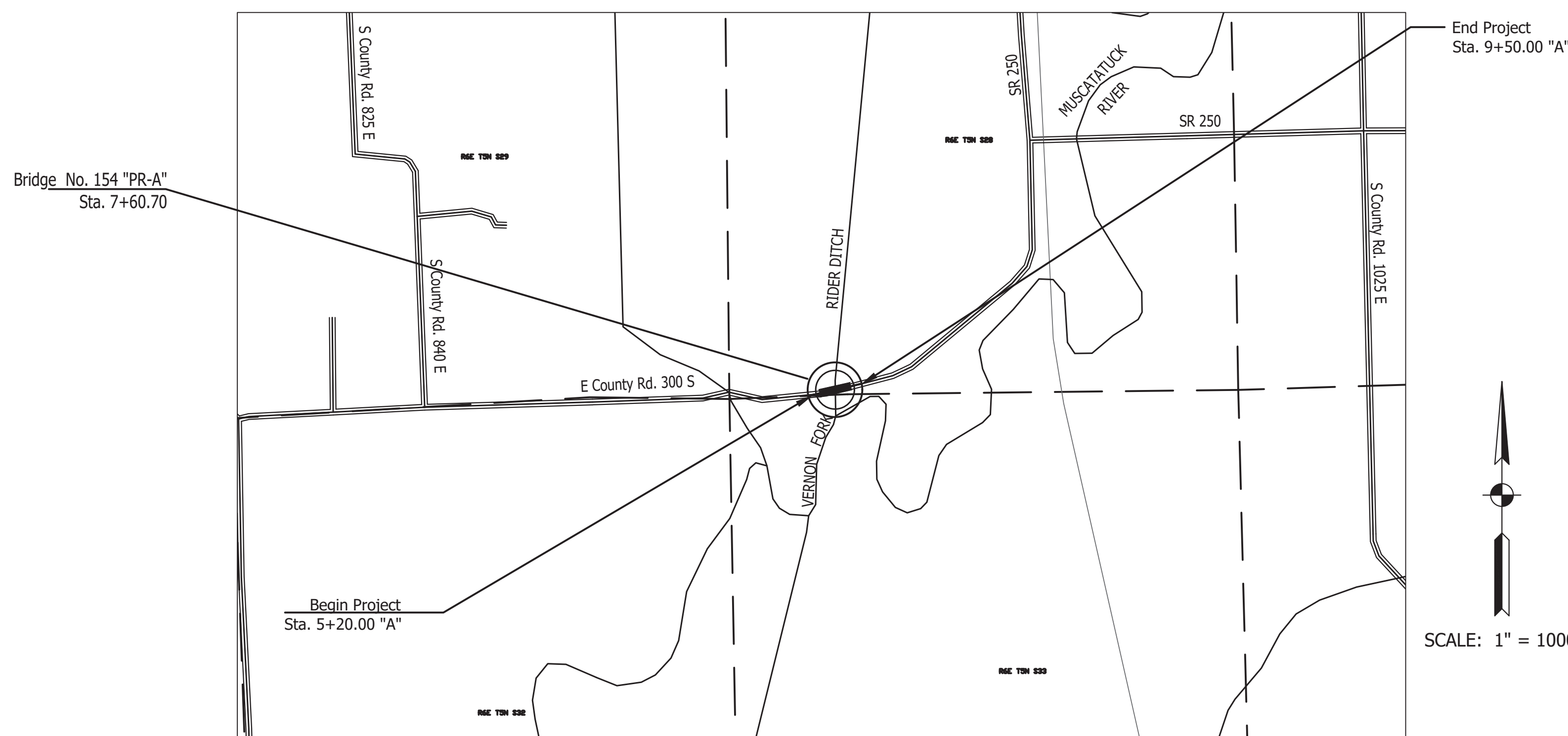
APPROVED:
JACKSON COUNTY BOARD OF COMMISSIONERS
DATE: _____

MATT REEDY
PRESIDENT

DREW MARKET
MEMBER

BOB GILLASPY
MEMBER

KATHY HOENSTREITER JACKSON COUNTY AUDITOR



LOCATION MAP
JACKSON COUNTY

LATITUDE: 38°50'14.35"N LONGITUDE: 85°51'58.07"W

BRIDGE LENGTH: 0.021 MI.
ROADWAY LENGTH: 0.060 MI.
TOTAL LENGTH: 0.081 MI.
MAX. GRADE: 3.87 %

Jackson Bridge 154 current plans
08-10-2021

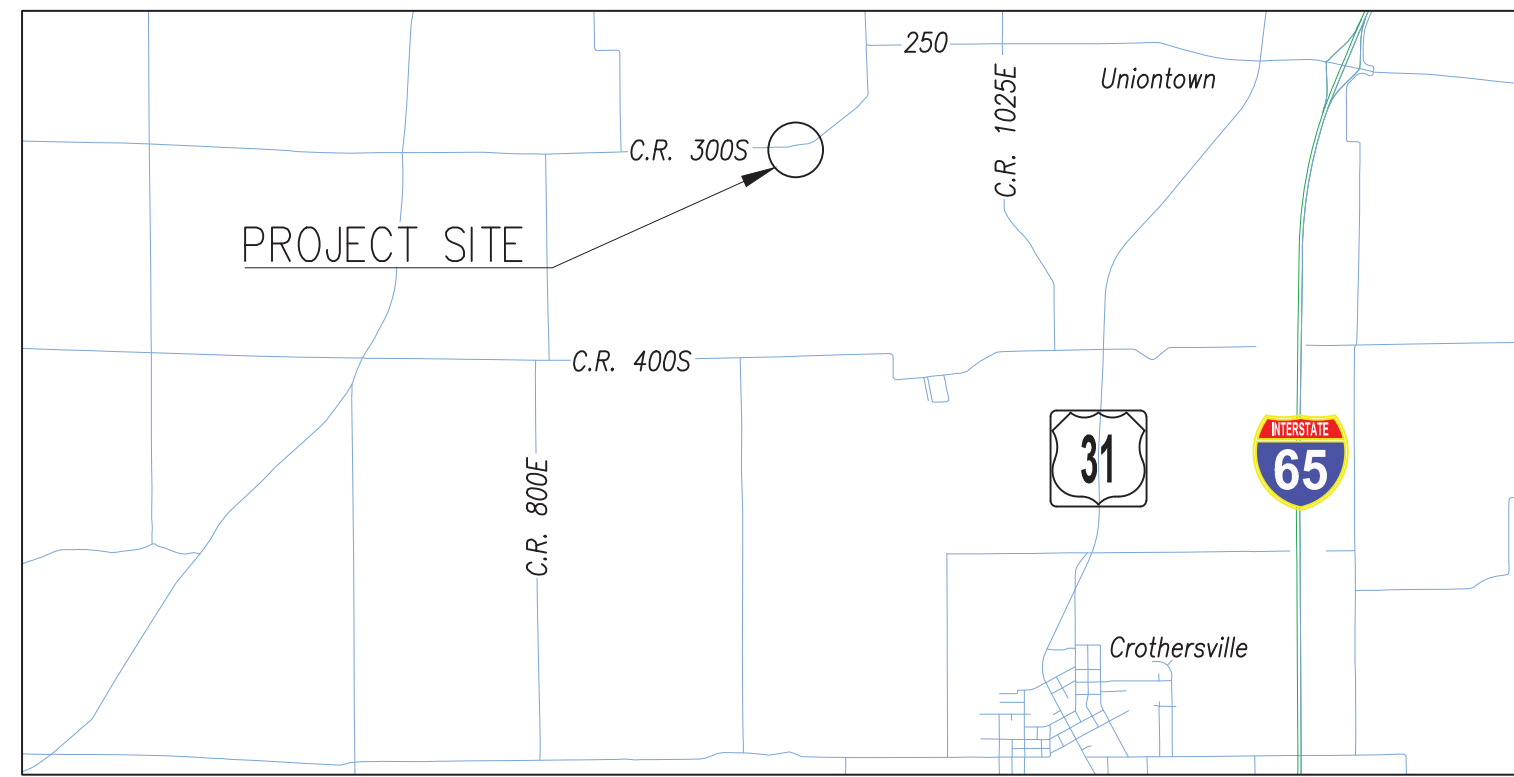
INDIANA DEPARTMENT OF TRANSPORTATION
STANDARD SPECIFICATIONS DATED 2020
TO BE USED WITH THESE PLANS

JSE
JANSSEN & SPAANS ENGINEERING
9120 Harrison Park Court Indianapolis, Indiana 46216 (317) 254-9686 Fax (317) 259-8262
5921 Stratton Circle Columbus, Indiana 47203 (812) 372-6573 Fax (812) 372-7054

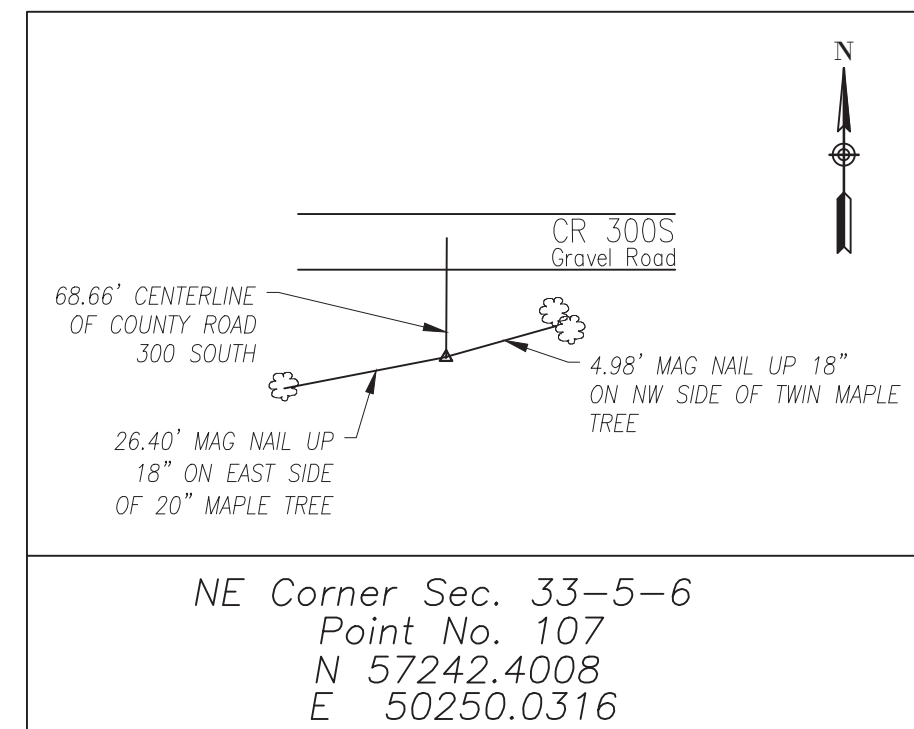
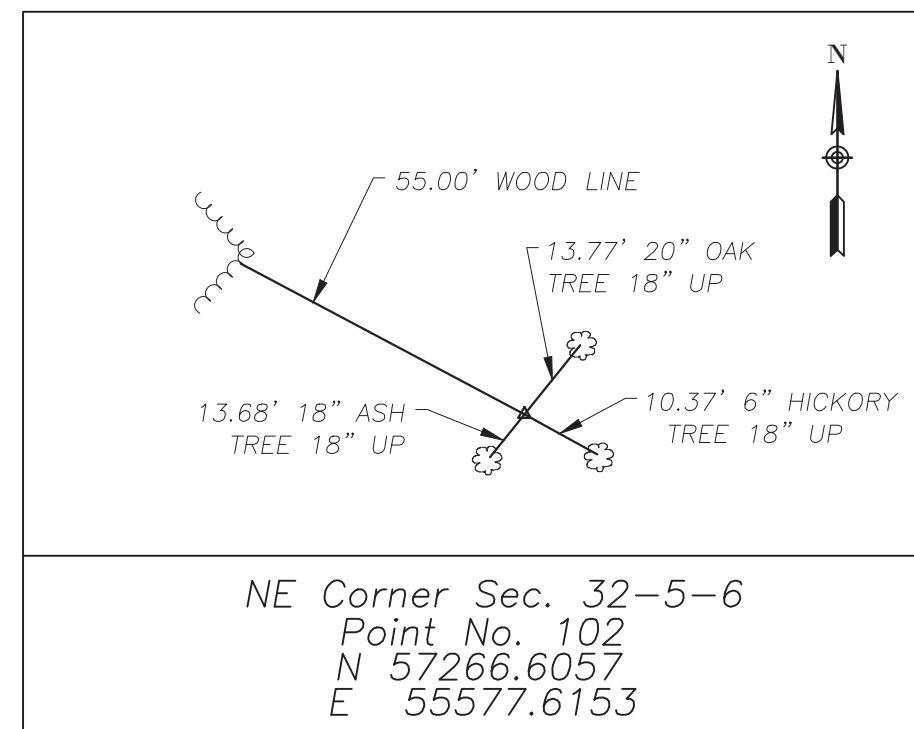
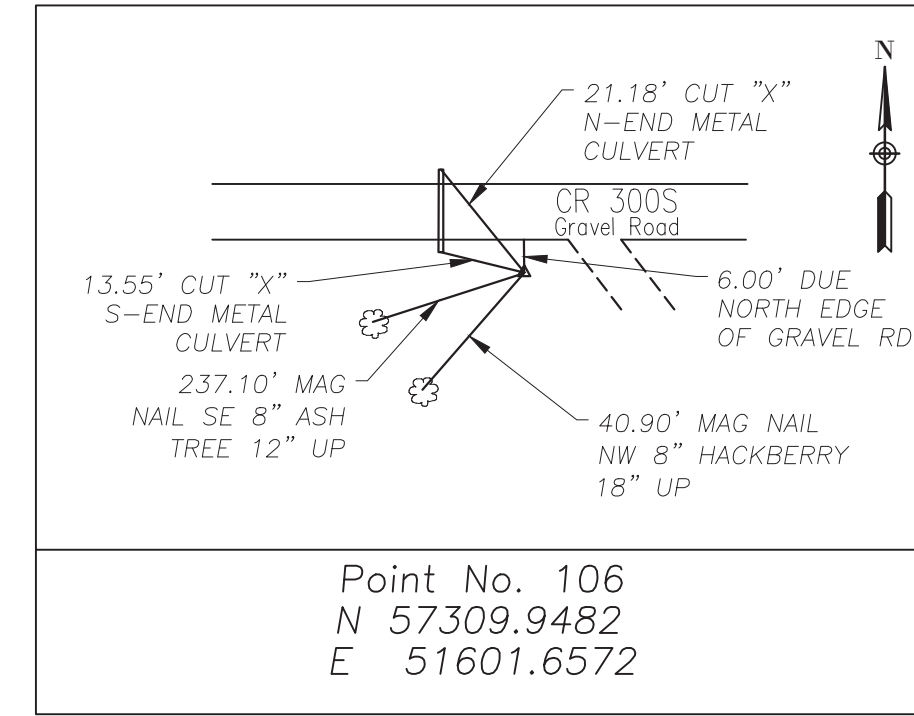
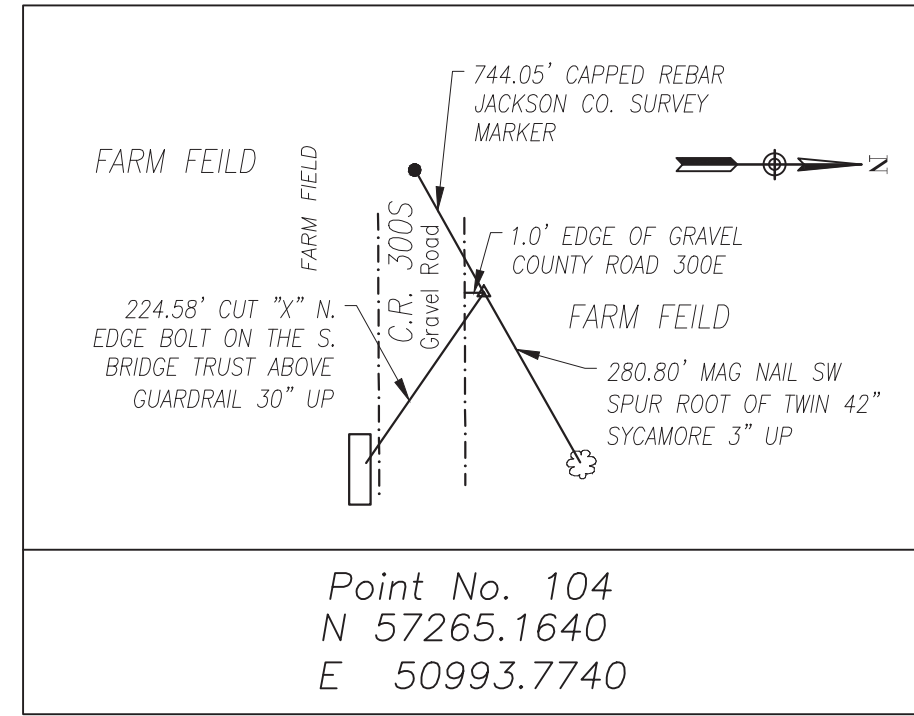
PLANS PREPARED BY: JANSSEN & SPAANS ENGINEERING, INC. (317) 254-9686 PHONE NUMBER
CERTIFIED BY: _____ DATE
APPROVED FOR LETTING: _____ DATE
INDIANA DEPARTMENT OF TRANSPORTATION

| | |
|-------------|-----------------|
| BRIDGE FILE | |
| DESIGNATION | 1703020 |
| SURVEY BOOK | 1 of 13 |
| CONTRACT | B-40895 |
| SHEETS | PROJECT 1703020 |

VICINITY MAP
N.T.S.



| Point Number | Northing | Easting | Monument Description | Found/Set/Calculated |
|--------------|------------|------------|---|----------------------|
| 500 | 57255.9097 | 51007.8335 | No monument fnd or set | Calculated |
| 501 | 57264.5433 | 51320.6063 | No monument fnd or set | Calculated |
| 502 | 57307.8155 | 51501.5797 | No monument fnd or set | Calculated |
| 503 | 57313.5976 | 51537.3002 | No monument fnd or set | Calculated |
| 102 | 57266.6057 | 55577.6153 | 5/8" Rebar w/ Aluminum Cap Stamped "Jackson Co Survey Marker" | Found |
| 104 | 57265.1640 | 50993.7740 | 5/8" Rebar w/ Cap Stamped Ryan D Perry LS 21500015" | Set |
| 105 | 57264.0349 | 51265.4925 | 5/8" Rebar w/ Cap Stamped Ryan D Perry LS 21500015" | Set |
| 107 | 57242.4008 | 50250.0316 | 5/8" Rebar w/ Aluminum Cap Stamped "Jackson Co Survey Marker" | Found |



LEGEND

- ▲ SECTION CORNER MONUMENT (Type as Noted)
- CAPPED REBAR SET

Route Survey
Project: County Road 300 South over Rider Ditch, Jackson County
Date: May 6, 2020

- General:
- All monument and reference ties are shown on the route survey plat.
 - Should additional USPLS corners be needed where none were found, they should be re-established and tied to this survey.
 - Point numbers shown are actual field survey point numbers used for control and topography.
 - The bearing system for this survey is based on Grid North per the Indiana State Plane East Zone.
 - The coordinates for this survey are on a local (ground) coordinate system derived from GPS observations utilizing the INCORS Network (see coordinate system below for additional information).
 - All dimensions shown on this Location Control Route Survey are in U.S. Survey Feet, unless otherwise noted.

A Location Control Route Survey for the Jackson County Board of Commissioners for the design of a bridge rehabilitation project for County Road 300 South over Rider Ditch. The project is located on County Road 300 South from approximately 300 feet east to 300 feet west of Rider Ditch in Jackson County, Indiana. The project lies within Sections 28 and 33 all in Township 5 North, Range 6 East in Jackson County, Indiana.

This data collection survey is for the purpose of designing a roadway improvement project. It is not a property retracement survey, where apparent property lines, corners, subdivision or section corner information is shown, it is based on physical evidence or testimony and minimal deed analysis was done. All right of way shown is apparent existing right of way and is shown based on plat information. The location of the right of way lines shown are subject to change if additional information is uncovered in the right of way engineering process.

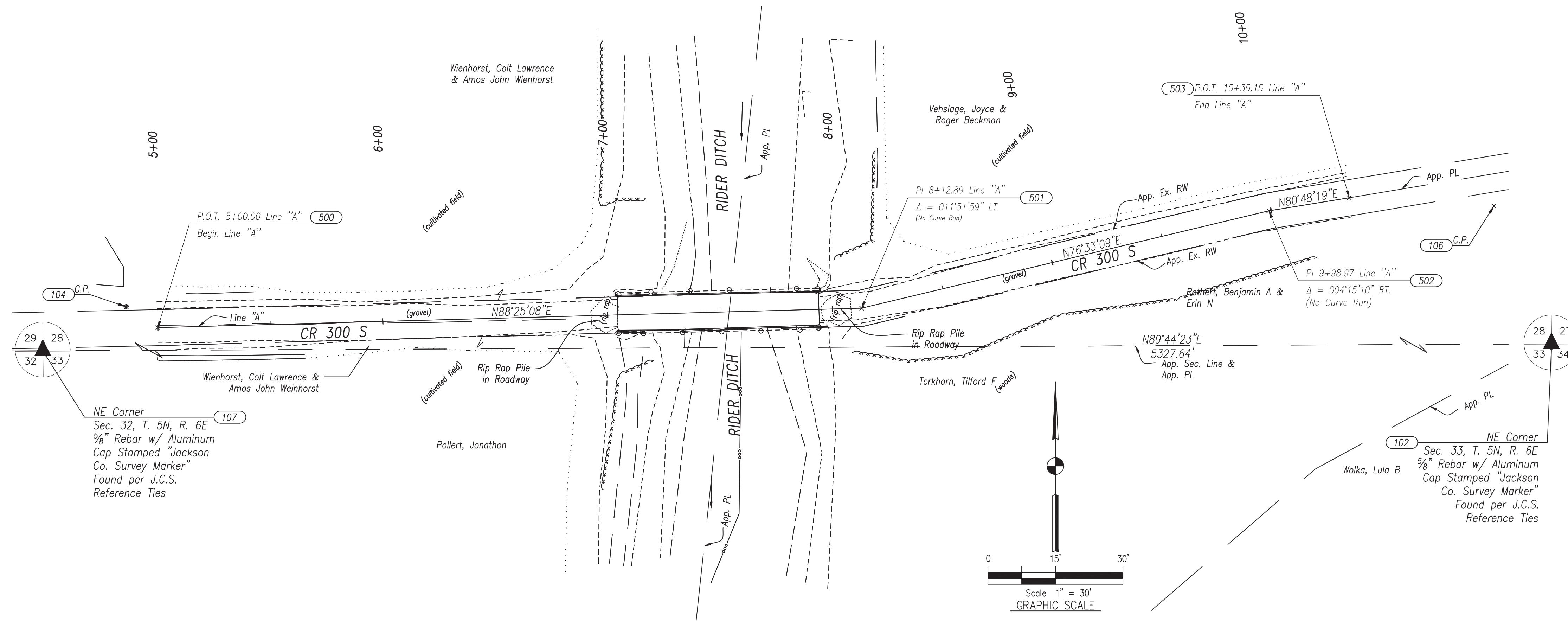
LINE "A"
A search was made for plans in the area of the project; however, none were found. Therefore, Line "A" was established as a best fit of the existing gravel roadway and stationing for Line "A" was assumed.

Coordinate System
The coordinate system for this project is based on a local coordinate system derived from the Indiana State Plane Coordinate System East Zone. The local coordinate system was derived by applying a scale factor of 0.999950327505 at point 104 (N = 1307265.2157, E = 270993.8950) and then subtracting 1250000.0000 from the northing and subtracting 220000.0000 from the Easting and processed with Leica Infinity using Geoid 12B and the NAD 1983 datum.

USPLS Corners and Monuments Found
The corners listed below have been accepted as prima facie evidence. If new evidence is discovered or monuments are found where none were found previously the location and uncertainties of the corners listed below would be subject to change. Any section corners not referenced by the Jackson County Surveyor are considered apparent section corners.

- 102 - Northeast Corner of Section 33, Township 5 North, Range 6 East; 5/8" Rebar with aluminum cap stamped "Jackson Co Survey Marker" found 2" above grade per reference ties obtained from the Office of the Jackson County Surveyor. No uncertainty is estimated for this monument.
- 107 - Northeast Corner of Section 32, Township 5 North, Range 6 East; 5/8" Rebar with aluminum cap stamped "Jackson Co Survey Marker" found 2" below grade per reference ties obtained from the Office of the Jackson County Surveyor. No uncertainty is estimated for this monument.

- Survey Control Monuments
- 104 - 5/8" Rebar with cap stamped "Ryan D. Perry LS21500015" set flush. This is an original monument set this survey; therefore, there is no uncertainty associated with this point.
 - 106 - 5/8" Rebar with cap stamped "Ryan D. Perry LS 21500015" set flush. This is an original monument set this survey; therefore, there is no uncertainty associated with this point.



NOTE: ALL PROPERTY LINES ARE APPARENT PROPERTY LINES AND WERE DRAWN ACCORDING TO THE RECORD PROPERTY DESCRIPTIONS WITH MINIMAL DEED ANALYSIS.

NOTE: ALL R/W SHOWN IS APPARENT EXISTING R/W AND IS DRAWN BASED ON PLANS FOR PREVIOUS PROJECTS AND/OR PHYSICAL EVIDENCE FOUND IN THE FIELD.

| | |
|--------------------------------|------|
| SURVEY STARTED | |
| 04/13/2020 | |
| SURVEY COMPLETED | |
| 04/14/2020 | |
| LOCATION CONTROL SURVEY SHEETS | |
| 1 | of 1 |
| DRAWN BY: JAM/MAH 10/9/2020 | |
| CHECKED: RDP 10/9/2020 | |

PREPARED BY:
Ryan D. Perry

I affirm, under penalties for perjury, that I have taken reasonable care to redact each Social Security number in this document, unless required by law.
Ryan D. Perry

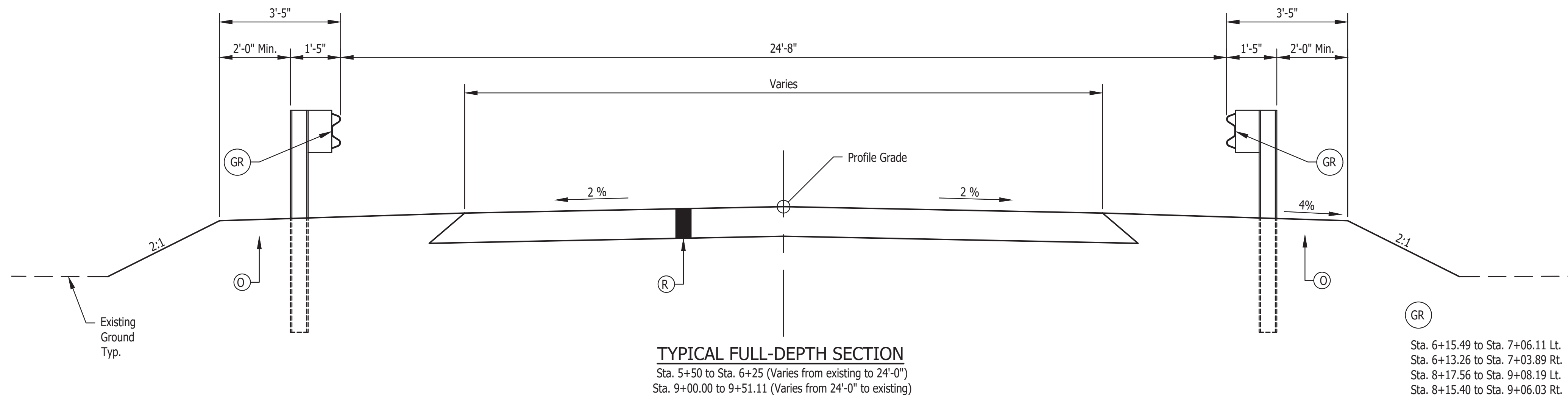
FIELD SURVEYOR STATEMENT
THIS SURVEY, TO THE BEST OF MY KNOWLEDGE AND BELIEF, IS EXECUTED ACCORDING TO THE PROVISIONS OF 865 I.A.C. 1-12-25 REGARDING ROUTE SURVEYS, EXCEPT THAT ANY DATA SHOWN REGARDING THE LOCATION OR DESCRIPTION OF ANY NEW PARCELS TO BE ACQUIRED OR THE EXISTING PARCELS IS NOT A PART OF THIS SURVEY.

RECOMMENDED FOR APPROVAL
[Signature]
October 15, 2020
REGISTERED LAND SURVEYOR, FIELD DATE

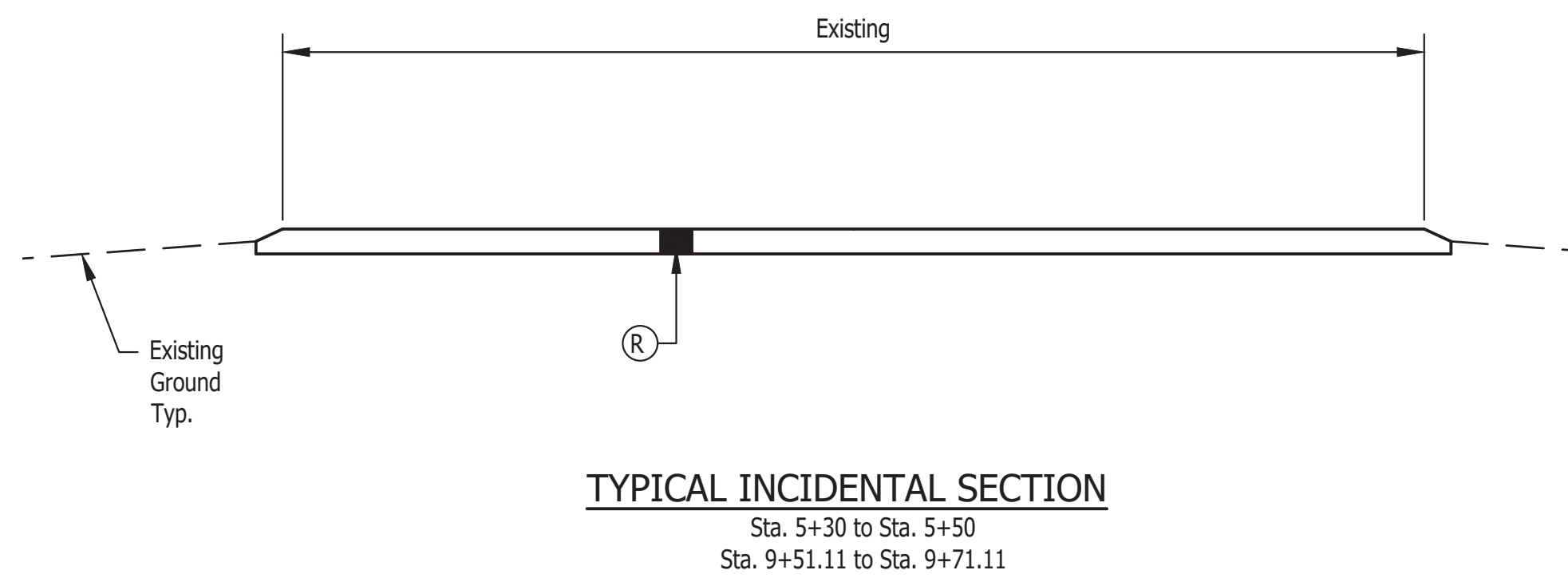
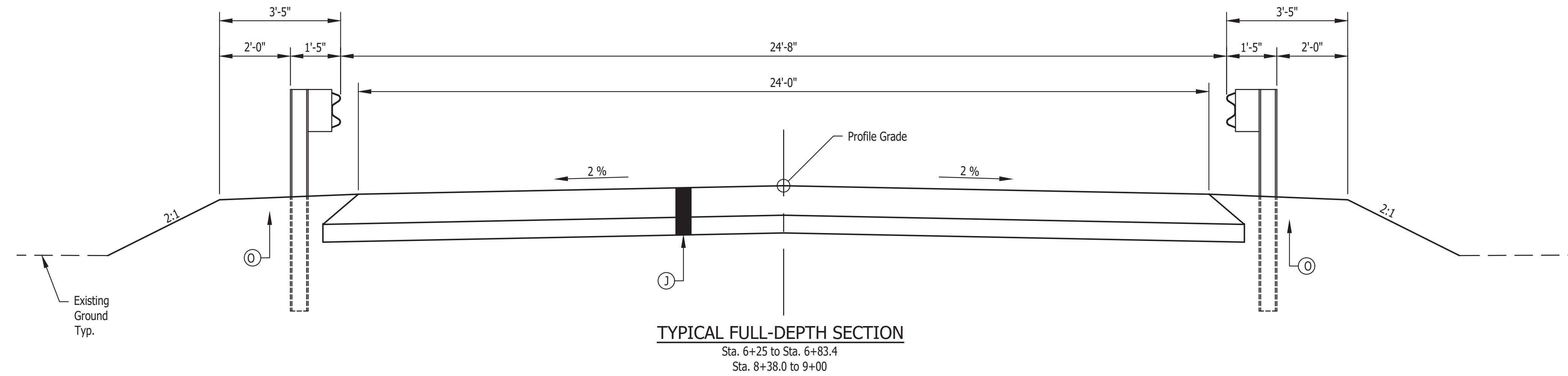
INDIANA DEPARTMENT OF TRANSPORTATION

County Road 300S over Rider Ditch
LOCATION CONTROL ROUTE SURVEY
JACKSON COUNTY, INDIANA

| | |
|------------------|-------------|
| HORIZONTAL SCALE | BRIDGE FILE |
| 1" = 30' | |
| ROUTE | DESIGNATION |
| CR 300E | 1703020 |
| SURVEY BOOK | PLAN SHEETS |
| ELECTRONIC | 1 of 1 |
| PROJ. NO. | COUNTY |
| | JACKSON |



Sta. 6+15.49 to Sta. 7+06.11 Lt.
 Sta. 6+13.26 to Sta. 7+03.89 Rt.
 Sta. 8+17.56 to Sta. 9+08.19 Lt.
 Sta. 8+15.40 to Sta. 9+06.03 Rt.



NOTES:
 For variable depth resurface sections, areas where pavement depth is greater than 4" in thickness shall be compacted in 3" to 6" lifts.

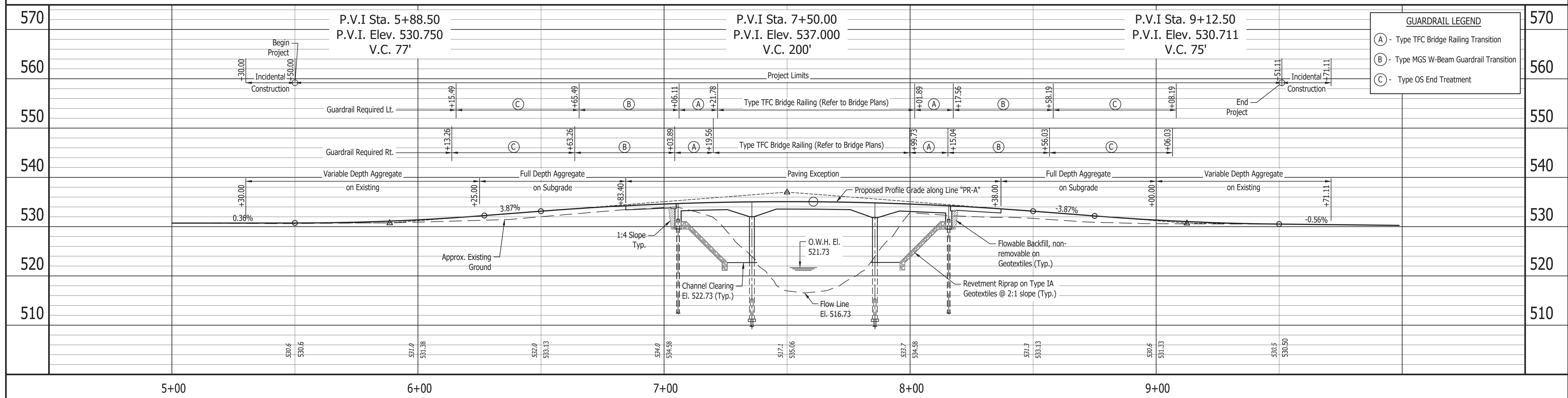
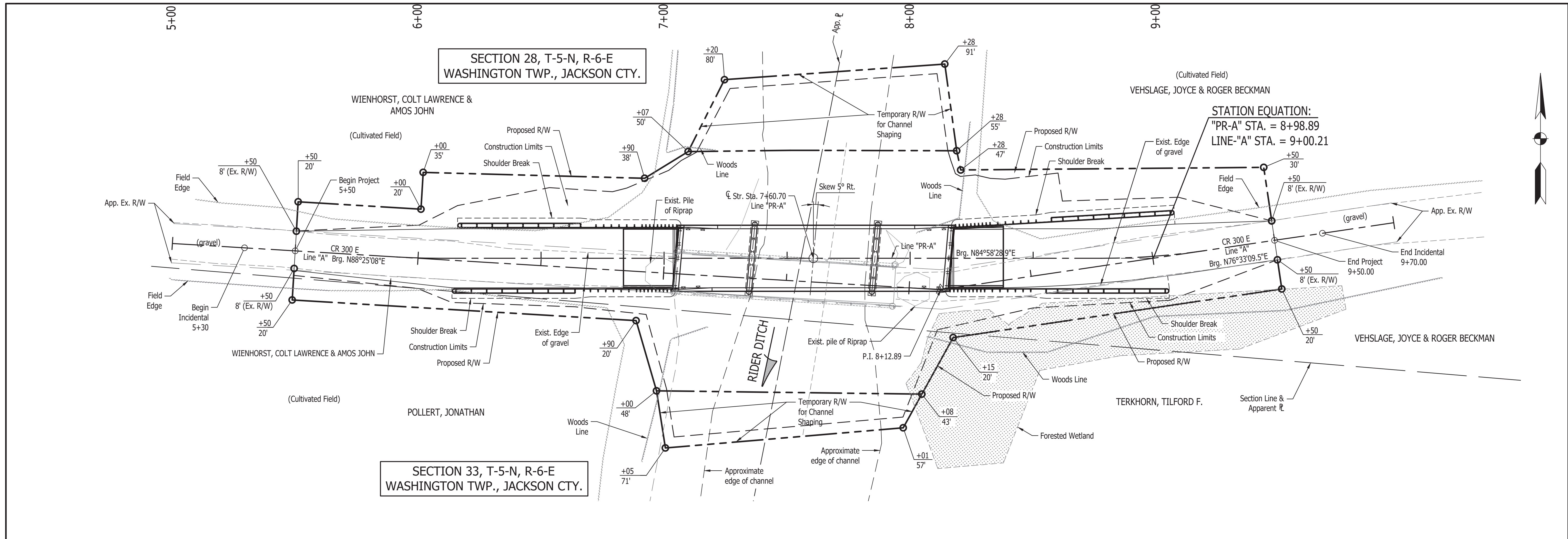
PRINT DATE: 10/19/20 PLOT SCALE: 1:1 EDIT DATE: 9/10/18 - 11:47 AM EDITED BY: THUTTON DRAWING FILE: P:\1286 - JACKSON CO BRIDGE 154 DESIGN\DWG\BRIDGE\05-TYP SECT.DWG

| LEGEND | |
|--------|--|
| Ⓜ | Full Depth Aggregate Pavement 10 in. Min. (placed in 3 in. to 6 in. lifts) Compacted Aggregate, No. 53 on Subgrade Treatment, Type II, (6 in. of Compacted Aggregate, No. 53) |
| Ⓡ | Resurface Variable Depth Compacted Aggregate, No. 53 |
| Ⓞ | Borrow |

| | | |
|--------------------------|-----------------|------|
| RECOMMENDED FOR APPROVAL | DESIGN ENGINEER | DATE |
| DESIGNED: GTC | DRAWN: KRO | |
| CHECKED: KDE | CHECKED: KDE | |

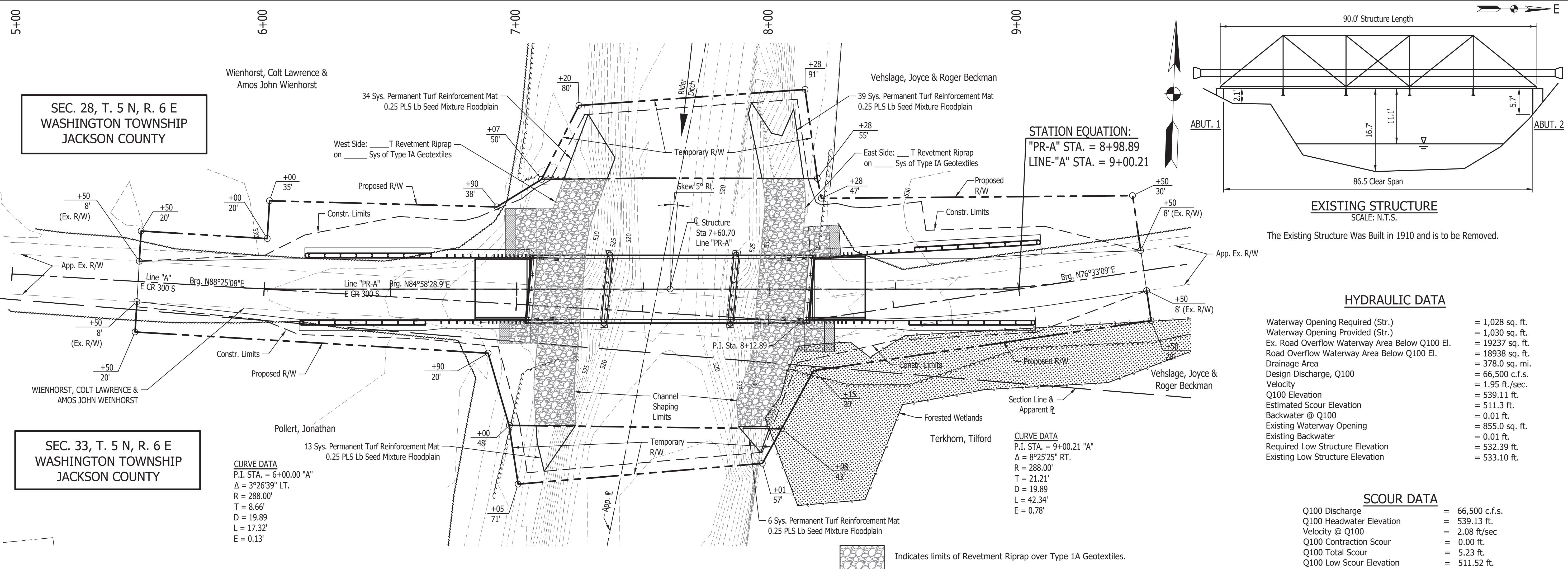
| | |
|---|--|
| INDIANA DEPARTMENT OF TRANSPORTATION | |
| TYPICAL SECTIONS | |

| | |
|--------------------------------|------------------------|
| HORIZONTAL SCALE 1" = 1'-0" | BRIDGE FILE - |
| VERTICAL SCALE N/A | DESIGNATION 1703020 |
| SURVEY BOOK | SHEETS |
| | 5 of 15 |
| CONTRACT B-40895 | PROJECT 1703020 |



| | | | | | |
|--|---|---|--|--|--|
| <p>XX Cor. XX X/X Sec. XX, T. XXX, R. XX Harrison Mon. Fnd.</p> <p>C.R.F.</p> <p>TBM 2 JCS Capped Rebar Elev. 528.98</p> | <p>C.P. 106</p> <p>C.R.F.</p> <p>TBM 3 Rebar w/ Cap "Ryan D. Perry LS21500015" Elev. 530.01</p> | | | | |
| <p>RECOMMENDED FOR APPROVAL: _____ DATE _____</p> <p>DESIGNED: GTC DRAWN: ACF</p> <p>CHECKED: KDE CHECKED: KDE</p> | | <p>INDIANA DEPARTMENT OF TRANSPORTATION</p> <p>PLAN & PROFILE LINE "PR-A"</p> | | <p>HORIZONTAL SCALE 1"=20'</p> <p>VERTICAL SCALE 1"=10'</p> <p>SURVEY BOOK _____ SHEET 6 of 15</p> <p>CONTRACT B-40895 PROJECT 1703020</p> | <p>FILE _____</p> <p>DESIGNATION 1703020</p> |

Last Saved By: Thorton
 Date: 12/18/2020
 File Name: 1703020
 Drawing Title: 1703020 - Jackson Co Bridge 154 Design(DWG)BRIDGE0906-PP.dwg



| | | | | | | | |
|-------|--|---|--|-----------------|-----------------|-----------------|-----------------|
| 550 | P.V.I Sta. 5+88.50 P.V.I. Elev. 530.750 V.C. 77' | P.V.I Sta. 7+50.00 P.V.I. Elev. 537.000 V.C. 200' | P.V.I Sta. 9+12.50 P.V.I. Elev. 530.711 V.C. 75' | 550 | | | |
| 540 | 111.51' (Structure Limits) | | | | | | |
| 530 | Existing Ground | | | | | | |
| 520 | Q100 El. 539.11 | | | | | | |
| 510 | Low Str. El. 532.39 | | | | | | |
| 500 | Proposed Grade along Line "PR-A" | | | | | | |
| | Flowable Backfill, non-removable on Geotextiles (Typ.) | | | | | | |
| | Revetment Riprap on Type IA Geotextiles @ 2:1 slope perpendicular to skew (Typ.) | | | | | | |
| | Channel Clearing El. 522.73 (Typ.) | | | | | | |
| | Flow Line El. 516.73 | | | | | | |
| | O.W.H. El. 521.73 | | | | | | |
| | 1:4 Slope Typ. | | | | | | |
| | Channel Shaping Limits | | | | | | |
| | Temporary R/W | | | | | | |
| | App. Ex. R/W | | | | | | |
| 530.7 | 530.6 530.60 | 531.0 531.38 | 532.0 533.13 | 532.8 534.58 | 531.6 533.13 | 530.6 531.33 | 530.5 530.50 |
| 5+00 | 6+00 | 7+00 | 8+00 | 9+00 | | | |

| | | | | | | | | | | | | | | | | | | | |
|--|---|---|--|------------------|------|--------|---|----------------|-------------|--------|---------|-------------|-------|----------|---------|---------|---------|--|---------|
| <p>XX Cor. XX X/X Sec. XX, T. XXX, R. XX Harrison Mon. Fnd.</p> <p>C.P. 106</p> <p>TBM 2 JCS Capped Rebar Elev. 528.98</p> <p>TBM 3 Rebar w/ Cap "Ryan D. Perry LS21500015" Elev. 530.01</p> | <p>RECOMMENDED FOR APPROVAL: _____ 9/17/2020</p> <p>DESIGNED: ZGC DRAWN: TLH</p> <p>CHECKED: KDE CHECKED: KDE</p> | <p>INDIANA DEPARTMENT OF TRANSPORTATION</p> <p>LAYOUT SHEET LINE "PR-A"</p> | <table border="1"> <tr><td>HORIZONTAL SCALE</td><td>FILE</td></tr> <tr><td>1"=20'</td><td>-</td></tr> <tr><td>VERTICAL SCALE</td><td>DESIGNATION</td></tr> <tr><td>1"=10'</td><td>1703020</td></tr> <tr><td>SURVEY BOOK</td><td>SHEET</td></tr> <tr><td>CONTRACT</td><td>7 of 15</td></tr> <tr><td>B-40895</td><td>PROJECT</td></tr> <tr><td></td><td>1703020</td></tr> </table> | HORIZONTAL SCALE | FILE | 1"=20' | - | VERTICAL SCALE | DESIGNATION | 1"=10' | 1703020 | SURVEY BOOK | SHEET | CONTRACT | 7 of 15 | B-40895 | PROJECT | | 1703020 |
| HORIZONTAL SCALE | FILE | | | | | | | | | | | | | | | | | | |
| 1"=20' | - | | | | | | | | | | | | | | | | | | |
| VERTICAL SCALE | DESIGNATION | | | | | | | | | | | | | | | | | | |
| 1"=10' | 1703020 | | | | | | | | | | | | | | | | | | |
| SURVEY BOOK | SHEET | | | | | | | | | | | | | | | | | | |
| CONTRACT | 7 of 15 | | | | | | | | | | | | | | | | | | |
| B-40895 | PROJECT | | | | | | | | | | | | | | | | | | |
| | 1703020 | | | | | | | | | | | | | | | | | | |

Last Saved By: T.Hutton
 Date: 12/18/2020
 Plot Scale: 1:1
 Drawing File: P:\1286 - Jackson Co Bridge 154 Design\DWG\BRIDGE\07-LAYOUT.dwg

GENERAL NOTES

There are no plans for the current existing structure.
 Reinforcing steel covering shall be 2 1/2 inches in top and 1" minimum in bottom of floor slab, and 2" in all other parts, unless noted.

DESIGN STRESSES

CONCRETE
 Class C $f_c = 4000$ psi
 Class A $f_c = 3500$ psi

REINFORCING STEEL
 Grade 60 $f_y = 60,000$ psi

STRUCTURAL STEEL FOR PILES
 ASTM A272, Grade 50 $f_y = 50$ ksi

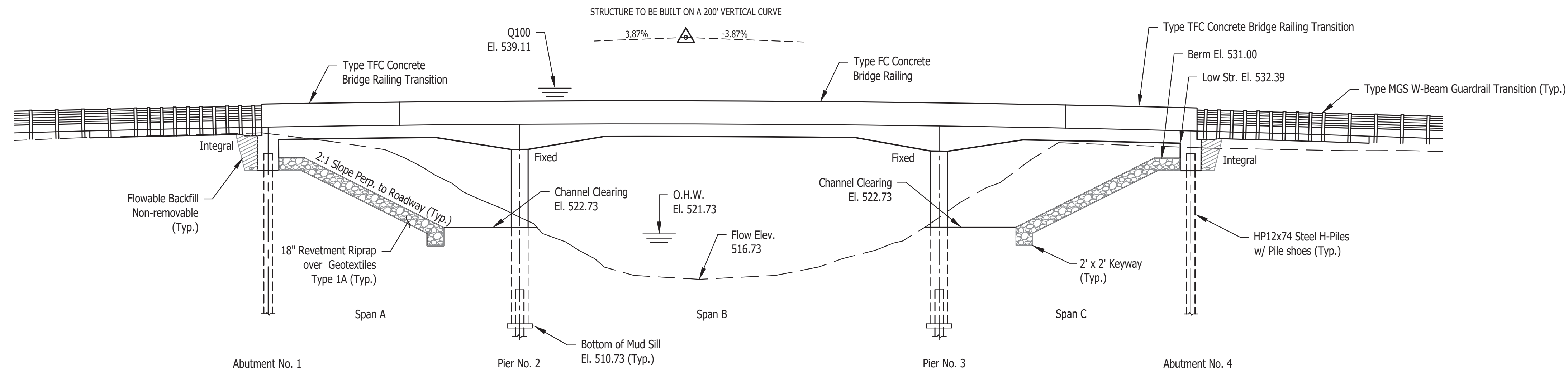
DESIGN DATA

Designed for HL-93 loading, in accordance with the AASHTO LRFD Bridge Design Specifications, Eight Edition, 2017 & all subsequent interims.

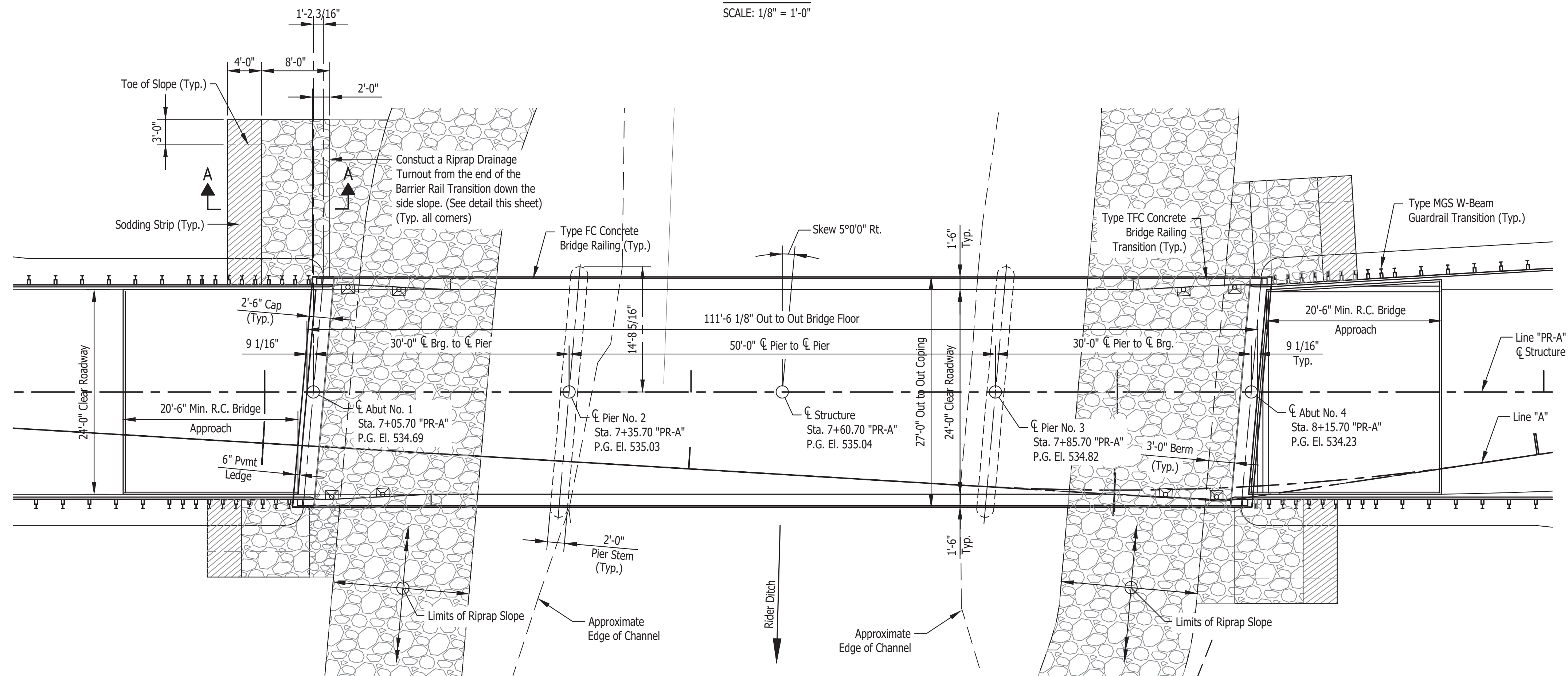
DEAD LOAD
 Actual weight plus 35lb/ft² for future wearing surface.

FLOOR SLAB
 Designed with a 17 1/2" structural depth plus 1/2" sacrificial wearing surface, with an 18" deep haunch.

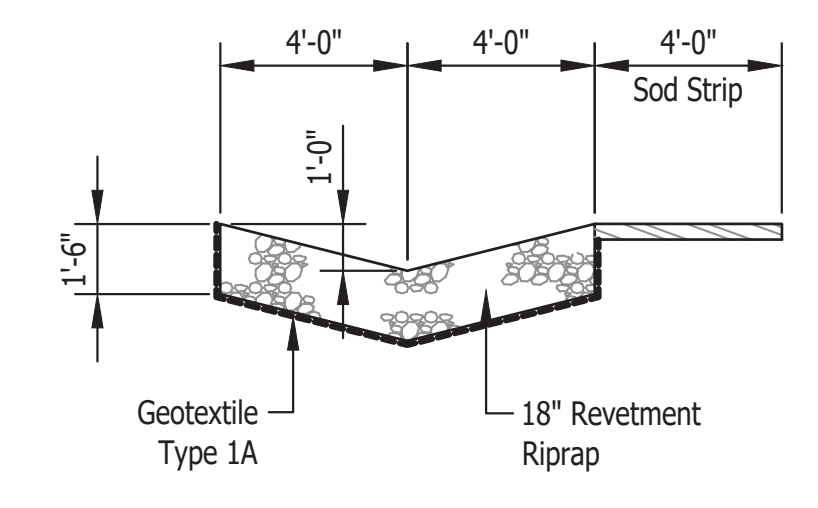
SEISMIC DESIGN DATA
 Seismic Performance Zone Zone X
 Acceleration Coefficient, SD1 X.XXX
 Seismic Soil Profile Type Class X



ELEVATION
 SCALE: 1/8" = 1'-0"



PLAN
 SCALE: 1/8" = 1'-0"



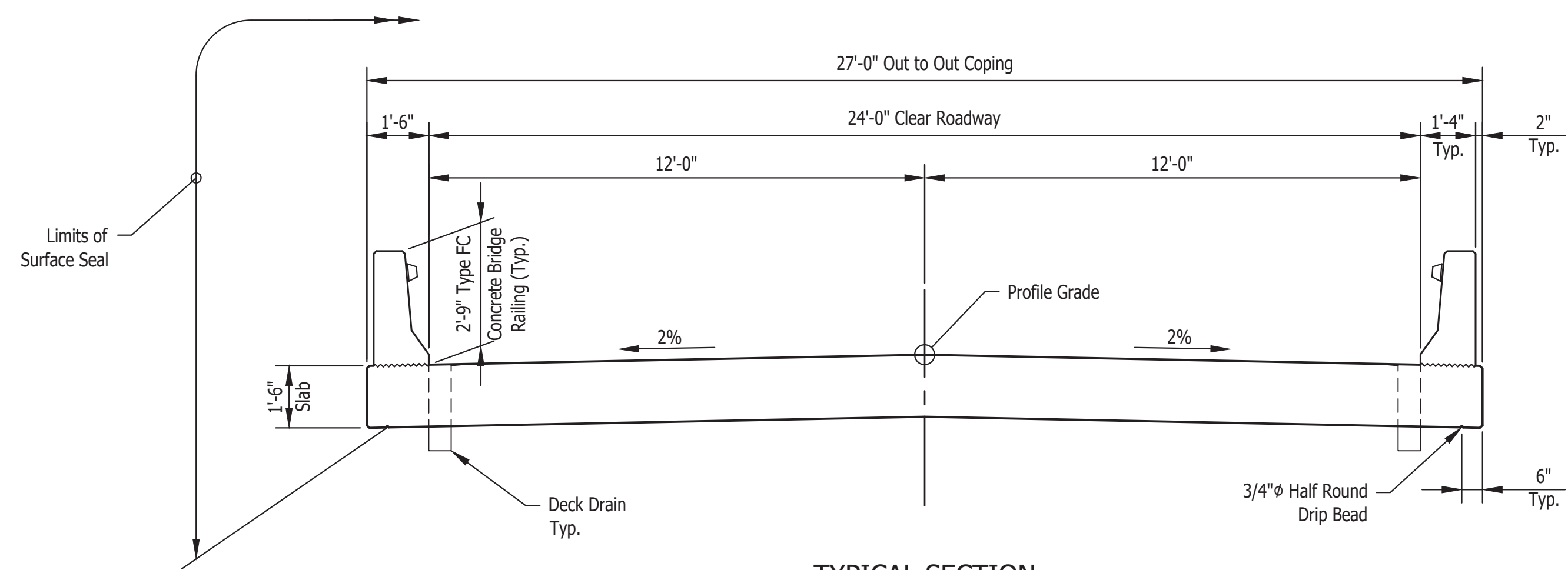
SECTION A-A
 SCALE: 1/4" = 1'-0"

NOTE TO REVIEWER:
 Deck drain locations as shown are preliminary.
 Final deck drain design to be completed at future submittal.

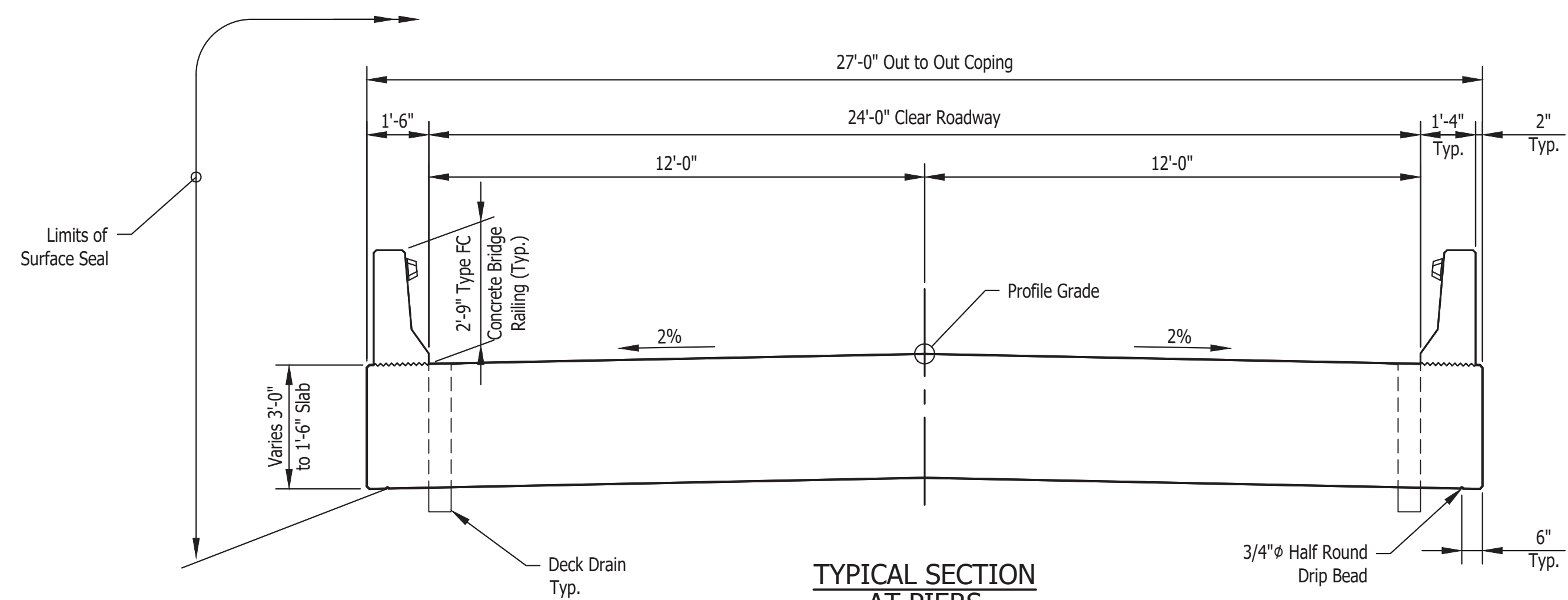
CONTINUOUS REINFORCED CONCRETE SLAB
 3 SPANS: 30'-0", 50'-0", & 30'-0"
 SKEW: 5° RT, CLEAR ROADWAY = 24'-0"
 E COUNTY ROAD 300 S OVER RIDER DITCH
 JACKSON COUNTY

Last Saved By: T.Hickson
 Date: 12/18/2020
 Plot Scale: 1:1
 Drawing File Path: L:\206 - Jackson Co Bridge - Design\DWG\Bridges\005-GENPLAN.dwg

| | | | | | |
|---|--|--------------------------------------|--|----------------------------|------------------------|
| RECOMMENDED FOR APPROVAL: _____ 9/17/2020 DESIGN ENGINEER DATE | | INDIANA DEPARTMENT OF TRANSPORTATION | | HORIZONTAL SCALE 1"=20' | FILE - |
| DESIGNED: ZDE DRAWN: TLH | | GENERAL PLAN SHEET 1 OF 2 | | VERTICAL SCALE 1"=10' | DESIGNATION 1703020 |
| CHECKED: KDE CHECKED: KDE | | | | SURVEY BOOK | SHEET 8 of 15 |
| | | | | CONTRACT B-40895 | PROJECT 1703020 |



TYPICAL SECTION
AT END BENTS
SCALE: 3/8" = 1'-0"



TYPICAL SECTION
AT PIERS
SCALE: 3/8" = 1'-0"

CONTINUOUS REINFORCED CONCRETE SLAB
3 SPANS: 30'-0", 50'-0", & 30'-0"
SKEW: 5° RT, CLEAR ROADWAY = 24'-0"
E COUNTY ROAD 300 S OVER RIDER DITCH
JACKSON COUNTY

Last Saved By: T.Hutton
 Date: 12/18/2020
 Plot Scale: 1:1
 Drawing File: I:\208 - Jackson Co Bridge 154 Design\DWG\BID\0500-GENPLAN TYP SEC.dwg

| | |
|---------------------------|--------------|
| RECOMMENDED FOR APPROVAL: | 9/17/2020 |
| DESIGN ENGINEER | DATE |
| DESIGNED: ZDE | DRAWN: TLH |
| CHECKED: KDE | CHECKED: KDE |

| | |
|---|--|
| INDIANA DEPARTMENT OF TRANSPORTATION | |
| GENERAL PLAN SHEET 2 OF 2 | |

| | |
|------------------|-------------|
| HORIZONTAL SCALE | FILE |
| 1"=20' | - |
| VERTICAL SCALE | DESIGNATION |
| 1"=10' | 1703020 |
| SURVEY BOOK | SHEET |
| | 9 of 15 |
| CONTRACT | PROJECT |
| B-40895 | 1703020 |

APPENDIX C

Early Coordination



Sample Early Coordination Letter

May 19, 2020

{See recipient list attached.}

Re: Early Coordination
Designation Number (Des. No.) 1703020
Bridge Project Bridge No. 36-00154
East County Road (CR) 300 South over Rider Ditch, 0.82 mile East of CR 840 East
Washington Township, Jackson County, Indiana

Dear Agency:

Jackson County and Federal Highway Administration (FHWA) intends to proceed with a project involving the aforementioned bridge project in Jackson County, Indiana. This letter is part of the early coordination phase of the environmental review process. We are requesting comments from your area of expertise regarding possible environmental effects associated with this project. **Please use the above designation number and description in your reply.** We will incorporate your comments into the environmental report for this project in accordance with the National Environmental Policy Act. Your cooperation in this endeavor is appreciated.

The project is located on CR 300 S over Rider Ditch, approximately 0.82 mile east of CR 840 E in Jackson County. Specifically, the project is located in Sections 28 and 33, Township 5 North, Range 6 East of the Crothersville, Indiana 7.5-minute United States Geological Survey (USGS) topographic quadrangle. The existing structure is a single span, 90 feet long steel bridge with a wooden deck, which was constructed in 1910, repaired in 1987 and 2011, and rehabilitated in 1992 and 2008. The bridge is eligible for the National Register of Historic Places. CR 300 S is classified as a Local Rural road. A typical cross section of CR 300 S consists of one 9 feet wide through-lane adjoined by an approximately 2-3 feet wide asphalt shoulders in each direction. No guardrails, curbs or sidewalks are provided. Land use in the vicinity of the project is primarily a mix of forested and agriculture with some residential homes west of the project area.

The need for this project is evident by the deteriorating condition of the existing structure. In the most recent Bridge Inspection Report, dated March 19, 2019 the bridge deck exhibited heavy wear from gravel, an isolated hole with patch, and minor splits. The superstructure exhibited heavy rusting and section loss of the lower connection plates. The substructure showed signs of heavy honeycombing and scaling in the concrete portion of the abutments. The structure was rated 2 out of 9 possible points, indicating critical condition.

The current proposed project would be to replace the existing bridge #36-00154. The existing truss bridge would be removed and relocated if a party comes forward to finance the bridge relocation or will be demolished in place. The new bridge would be a continuous three-span reinforced concrete slab bridge on the same alignment. It is anticipated that less than 0.5 acre of additional right-of-way will be required for this project. The amount of right-of-way will be defined as the design process advances.

Metric Environmental, LLC will perform waters and wetlands determinations and a biological assessment to identify any ecological resources that may be present. This project qualifies for the application of the USFWS

range-wide programmatic informal consultation for the Indiana bat and northern long-eared bat and project information will be submitted through USFW's Information for Planning and Consultation (IPaC) separately.

This project will require full Section 106 with Section 4(f) analysis and Bridge Marketing. Metric will prepare the required Consulting Parties Early Coordination Letter, Phase Ia Archaeology, Historic Property Report, Section 4(f) Evaluation and Alternatives Analysis, and Finding of Effect as required and submit documentation to the Indiana Department of Transportation Cultural Resources Office and the Indiana Department of Natural Resources (IDNR) Division of Historic Preservation and Archaeology (DHPA) State Historic Preservation Officer (SHPO) for review and concurrence.

Should we not receive your response within thirty (30) calendar days from the date of this letter, it will be assumed that your agency feels that there will be no adverse effects incurred as a result of the proposed project. However, should you find that an extension to the response time is necessary, a reasonable amount may be granted upon request. If you have any questions, please contact Susan Castle, Senior Consultant, Metric Environmental, at 317.608.2730, Susanc@MetricEnv.com, or 6971 Hillside Court, Indianapolis, Indiana 46250 or Jerry Ault, Highway Supervisor, Jackson County, at jault@jacksoncounty.in.gov or 812.358.2226. Thank you in advance for your input.

Sincerely,

Metric Environmental, LLC



Susan K. Castle
Senior Consultant

cc: File No. 19-0011
Jeff Matern, PE, JMatern@jsengr.com, JSE, Inc.
Jerry Ault, Highway Supervisor, jault@jacksoncounty.in.gov Jackson county

Attachments: Early Coordination Recipients, Location Map, United States Geological Service Topographic Map, 2016 Aerial Photograph, Natural Resources Conservation Service Soils Map, Soils Map Legend, National Wetland Inventory Map, and Federal Emergency Management Association Flood Insurance Rate Map

Maps and Graphics Provided with this Letter are located in Appendix B of this document



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204

Eric J. Holcomb, Governor
Joe McGuinness, Commissioner

The following agencies received Early Coordination Letters:

Federal Highway Administration
Seymour District
{Michelle.Allen@dot.gov}
{Erica.Tait@dot.gov}

Indiana Geological Survey
{<https://igs.indiana.edu/eAssessment/>}

Indiana Department of Environmental Management
Wellhead Protection Proximity
{<https://www.in.gov/idem/cleanwater/2456.htm>}

Indiana Department of Natural Resources
Division of Fish and Wildlife
{environmentalreview@dnr.in.gov}

Indiana Department of Environmental Management
Proposed Roadway Construction Projects
{<https://www.in.gov/idem/5284.htm>}

INDOT Office of Public Involvement
{rclark@indot.in.gov}

United States Department of Housing and Urban
Development
{Paul.J.Lehmann@hud.gov}

National Parks Service
Midwest Regional Office
{Hector_Santiago@nps.gov}

INDOT Seymour District
{DDye@indot.in.gov}

U.S. Fish and Wildlife Service
Bloomington Indiana Field Office
{robin_mcwilliams@fws.gov}

Forest Supervisor
Hoosier National Forest
{kamick@fs.fed.us}

Natural Resources Conservation Service
{Rick.Neilson@in.usda.gov}

United States Army Corps of Engineers
{CELRL.Door.To.The.Corps@usace.army.mil}

Jackson County Surveyor
Daniel Blann, Surveyor
{dblann@jacksoncounty.in.gov}

Jackson County Highway Department
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{jault@jacksoncounty.in.gov}

Jackson County – Emergency Management
Duane Davis - Director
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Jackson County Commissioners
Drew Markel
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State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

DNR #: ER-22575

Request Received: May 19, 2020

Requestor: Metric Environmental
Susan Castle
6971 Hillside Court
Indianapolis, IN 46250

Project: CR 300 South bridge (#36-00154) replacement over Rider Ditch, about 0.82 mile east of CR 840 East; Des #1703020

County/Site info: Jackson

The Indiana Department of Natural Resources has reviewed the above referenced project per your request. Our agency offers the following comments for your information and in accordance with the National Environmental Policy Act of 1969.

If our agency has regulatory jurisdiction over the project, the recommendations contained in this letter may become requirements of any permit issued. If we do not have permitting authority, all recommendations are voluntary.

Regulatory Assessment: This proposal will require the formal approval for construction in a floodway under the Flood Control Act, IC 14-28-1. Please submit a copy of this letter with the permit application.

Natural Heritage Database: The Natural Heritage Program's data have been checked. Little Spectaclecase (*Villosa lianosa*), a state species of special concern, has been documented in Rider Ditch within the project area.

Fish & Wildlife Comments: As long as standard erosion control measures are implemented, and in-channel disturbance is minimized as much as possible, we do not foresee any impacts to Little Spectaclecase as a result of this project.

Avoid and minimize impacts to fish, wildlife, and botanical resources to the greatest extent possible, and compensate for impacts. The following are recommendations that address potential impacts identified in the proposed project area:

1) Bank Stabilization & Wildlife Passage:

The banks under the bridge currently appear to facilitate the unimpacted movement of wildlife along the creek banks under the road. The new, replacement, or rehabbed structure, and any bank stabilization under the structure, should not create conditions that are less favorable for wildlife passage under the structure compared to current conditions. A level area of natural ground under the structure is ideal for wildlife passage. If channel clearing will result in a flat bench area above the normal water level under the structure, this area should allow wildlife passage and should remain free of riprap and other similar materials that can impair wildlife passage.

Minimize the use of riprap and use alternative erosion protection materials whenever possible. Riprap must not be placed in the active thalweg channel or placed in the streambed in a manner that precludes fish or aquatic organism passage (riprap must not be placed above the existing streambed elevation). Where riprap must be used, we recommend placing only enough riprap to provide stream bank toe protection, such as from the toe of the bank up to the ordinary high water mark (OHWM). The banks above the OHWM should be restored, stabilized, and revegetated using geotextiles and a mixture of grasses, sedges, wildflowers, shrubs, and trees native to the area and specifically for stream bank/floodway stabilization purposes as soon as possible upon

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completion.

While hard armoring alone (e.g. riprap or glacial stone) may be needed in certain instances, soft armoring and bioengineering techniques should be considered first. In many instances, one or more methods are necessary to increase the likelihood of vegetation establishment. Combining vegetation with most bank stabilization methods can provide additional bank protection and help reduce impacts upon fish and wildlife. If hard armoring is needed, wildlife passage can be facilitated by using a smooth-surfaced armoring material instead of riprap, such as articulated concrete block mats, fabric-formed concrete mats, or other similar smooth-surfaced material.

Information about bioengineering techniques can be found at <http://www.in.gov/legislative/iac/20120404-IR-312120154NRA.xml.pdf>. Also, the following is a USDA/NRCS document that outlines many different bioengineering techniques for streambank stabilization: <http://directives.sc.egov.usda.gov/17553.wba>.

2) Riparian Habitat:

We recommend a mitigation plan be developed (and submitted with the permit application, if required) for any unavoidable habitat impacts that will occur. The DNR's Habitat Mitigation guidelines (and plant lists) can be found online at: <http://iac.iga.in.gov/iac/20200527-IR-312200284NRA.xml.pdf>.

Impacts to non-wetland forest of one (1) acre or more should be mitigated at a minimum 2:1 ratio. If less than one acre of non-wetland forest is removed in a rural setting, replacement should be at a 1:1 ratio based on area. Impacts to wetland habitat should be mitigated at the appropriate ratio according to the 1991 INDOT/IDNR/USFWS Memorandum of Understanding.

The mitigation site should be located in the floodway, downstream of the one (1) square mile drainage area of that stream (or another stream within the 8-digit HUC, preferably as close to the impact site as possible) and adjacent to existing forested riparian habitat.

The additional measures listed below should be implemented to avoid, minimize, or compensate for impacts to fish, wildlife, and botanical resources:

1. Revegetate all bare and disturbed areas with a mixture of native grasses, sedges, wildflowers, and also native hardwood trees and shrubs if any woody plants are disturbed during construction as soon as possible upon completion. Do not use any varieties of Tall Fescue or other non-native plants, including prohibited invasive species (see 312 IAC 18-3-25).
2. Minimize and contain within the project limits inchannel disturbance and the clearing of trees and brush.
3. Do not work in the waterway from April 1 through June 30 without the prior written approval of the Division of Fish and Wildlife.
4. Do not cut any trees suitable for Indiana bat or Northern Long-eared bat roosting (greater than 5 inches dbh, living or dead, with loose hanging bark, or with cracks, crevices, or cavities) from April 1 through September 30,
5. Do not excavate in the low flow area except for the placement of piers, foundations, and riprap, or removal of the old structure.
6. Do not construct any temporary runarounds, access bridges, causeways, cofferdams, diversions, or pumparounds.
7. Use minimum average 6 inch graded riprap stone extended below the normal water level to provide habitat for aquatic organisms in the voids.
8. Appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from entering the stream or leaving the construction site; maintain these measures until construction is complete and all disturbed areas are

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stabilized.

9. Seed and protect all disturbed streambanks and slopes not protected by other methods that are 3:1 or steeper with erosion control blankets that are heavy-duty, biodegradable, and net free or that use loose-woven / Leno-woven netting to minimize the entrapment and snaring of small-bodied wildlife such as snakes and turtles (follow manufacturer's recommendations for selection and installation); seed and apply mulch on all other disturbed areas.

Contact Staff:

Christie L. Stanifer, Environ. Coordinator, Fish & Wildlife
Our agency appreciates this opportunity to be of service. Please contact the above staff member at (317) 232-4080 if we can be of further assistance.

Christie L. Stanifer

Date: June 18, 2020

Christie L. Stanifer
Environ. Coordinator
Division of Fish and Wildlife



United States Department of the Interior



FISH AND WILDLIFE SERVICE

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

Phone: (812) 334-4261 Fax: (812) 334-4273

<http://www.fws.gov/midwest/Endangered/section7/s7process/step1.html>

In Reply Refer To:

July 06, 2020

Consultation Code: 03E12000-2020-I-2117

Event Code: 03E12000-2020-E-08439

Project Name: Bridge Project (Des. No. 1703020) East County Road 300 South over Rider Ditch, Jackson County

Subject: Concurrence verification letter for the 'Bridge Project (Des. No. 1703020) East County Road 300 South over Rider Ditch, Jackson County' project under the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

To whom it may concern:

The U.S. Fish and Wildlife Service (Service) has received your request to verify that the **Bridge Project (Des. No. 1703020) East County Road 300 South over Rider Ditch, Jackson County** (Proposed Action) may rely on the concurrence provided in the February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat (PBO) to satisfy requirements under Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended; 16 U.S.C 1531 *et seq.*).

Based on the information you provided (Project Description shown below), you have determined that the Proposed Action is within the scope and adheres to the criteria of the PBO, including the adoption of applicable avoidance and minimization measures, and may affect, but is not likely to adversely affect (NLAA) the endangered Indiana bat (*Myotis sodalis*) and/or the threatened Northern long-eared bat (*Myotis septentrionalis*).

The Service has 14 calendar days to notify the lead Federal action agency or designated non-federal representative if we determine that the Proposed Action does not meet the criteria for a NLAA determination under the PBO. If we do not notify the lead Federal action agency or designated non-federal representative within that timeframe, you may proceed with the Proposed Action under the terms of the NLAA concurrence provided in the PBO. This verification period allows Service Field Offices to apply local knowledge to implementation of the PBO, as we may identify a small subset of actions having impacts that were unanticipated. In such instances,

Service Field Offices may request additional information that is necessary to verify inclusion of the proposed action under the PBO.

For Proposed Actions that include bridge/structure removal, replacement, and/or maintenance activities: If your initial bridge/structure assessments failed to detect Indiana bats, but you later detect bats during construction, please submit the Post Assessment Discovery of Bats at Bridge/Structure Form (User Guide Appendix E) to this Service Office. In these instances, potential incidental take of Indiana bats may be exempted provided that the take is reported to the Service.

If the Proposed Action is modified, or new information reveals that it may affect the Indiana bat and/or Northern long-eared bat in a manner or to an extent not considered in the PBO, further review to conclude the requirements of ESA Section 7(a)(2) may be required. If the Proposed Action may affect any other federally-listed or proposed species, and/or any designated critical habitat, additional consultation between the lead Federal action agency and this Service Office is required. If the proposed action has the potential to take bald or golden eagles, additional coordination with the Service under the Bald and Golden Eagle Protection Act may also be required. In either of these circumstances, please contact this Service Office.

Project Description

The following project name and description was collected in IPaC as part of the endangered species review process.

Name

Bridge Project (Des. No. 1703020) East County Road 300 South over Rider Ditch, Jackson County

Description

The Jackson County Commissioners with oversight from the Indiana Department of Transportation (INDOT), and partial funding from Federal Highway Administration (FHWA), intend to proceed with a bridge project on East CR 300 South over Rider Ditch, in Washington Township, Jackson County.

The project is located on East CR 300 South over Rider Ditch, approximately 0.82 mile east of CR 840 E in southeast Jackson County. Specifically, the project is located in Sections 28 and 33, Township 5 North, Range 6 East as illustrated on the Crothersville, Indiana 7.5-minute United States Geological Survey (USGS) topographic quadrangle.

The project is located in a rural agricultural area, and suitable summer habitat does exist within and adjacent to the project area. The scope of work currently proposes the replacement of the existing single-span, steel pony truss bridge with a new, three-span, concrete slab bridge on the same alignment. The existing bridge is 90 feet in length with a wood plank deck.

Approximately 0.03 acre of trees and brush will be removed from the northwest, southwest and southeast quadrants of the bridge crossing to make room for the installation of the new bridge structure. The northeast quadrant has been cleared of trees and consists of mainly low ground covers such as red fescue and red clover and poison ivy. The dominant vegetation observed in the project area consisted of bitternut hickory in the tree stratum, pawpaw and silver maple in the sapling and shrub stratum, yellow wingstem and creeping jenny in the herb stratum.

At this time, it is estimated that less than 0.5 acre of new, additional permanent right-of-way will be required to complete the project. There is no existing, permanent lighting present at the project site, and no new permanent lighting is proposed as part of the bridge replacement project. Temporary lighting could be used during construction at the contractor's discretion; however, due to the rural location of the project it is unlikely that night-time work would be conducted. The project is scheduled to let in October 2021, and tree clearing activities are anticipated to begin in early 2022, during the inactive season for bats.

On May 20, 2020, the INDOT Seymour District reviewed the U.S. Fish and Wildlife Service database and it was revealed that no endangered bat species have been documented in or within 0.5 mile of the project area. On May 27, 2020 qualified individuals with Metric Environmental inspected Jackson County Bridge No. 154 for bats. No bats were seen or heard in/or under the bridge and no evidence such as guano or urine staining was observed.

Determination Key Result

Based on your answers provided, this project(s) may affect, but is not likely to adversely affect the endangered Indiana bat and/or the threatened Northern long-eared bat, therefore, consultation with the U.S. Fish and Wildlife Service pursuant to Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended 16 U.S.C. 1531 *et seq.*) is required. However, also based on your answers provided, this project may rely on the concurrence provided in the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

Qualification Interview

1. Is the project within the range of the Indiana bat^[1]?

[1] See [Indiana bat species profile](#)

Automatically answered

Yes

2. Is the project within the range of the Northern long-eared bat^[1]?

[1] See [Northern long-eared bat species profile](#)

Automatically answered

Yes

3. Which Federal Agency is the lead for the action?

A) *Federal Highway Administration (FHWA)*

4. Are *all* project activities limited to non-construction^[1] activities only? (examples of non-construction activities include: bridge/abandoned structure assessments, surveys, planning and technical studies, property inspections, and property sales)

[1] Construction refers to activities involving ground disturbance, percussive noise, and/or lighting.

No

5. Does the project include *any* activities that are **greater than** 300 feet from existing road/rail surfaces^[1]?

[1] Road surface is defined as the actively used [e.g. motorized vehicles] driving surface and shoulders [may be pavement, gravel, etc.] and rail surface is defined as the edge of the actively used rail ballast.

No

6. Does the project include *any* activities **within** 0.5 miles of a known Indiana bat and/or NLEB hibernaculum^[1]?

[1] For the purpose of this consultation, a hibernaculum is a site, most often a cave or mine, where bats hibernate during the winter (see suitable habitat), but could also include bridges and structures if bats are found to be hibernating there during the winter.

No

7. Is the project located **within** a karst area?

No

8. Is there *any* suitable^[1] summer habitat for Indiana Bat or NLEB **within** the project action area^[2]? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] The action area is defined as all areas to be affected directly or indirectly by the Federal action and not merely the immediate area involved in the action (50 CFR Section 402.02). Further clarification is provided by the [national consultation FAQs](#).

Yes

9. Will the project remove *any* suitable summer habitat^[1] and/or remove/trim any existing trees **within** suitable summer habitat?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

Yes

10. Will the project clear more than 20 acres of suitable habitat per 5-mile section of road/rail?

No

11. Have presence/probable absence (P/A) summer surveys^{[1][2]} been conducted^{[3][4]} **within** the suitable habitat located within your project action area?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] Presence/probable absence summer surveys conducted within the fall swarming/spring emergence home range of a documented Indiana bat hibernaculum (contact local Service Field Office for appropriate distance from hibernacula) that result in a negative finding requires additional consultation with the local Service Field Office to determine if clearing of forested habitat is appropriate and/or if seasonal clearing restrictions are needed to avoid and minimize potential adverse effects on fall swarming and spring emerging Indiana bats.

[3] For projects within the range of either the Indiana bat or NLEB in which suitable habitat is present, and no bat surveys have been conducted, the transportation agency will assume presence of the appropriate species. This assumption of presence should be based upon the presence of suitable habitat and the capability of bats to occupy it because of their mobility.

[4] Negative presence/probable absence survey results obtained using the [summer survey guidance](#) are valid for a minimum of two years from the completion of the survey unless new information (e.g., other nearby surveys) suggest otherwise.

No

12. Does the project include activities **within documented Indiana bat habitat**^{[1][2]}?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry triangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

No

13. Will the removal or trimming of habitat or trees occur **within** suitable but **undocumented Indiana bat** roosting/foraging habitat or travel corridors?

Yes

14. What time of year will the removal or trimming of habitat or trees **within** suitable but **undocumented Indiana bat** roosting/foraging habitat or travel corridors occur^[1]?

[1] Coordinate with the local Service Field Office for appropriate dates.

B) During the inactive season

15. Does the project include activities **within documented NLEB habitat**^{[1][2]}?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry biangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

No

16. Will the removal or trimming of habitat or trees occur **within** suitable but **undocumented NLEB** roosting/foraging habitat or travel corridors?

Yes

17. What time of year will the removal or trimming of habitat or trees **within** suitable but **undocumented NLEB** roosting/foraging habitat or travel corridors occur?

B) During the inactive season

18. Will *any* tree trimming or removal occur **within** 100 feet of existing road/rail surfaces?

Yes

19. Will *any* tree trimming or removal occur **between** 100-300 feet of existing road/rail surfaces?

No

20. Are *all* trees that are being removed clearly demarcated?

Yes

21. Will the removal of habitat or the removal/trimming of trees include installing new or replacing existing **permanent** lighting?

No

22. Does the project include wetland or stream protection activities associated with compensatory wetland mitigation?

No

23. Does the project include slash pile burning?

No

24. Does the project include *any* bridge removal, replacement, and/or maintenance activities (e.g., any bridge repair, retrofit, maintenance, and/or rehabilitation work)?

Yes

25. Is there *any* suitable habitat^[1] for Indiana bat or NLEB **within** 1,000 feet of the bridge? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's current [summer survey guidance](#) for our current definitions of suitable habitat.

Yes

26. Has a bridge assessment^[1] been conducted **within** the last 24 months^[2] to determine if the bridge is being used by bats?

[1] See [User Guide Appendix D](#) for bridge/structure assessment guidance

[2] Assessments must be completed no more than 2 years prior to conducting any work below the deck surface on all bridges that meet the physical characteristics described in the Programmatic Consultation, regardless of whether assessments have been conducted in the past. Due to the transitory nature of bat use, a negative result in one year does not guarantee that bats will not use that bridge/structure in subsequent years.

Yes

SUBMITTED DOCUMENTS

- *USFWS Bat Datasheet.pdf* <https://ecos.fws.gov/ipac/project/EBM3O4E6JNASZEVKA2WZBYUD4U/projectDocuments/22430543>

27. Did the bridge assessment detect *any* signs of Indiana bats and/or NLEBs roosting in/under the bridge (bats, guano, etc.)^[1]?

[1] If bridge assessment detects signs of *any* species of bats, coordination with the local FWS office is needed to identify potential threatened or endangered bat species. Additional studies may be undertaken to try to identify which bat species may be utilizing the bridge prior to allowing *any* work to proceed.

Note: There is a small chance bridge assessments for bat occupancy do not detect bats. Should a small number of bats be observed roosting on a bridge just prior to or during construction, such that take is likely to occur or does occur in the form of harassment, injury or death, the PBO requires the action agency to report the take. Report all unanticipated take within 2 working days of the incident to the USFWS. Construction activities may continue without delay provided the take is reported to the USFWS and is limited to 5 bats per project.

No

28. Will the bridge removal, replacement, and/or maintenance activities include installing new or replacing existing **permanent** lighting?

No

29. Does the project include the removal, replacement, and/or maintenance of *any* structure other than a bridge? (e.g., rest areas, offices, sheds, outbuildings, barns, parking garages, etc.)

No

30. Will the project involve the use of **temporary** lighting *during* the active season?

Yes

31. Is there *any* suitable habitat **within** 1,000 feet of the location(s) where **temporary** lighting will be used?

Yes

32. Will the project install new or replace existing **permanent** lighting?

No

33. Does the project include percussives or other activities (**not including tree removal/trimming or bridge/structure work**) that will increase noise levels above existing traffic/background levels?

No

34. Are *all* project activities that are **not associated with** habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives, limited to actions that DO NOT cause any additional stressors to the bat species?

Examples: lining roadways, unlighted signage , rail road crossing signals, signal lighting, and minor road repair such as asphalt fill of potholes, etc.

Yes

35. Will the project raise the road profile **above the tree canopy**?

No

36. Are the project activities that are not associated with habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives consistent with a No Effect determination in this key?

Automatically answered

Yes, other project activities are limited to actions that DO NOT cause any additional stressors to the bat species as described in the BA/BO

37. Is the habitat removal portion of this project consistent with a Not Likely to Adversely Affect determination in this key?

Automatically answered

Yes, because the tree removal/trimming that occurs outside of the Indiana bat's active season occurs greater than 0.5 miles from the nearest hibernaculum, is less than 100 feet from the existing road/rail surface, includes clear demarcation of the trees that are to be removed, and does not alter documented roosts and/or surrounding summer habitat within 0.25 miles of a documented roost.

38. Is the habitat removal portion of this project consistent with a Not Likely to Adversely Affect determination in this key?

Automatically answered

Yes, because the tree removal/trimming that occurs outside of the NLEB's active season occurs greater than 0.5 miles from the nearest hibernaculum, is less than 100 feet from the existing road/rail surface, includes clear demarcation of the trees that are to be removed, and does not alter documented roosts and/or surrounding summer habitat within 0.25 miles of a documented roost.

39. Is the bridge removal, replacement, or maintenance activities portion of this project consistent with a No Effect determination in this key?

Automatically answered

Yes, because the bridge has been assessed using the criteria documented in the BA and no signs of bats were detected

40. General AMM 1

Will the project ensure *all* operators, employees, and contractors working in areas of known or presumed bat habitat are aware of *all* FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable Avoidance and Minimization Measures?

Yes

41. Tree Removal AMM 1

Can *all* phases/aspects of the project (e.g., temporary work areas, alignments) be modified, to the extent practicable, to avoid tree removal^[1] in excess of what is required to implement the project safely?

Note: Tree Removal AMM 1 is a minimization measure, the full implementation of which may not always be practicable. Projects may still be NLAA as long as Tree Removal AMMs 2, 3, and 4 are implemented and LAA as long as Tree Removal AMMs 3, 5, 6, and 7 are implemented.

[1] The word “trees” as used in the AMMs refers to trees that are suitable habitat for each species within their range. See the USFWS’ current summer survey guidance for our latest definitions of suitable habitat.

Yes

42. Tree Removal AMM 3

Can tree removal be limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits)?

Yes

43. Tree Removal AMM 4

Can the project avoid cutting down/removal of *all* (1) **documented**^[1] Indiana bat or NLEB roosts^[2] (that are still suitable for roosting), (2) trees **within** 0.25 miles of roosts, and (3) documented foraging habitat any time of year?

[1] The word documented means habitat where bats have actually been captured and/or tracked.

[2] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry biangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

Yes

44. Lighting AMM 1

Will *all* **temporary** lighting be directed away from suitable habitat during the active season?

Yes

Project Questionnaire

1. Have you made a No Effect determination for *all* other species indicated on the FWS IPaC generated species list?

N/A

2. Have you made a May Affect determination for *any* other species on the FWS IPaC generated species list?

N/A

3. How many acres^[1] of trees are proposed for removal between 0-100 feet of the existing road/rail surface?

[1] If described as number of trees, multiply by 0.09 to convert to acreage and enter that number.

0.03

4. Please describe the proposed bridge work:

The scope of work currently proposes the replacement of the existing single-span, steel pony truss bridge with a new, three-span, concrete slab bridge on the same alignment.

5. Please state the timing of all proposed bridge work:

The project is scheduled to let in October 2021, and tree clearing activities are anticipated to begin in early 2022, prior to the start of bridge construction and during the inactive season for bats.

6. Please enter the date of the bridge assessment:

On May 27, 2020

Avoidance And Minimization Measures (AMMs)

This determination key result includes the commitment to implement the following Avoidance and Minimization Measures (AMMs):

GENERAL AMM 1

Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs.

LIGHTING AMM 1

Direct temporary lighting away from suitable habitat during the active season.

TREE REMOVAL AMM 1

Modify all phases/aspects of the project (e.g., temporary work areas, alignments) to avoid tree removal.

TREE REMOVAL AMM 2

Apply time of year restrictions for tree removal when bats are not likely to be present, or limit tree removal to 10 or fewer trees per project at any time of year within 100 feet of existing road/rail surface and **outside of documented** roosting/foraging habitat or travel corridors; visual emergence survey must be conducted with no bats observed.

TREE REMOVAL AMM 3

Ensure tree removal is limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits).

TREE REMOVAL AMM 4

Do not remove **documented** Indiana bat or NLEB roosts that are still suitable for roosting, or trees within 0.25 miles of roosts, or **documented** foraging habitat any time of year.

Determination Key Description: FHWA, FRA, FTA Programmatic Consultation For Transportation Projects Affecting NLEB Or Indiana Bat

This key was last updated in IPaC on December 02, 2019. Keys are subject to periodic revision.

This decision key is intended for projects/activities funded or authorized by the Federal Highway Administration (FHWA), Federal Railroad Administration (FRA), and/or Federal Transit Administration (FTA), which may require consultation with the U.S. Fish and Wildlife Service (Service) under Section 7 of the Endangered Species Act (ESA) for the endangered **Indiana bat** (*Myotis sodalis*) and the threatened **Northern long-eared bat** (NLEB) (*Myotis septentrionalis*).

This decision key should only be used to verify project applicability with the Service's [February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects](#). The programmatic biological opinion covers limited transportation activities that may affect either bat species, and addresses situations that are both likely and not likely to adversely affect either bat species. This decision key will assist in identifying the effect of a specific project/activity and applicability of the programmatic consultation. The programmatic biological opinion is not intended to cover all types of transportation actions. Activities outside the scope of the programmatic biological opinion, or that may affect ESA-listed species other than the Indiana bat or NLEB, or any designated critical habitat, may require additional ESA Section 7 consultation.



United States Department of the Interior



FISH AND WILDLIFE SERVICE

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

Phone: (812) 334-4261 Fax: (812) 334-4273

<http://www.fws.gov/midwest/Endangered/section7/s7process/step1.html>

In Reply Refer To:

July 06, 2020

Consultation Code: 03E12000-2020-SLI-2117

Event Code: 03E12000-2020-E-08431

Project Name: Bridge Project (Des. No. 1703020) East County Road 300 South over Rider Ditch, Jackson County

Subject: List of threatened and endangered species that may occur in your proposed project location, and/or may be affected by your proposed project

To Whom It May Concern:

The attached species list identifies any federally threatened, endangered, proposed and candidate species that may occur within the boundary of your proposed project or may be affected by your proposed project. The list also includes designated critical habitat if present within your proposed project area or affected by your project. This list is provided to you as the initial step of the consultation process required under section 7(c) of the Endangered Species Act, also referred to as Section 7 Consultation.

Section 7 of the Endangered Species Act of 1973 requires that actions authorized, funded, or carried out by Federal agencies not jeopardize federally threatened or endangered species or adversely modify designated critical habitat. To fulfill this mandate, Federal agencies (or their designated non-federal representative) must consult with the Service if they determine their project “may affect” listed species or critical habitat.

Under 50 CFR 402.12(e) (the regulations that implement Section 7 of the Endangered Species Act) the accuracy of this species list should be verified after 90 days. This verification can be completed formally or informally. You may verify the list by visiting the ECOS-IPaC website <http://ecos.fws.gov/ipac/> at regular intervals during project planning and implementation and completing the same process you used to receive the attached list. As an alternative, you may contact this Ecological Services Field Office for updates.

Please use the species list provided and visit the U.S. Fish and Wildlife Service's Region 3 Section 7 Technical Assistance website at - <http://www.fws.gov/midwest/endangered/section7/s7process/index.html>. This website contains step-by-step instructions which will help you

determine if your project will have an adverse effect on listed species and will help lead you through the Section 7 process.

For all **wind energy projects** and **projects that include installing towers that use guy wires or are over 200 feet in height**, please contact this field office directly for assistance, even if no federally listed plants, animals or critical habitat are present within your proposed project or may be affected by your proposed project.

Although no longer protected under the Endangered Species Act, be aware that bald eagles are protected under the Bald and Golden Eagle Protection Act (16 U.S.C. 668 *et seq.*) and Migratory Bird Treaty Act (16 U.S.C. 703 *et seq.*), as are golden eagles. Projects affecting these species may require measures to avoid harming eagles or may require a permit. If your project is near an eagle nest or winter roost area, see our Eagle Permits website at <http://www.fws.gov/midwest/midwestbird/EaglePermits/index.html> to help you determine if you can avoid impacting eagles or if a permit may be necessary.

We appreciate your concern for threatened and endangered species. Please include the Consultation Tracking Number in the header of this letter with any request for consultation or correspondence about your project that you submit to our office.

Attachment(s):

- Official Species List

Official Species List

This list is provided pursuant to Section 7 of the Endangered Species Act, and fulfills the requirement for Federal agencies to "request of the Secretary of the Interior information whether any species which is listed or proposed to be listed may be present in the area of a proposed action".

This species list is provided by:

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

(812) 334-4261

Project Summary

Consultation Code: 03E12000-2020-SLI-2117

Event Code: 03E12000-2020-E-08431

Project Name: Bridge Project (Des. No. 1703020) East County Road 300 South over Rider Ditch, Jackson County

Project Type: TRANSPORTATION

Project Description: The Jackson County Commissioners with oversight from the Indiana Department of Transportation (INDOT), and partial funding from Federal Highway Administration (FHWA), intend to proceed with a bridge project on East CR 300 South over Rider Ditch, in Washington Township, Jackson County.

The project is located on East CR 300 South over Rider Ditch, approximately 0.82 mile east of CR 840 E in southeast Jackson County. Specifically, the project is located in Sections 28 and 33, Township 5 North, Range 6 East as illustrated on the Crothersville, Indiana 7.5-minute United States Geological Survey (USGS) topographic quadrangle.

The project is located in a rural agricultural area, and suitable summer habitat does exist within and adjacent to the project area. The scope of work currently proposes the replacement of the existing single-span, steel pony truss bridge with a new, three-span, concrete slab bridge on the same alignment. The existing bridge is 90 feet in length with a wood plank deck.

Approximately 0.03 acre of trees and brush will be removed from the northwest, southwest and southeast quadrants of the bridge crossing to make room for the installation of the new bridge structure. The northeast quadrant has been cleared of trees and consists of mainly low ground covers such as red fescue and red clover and poison ivy. The dominant vegetation observed in the project area consisted of bitternut hickory in the tree stratum, pawpaw and silver maple in the sapling and shrub stratum, yellow wingstem and creeping jenny in the herb stratum.

At this time, it is estimated that less than 0.5 acre of new, additional permanent right-of-way will be required to complete the project. There is no existing, permanent lighting present at the project site, and no new permanent lighting is proposed as part of the bridge replacement project. Temporary lighting could be used during construction at the contractor's discretion; however, due to the rural location of the project it is unlikely

that night-time work would be conducted. The project is scheduled to let in October 2021, and tree clearing activities are anticipated to begin in early 2022, during the inactive season for bats.

On May 20, 2020, the INDOT Seymour District reviewed the U.S. Fish and Wildlife Service database and it was revealed that no endangered bat species have been documented in or within 0.5 mile of the project area. On May 27, 2020 qualified individuals with Metric Environmental inspected Jackson County Bridge No. 154 for bats. No bats were seen or heard in/or under the bridge and no evidence such as guano or urine staining was observed.

Project Location:

Approximate location of the project can be viewed in Google Maps: <https://www.google.com/maps/place/38.83730710286185N85.86614660724364W>



Counties: Jackson, IN

Endangered Species Act Species

There is a total of 2 threatened, endangered, or candidate species on this species list.

Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species. Note that 1 of these species should be considered only under certain conditions.

IPaC does not display listed species or critical habitats under the sole jurisdiction of NOAA Fisheries¹, as USFWS does not have the authority to speak on behalf of NOAA and the Department of Commerce.

See the "Critical habitats" section below for those critical habitats that lie wholly or partially within your project area under this office's jurisdiction. Please contact the designated FWS office if you have questions.

-
1. [NOAA Fisheries](#), also known as the National Marine Fisheries Service (NMFS), is an office of the National Oceanic and Atmospheric Administration within the Department of Commerce.

Mammals

| NAME | STATUS |
|--|------------|
| Indiana Bat <i>Myotis sodalis</i> There is final critical habitat for this species. Your location is outside the critical habitat. Species profile: https://ecos.fws.gov/ecp/species/5949 Species survey guidelines: https://ecos.fws.gov/ipac/guideline/survey/population/1/office/31440.pdf | Endangered |
| Northern Long-eared Bat <i>Myotis septentrionalis</i> No critical habitat has been designated for this species. This species only needs to be considered under the following conditions: <ul style="list-style-type: none"> ▪ Incidental take of the NLEB is not prohibited here. Federal agencies may consult using the 4(d) rule streamlined process. Transportation projects may consult using the programmatic process. See www.fws.gov/midwest/endangered/mammals/nleb/index.html Species profile: https://ecos.fws.gov/ecp/species/9045 | Threatened |

Critical habitats

THERE ARE NO CRITICAL HABITATS WITHIN YOUR PROJECT AREA UNDER THIS OFFICE'S JURISDICTION.

From: [Dye, David](#)
To: [Elayna Stoner](#)
Subject: RE: Des 1703020 _ Jackson County Bridge #154 _ NLAA Determination Review Request
Date: Monday, July 6, 2020 4:39:37 PM
Attachments: [image001.png](#)
[image002.png](#)
[image003.png](#)
[image004.png](#)
[image005.png](#)
[image006.png](#)

Hi Elayna,

I have reviewed and submitted this determination to USFWS for their 14-day review period.

Let me know if you have any additional questions.

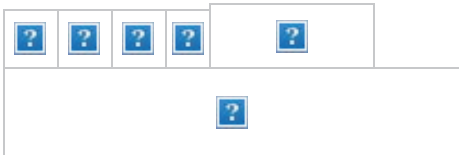
David Dye

Environmental Section Manager

185 Agrico Lane
Seymour, IN 47274

Office: (812) 524-3723

Email: ddye@indot.in.gov



From: Elayna Stoner <elaynas@metricenv.com>
Sent: Monday, July 06, 2020 3:57 PM
To: Dye, David <DDYE@indot.IN.gov>
Subject: Des 1703020 _ Jackson County Bridge #154 _ NLAA Determination Review Request

****** This is an EXTERNAL email. Exercise caution. DO NOT open attachments or click links from unknown senders or unexpected email. ******

David

RE: Jackson County Bridge No. 154
Des 1703020
East CR 300 South over Rider Ditch
0.82 mile east of CR 840 E
Jackson County, IN

Hi there, hope all is well. I've generated a NLAA determination for the Jackson County Bridge 154 project over Rider Ditch. Jackson County Bridge 154 is a Non-Select historic bridge. The preferred alternative at this time is replacement; however, the NEPA process is still ongoing.

The IPaC Record Locator No: 424-22430551 and I've attached the Consistency letter for your

June 4, 2020

Susan Castle
Metric Environmental
6971 Hillsdale Court
Indianapolis, Indiana 46250

Dear Ms. Castle:

The proposed project to address the deteriorating condition of the bridge that carries East County Road 300 South over Rider Ditch in Washington Township, Jackson County, Indiana (Des No 1703020), as referred to in your letter received May 19, 2020 will cause a conversion of prime farmland.

The attached packet of information is for your use completing Parts VI and VII of the AD-1106. After Completion, the federal funding agency needs to forward one copy to NRCS for our records.

If you need additional information, please contact John Allen at 317-295-5859.

Sincerely,

RICK NEILSON
State Soil Scientist

Enclosures



FARMLAND CONVERSION IMPACT RATING

| | | | | | |
|--|--|--|--|------------------------------------|---------------------------------|
| PART I (To be completed by Federal Agency) | | Date Of Land Evaluation Request 5/19/2020 | | | |
| Name of Project DES1703020 CR 300 South Bridge Repl | | Federal Agency Involved Federal Highway Administration | | | |
| Proposed Land Use | | County and State Jackson County, Indiana | | | |
| PART II (To be completed by NRCS) | | Date Request Received By NRCS 5/19/2020 | | Person Completing Form: JRA | |
| Does the site contain Prime, Unique, Statewide or Local Important Farmland? <i>(If no, the FPPA does not apply - do not complete additional parts of this form)</i> | | YES <input checked="" type="checkbox"/> | NO <input type="checkbox"/> | Acres Irrigated | Average Farm Size 303 ac |
| Major Crop(s) Corn | Farmable Land In Govt. Jurisdiction Acres: 259626 % 79 | Amount of Farmland As Defined in FPPA Acres: 190548 % 58 | | | |
| Name of Land Evaluation System Used LESA | Name of State or Local Site Assessment System | Date Land Evaluation Returned by NRCS 6/4/2020 | | | |
| PART III (To be completed by Federal Agency) | | Alternative Site Rating | | | |
| | | Site A | Site B | Site C | Site D |
| A. Total Acres To Be Converted Directly | | 0.10 | | | |
| B. Total Acres To Be Converted Indirectly | | | | | |
| C. Total Acres In Site | | | | | |
| PART IV (To be completed by NRCS) Land Evaluation Information | | | | | |
| A. Total Acres Prime And Unique Farmland | | 0.10 | | | |
| B. Total Acres Statewide Important or Local Important Farmland | | 0.00 | | | |
| C. Percentage Of Farmland in County Or Local Govt. Unit To Be Converted | | <0.001 | | | |
| D. Percentage Of Farmland in Govt. Jurisdiction With Same Or Higher Relative Value | | 46 | | | |
| PART V (To be completed by NRCS) Land Evaluation Criterion Relative Value of Farmland To Be Converted (Scale of 0 to 100 Points) | | 77 | | | |
| PART VI (To be completed by Federal Agency) Site Assessment Criteria <i>(Criteria are explained in 7 CFR 658.5 b. For Corridor project use form NRCS-CPA-106)</i> | | Maximum Points | Site A | Site B | Site C |
| 1. Area In Non-urban Use | | (15) | 15 | | |
| 2. Perimeter In Non-urban Use | | (10) | 10 | | |
| 3. Percent Of Site Being Farmed | | (20) | 15 | | |
| 4. Protection Provided By State and Local Government | | (20) | 0 | | |
| 5. Distance From Urban Built-up Area | | (15) | 10 | | |
| 6. Distance To Urban Support Services | | (15) | 10 | | |
| 7. Size Of Present Farm Unit Compared To Average | | (10) | 10 | | |
| 8. Creation Of Non-farmable Farmland | | (10) | 0 | | |
| 9. Availability Of Farm Support Services | | (5) | 3 | | |
| 10. On-Farm Investments | | (20) | 0 | | |
| 11. Effects Of Conversion On Farm Support Services | | (10) | 0 | | |
| 12. Compatibility With Existing Agricultural Use | | (10) | 0 | | |
| TOTAL SITE ASSESSMENT POINTS | | 160 | 73 | 0 | 0 |
| PART VII (To be completed by Federal Agency) | | | | | |
| Relative Value Of Farmland (From Part V) | | 100 | 77 | 0 | 0 |
| Total Site Assessment (From Part VI above or local site assessment) | | 160 | 73 | 0 | 0 |
| TOTAL POINTS (Total of above 2 lines) | | 260 | 150 | 0 | 0 |
| Site Selected: A | | Date Of Selection | Was A Local Site Assessment Used? YES <input type="checkbox"/> NO <input checked="" type="checkbox"/> | | |
| Reason For Selection: Bridge replacement due to the bridge deck exhibited heavy wear from gravel, an isolated hole with patch, and minor splits. The superstructure exhibited heavy rusting and section loss of the lower connection plates. The substructure showed signs of heavy honeycombing and scaling in the concrete | | | | | |
| Name of Federal agency representative completing this form: Susan Castle, Metric Environmental, LLC | | | | Date: June 11, 2020 | |

(See Instructions on reverse side)



Organization and Project Information

Project ID: 19-0011
Des. ID: 1703020
Project Title: Bridge Project Bridge No. 36-00154
Name of Organization: Metric Environmental, LLC
Requested by: Susan Castle

Environmental Assessment Report

1. Geological Hazards:

- High liquefaction potential
- 1% Annual Chance Flood Hazard

2. Mineral Resources:

- Bedrock Resource: Moderate Potential
- Sand and Gravel Resource: Low Potential

3. Active or abandoned mineral resources extraction sites:

- None documented in the area

*All map layers from Indiana Map (maps.indiana.edu)

DISCLAIMER:

This document was compiled by Indiana University, Indiana Geological Survey, using data believed to be accurate; however, a degree of error is inherent in all data. This product is distributed "AS-IS" without warranties of any kind, either expressed or implied, including but not limited to warranties of suitability to a particular purpose or use. No attempt has been made in either the design or production of these data and document to define the limits or jurisdiction of any federal, state, or local government. The data used to assemble this document are intended for use only at the published scale of the source data or smaller (see the metadata links below) and are for reference purposes only. They are not to be construed as a legal document or survey instrument. A detailed on-the-ground survey and historical analysis of a single site may differ from these data and this document.

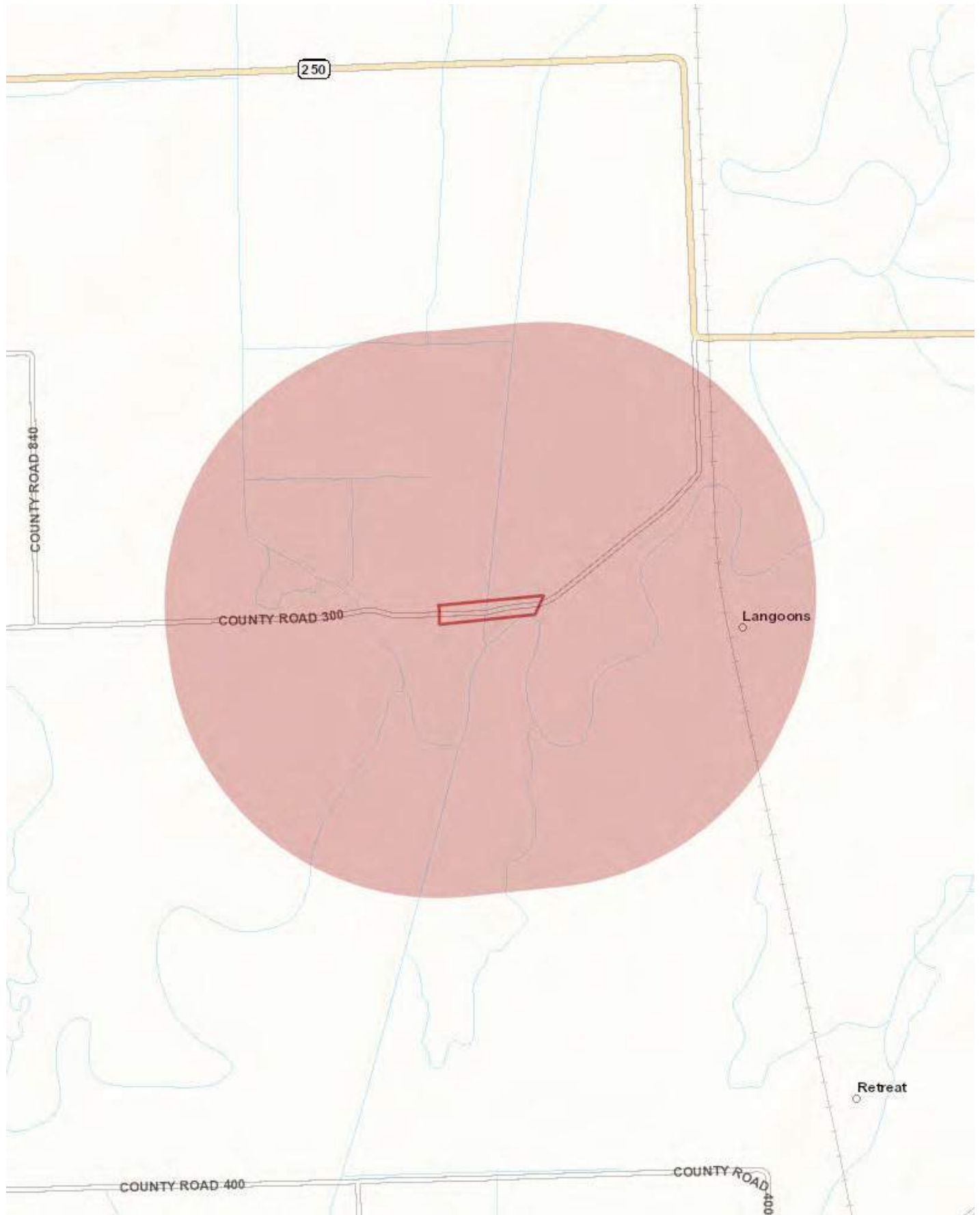
This information was furnished by Indiana Geological Survey

Address: 420 N. Walnut St., Bloomington, IN 47404

Email: IGSEnvir@indiana.edu

Phone: 812 855-7428

Date: May 19, 2020





Indiana Department of Environmental Management

We Protect Hoosiers and Our Environment.

100 North Senate Avenue - Indianapolis, IN 46204
(800) 451-6027 - (317) 232-8603 - www.idem.IN.gov

Jackson County Highway Department
Jerry Ault
360 South CR 25 East
Brownstown, IN 47220
Date May 19, 2020

Metric Environmental, LLC
Susan Castle
6971 Hillside Court
Indianapolis, IN 46250

To Engineers and Consultants Proposing Roadway Construction Projects:

RE: Des. No. 1703020, Bridge Project, Bridge No. 36-00154, East County Road (CR) 300 South over Rider Ditch, 0.82 mile East of CR 840 East, Washington Township, Jackson County, Indiana. The current proposed project would be to replace the existing bridge #36-00154. The existing truss bridge would be removed and relocated if a party comes forward to finance the bridge relocation or will be demolished in place. The new bridge would be a continuous three-span reinforced concrete slab bridge on the same alignment. It is anticipated that less than 0.5 acre of additional right-of-way will be required for this project. The amount of right-of-way will be defined as the design process advances.

This letter from the Indiana Department of Environmental Management (IDEM) serves as a standardized response to enquiries inviting IDEM comments on roadway construction, reconstruction, or other improvement projects within existing roadway corridors when the proposed scope of the project is beneath the threshold requiring a formal National Environmental Policy Act-mandated Environmental Assessment or Environmental Impact Statement. As the letter attempts to address all roadway-related environmental topics of potential concern, it is possible that not every topic addressed in the letter will be applicable to your particular roadway project.

For additional information on specific roadway-related topics of interest, please visit the appropriate Web pages cited below, many of which provide contact information for persons within the various program areas who can answer questions not fully addressed in this letter. Also please be mindful that some environmental requirements may be subject to change and so each person intending to include a copy of this letter in their project documentation packet is advised to download the most recently revised version of the letter; found at: <http://www.in.gov/idem/5283.htm> (<http://www.in.gov/idem/5283.htm>).

To ensure that all environmentally-related issues are adequately addressed, IDEM recommends that you read this letter in its entirety, and consider each of the following issues as you move forward with the planning of your proposed roadway construction, reconstruction, or improvement project:

WATER AND BIOTIC QUALITY

1. Section 404 of the Clean Water Act requires that you obtain a permit from the U.S. Army Corps of Engineers (USACE) before discharging dredged or fill materials into any wetlands or other waters, such as rivers, lakes, streams, and ditches. Other activities regulated include the relocation, channelization, widening, or other such alteration of a stream, and the mechanical clearing (use of heavy construction equipment) of wetlands. Thus, as a project owner or sponsor, it is your responsibility to ensure that no wetlands are

disturbed without the proper permit. Although you may initially refer to the U.S. Fish and Wildlife Service National Wetland Inventory maps as a means of identifying potential areas of concern, please be mindful that those maps do not depict jurisdictional wetlands regulated by the USACE or the Department of Environmental Management. A valid jurisdictional wetlands determination can only be made by the USACE, using the 1987 Wetland Delineation Manual.

USACE recommends that you have a consultant check to determine whether your project will abut, or lie within, a wetland area. To view a list of consultants that have requested to be included on a list posted by the USACE on their Web site, see USACE Permits and Public Notices (<http://www.lrl.usace.army.mil/orf/default.asp>) (<http://www.lrl.usace.army.mil/orf/default.asp>) and then click on "Information" from the menu on the right-hand side of that page. Their "Consultant List" is the fourth entry down on the "Information" page. Please note that the USACE posts all consultants that request to appear on the list, and that inclusion of any particular consultant on the list does not represent an endorsement of that consultant by the USACE, or by IDEM.

Much of northern Indiana (Newton, Lake, Porter, LaPorte, St. Joseph, Elkhart, LaGrange, Steuben, and Dekalb counties; large portions of Jasper, Starke, Marshall, Noble, Allen, and Adams counties; and lesser portions of Benton, White, Pulaski, Kosciusko, and Wells counties) is served by the USACE District Office in Detroit (313-226-6812). The central and southern portions of the state (large portions of Benton, White, Pulaski, Kosciusko, and Wells counties; smaller portions of Jasper, Starke, Marshall, Noble, Allen, and Adams counties; and all other Indiana counties located in north-central, central, and southern Indiana) are served by the USACE Louisville District Office (502-315-6733).

Additional information on contacting these U.S. Army Corps of Engineers (USACE) District Offices, government agencies with jurisdiction over wetlands, and other water quality issues, can be found at <http://www.in.gov/idem/4396.htm> (<http://www.in.gov/idem/4396.htm>). IDEM recommends that impacts to wetlands and other water resources be avoided to the fullest extent.

2. In the event a Section 404 wetlands permit is required from the USACE, you also must obtain a Section 401 Water Quality Certification from the IDEM Office of Water Quality Wetlands Program. To learn more about the Wetlands Program, visit: <http://www.in.gov/idem/4384.htm> (<http://www.in.gov/idem/4384.htm>).
3. If the USACE determines that a wetland or other water body is isolated and not subject to Clean Water Act regulation, it is still regulated by the state of Indiana. A State Isolated Wetland permit from IDEM's Office of Water Quality (OWQ) is required for any activity that results in the discharge of dredged or fill materials into isolated wetlands. To learn more about isolated wetlands, contact the OWQ Wetlands Program at 317-233-8488.
4. If your project will involve over a 0.5 acre of wetland impact, stream relocation, or other large-scale alterations to water bodies such as the creation of a dam or a water diversion, you should seek additional input from the OWQ Wetlands Program staff. Consult the Web at: <http://www.in.gov/idem/4384.htm> (<http://www.in.gov/idem/4384.htm>) for the appropriate staff contact to further discuss your project.
5. Work within the one-hundred year floodway of a given water body is regulated by the Department of Natural Resources, Division of Water. The Division issues permits for activities regulated under the following statutes:
 - o IC 14-26-2 Lakes Preservation Act 312 IAC 11
 - o IC 14-26-5 Lowering of Ten Acre Lakes Act No related code
 - o IC 14-28-1 Flood Control Act 310 IAC 6-1
 - o IC 14-29-1 Navigable Waterways Act 312 IAC 6
 - o IC 14-29-3 Sand and Gravel Permits Act 312 IAC 6

- o IC 14-29-4 Construction of Channels Act No related code

For information on these Indiana (statutory) Code and Indiana Administrative Code citations, see the DNR Web site at: <http://www.in.gov/dnr/water/9451.htm> (<http://www.in.gov/dnr/water/9451.htm>) . Contact the DNR Division of Water at 317-232-4160 for further information.

The physical disturbance of the stream and riparian vegetation, especially large trees overhanging any affected water bodies should be limited to only that which is absolutely necessary to complete the project. The shade provided by the large overhanging trees helps maintain proper stream temperatures and dissolved oxygen for aquatic life.

6. For projects involving construction activity (which includes clearing, grading, excavation and other land disturbing activities) that result in the disturbance of one (1), or more, acres of total land area, contact the Office of Water Quality – Watershed Planning Branch (317/233-1864) regarding the need for of a Rule 5 Storm Water Runoff Permit. Visit the following Web page
 - o <http://www.in.gov/idem/4902.htm> (<http://www.in.gov/idem/4902.htm>)

To obtain, and operate under, a Rule 5 permit you will first need to develop a Construction Plan (<http://www.in.gov/idem/4917.htm#constreq> (<http://www.in.gov/idem/4917.htm#constreq>)), and as described in 327 IAC 15-5-6.5 (<http://www.in.gov/legislative/iac/T03270/A00150> [PDF] (<http://www.in.gov/legislative/iac/T03270/A00150.PDF>), pages 16 through 19). Before you may apply for a Rule 5 Permit, or begin construction, you must submit your Construction Plan to your county Soil and Water Conservation District (SWCD) (<http://www.in.gov/isda/soil/contacts/map.html> (<http://www.in.gov/isda/soil/contacts/map.html>)).

Upon receipt of the construction plan, personnel of the SWCD or the Indiana Department of Environmental Management will review the plan to determine if it meets the requirements of 327 IAC 15-5. Plans that are deemed deficient will require re-submittal. If the plan is sufficient you will be notified and instructed to submit the verification to IDEM as part of the Rule 5 Notice of Intent (NOI) submittal. Once construction begins, staff of the SWCD or Indiana Department of Environmental Management will perform inspections of activities at the site for compliance with the regulation.

Please be mindful that approximately 149 Municipal Separate Storm Sewer System (MS4) areas are now being established by various local governmental entities throughout the state as part of the implementation of Phase II federal storm water requirements. All of these MS4 areas will eventually take responsibility for Construction Plan review, inspection, and enforcement. As these MS4 areas obtain program approval from IDEM, they will be added to a list of MS4 areas posted on the IDEM Website at: <http://www.in.gov/idem/4900.htm> (<http://www.in.gov/idem/4900.htm>).

If your project is located in an IDEM-approved MS4 area, please contact the local MS4 program about meeting their storm water requirements. Once the MS4 approves the plan, the NOI can be submitted to IDEM.

Regardless of the size of your project, or which agency you work with to meet storm water requirements, IDEM recommends that appropriate structures and techniques be utilized both during the construction phase, and after completion of the project, to minimize the impacts associated with storm water runoff. The use of appropriate planning and site development and appropriate storm water quality measures are recommended to prevent soil from leaving the construction site during active land disturbance and for post construction water quality concerns. Information and assistance regarding storm water related to construction activities are available from the Soil and Water Conservation District (SWCD) offices in each county or from IDEM.

7. For projects involving impacts to fish and botanical resources, contact the Department of Natural Resources - Division of Fish and Wildlife (317/232-4080) for addition project input.
8. For projects involving water main construction, water main extensions, and new public water supplies, contact the Office of Water Quality - Drinking Water Branch (317-308-3299) regarding the need for permits.
9. For projects involving effluent discharges to waters of the State of Indiana , contact the Office of Water Quality - Permits Branch (317-233-0468) regarding the need for a National Pollutant Discharge Elimination System (NPDES) permit.
10. For projects involving the construction of wastewater facilities and sewer lines, contact the Office of Water Quality - Permits Branch (317-232-8675) regarding the need for permits.

AIR QUALITY

The above-noted project should be designed to minimize any impact on ambient air quality in, or near, the project area. The project must comply with all federal and state air pollution regulations. Consideration should be given to the following:

1. Regarding open burning, and disposing of organic debris generated by land clearing activities; some types of open burning are allowed (<http://www.in.gov/idem/4148.htm> (<http://www.in.gov/idem/4148.htm>)) under specific conditions. You also can seek an open burning variance from IDEM.

However, IDEM generally recommends that you take vegetative wastes to a registered yard waste composting facility or that the waste be chipped or shredded with composting on site (you must register with IDEM if more than 2,000 pounds is to be composted; contact 317/232-0066). The finished compost can then be used as a mulch or soil amendment. You also may bury any vegetative wastes (such as leaves, twigs, branches, limbs, tree trunks and stumps) onsite, although burying large quantities of such material can lead to subsidence problems, later on.

Reasonable precautions must be taken to minimize fugitive dust emissions from construction and demolition activities. For example, wetting the area with water, constructing wind barriers, or treating dusty areas with chemical stabilizers (such as calcium chloride or several other commercial products). Dirt tracked onto paved roads from unpaved areas should be minimized.

Additionally, if construction or demolition is conducted in a wooded area where blackbirds have roosted or abandoned buildings or building sections in which pigeons or bats have roosted for 3-5 years precautionary measures should be taken to avoid an outbreak of histoplasmosis. This disease is caused by the fungus *Histoplasma capsulatum*, which stems from bird or bat droppings that have accumulated in one area for 3-5 years. The spores from this fungus become airborne when the area is disturbed and can cause infections over an entire community downwind of the site. The area should be wetted down prior to cleanup or demolition of the project site. For more detailed information on histoplasmosis prevention and control, please contact the Acute Disease Control Division of the Indiana State Department of Health at (317) 233-7272.

2. The U.S. EPA and the Surgeon General recommend that people not have long-term exposure to radon at levels above 4 pCi/L. (For a county-by-county map of predicted radon levels in Indiana, visit: <http://www.in.gov/idem/4145.htm> (<http://www.in.gov/idem/4145.htm>).

The U.S. EPA further recommends that all homes (and apartments within three stories of ground level) be tested for radon. If in-home radon levels are determined to be 4 pCi/L, or higher, EPA recommends a follow-up test. If the second test confirms that radon levels are 4 pCi/L, or higher, EPA recommends the installation

of radon-reduction measures. (For a list of qualified radon testers and radon mitigation (or reduction) specialists visit: http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon_testers_mitigators_list.pdf (http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon_testers_mitigators_list.pdf.) It also is recommended that radon reduction measures be built into all new homes, particularly in areas like Indiana that have moderate to high predicted radon levels.

To learn more about radon, radon risks, and ways to reduce exposure visit:

<http://www.in.gov/isdh/regsvcs/radhealth/radon.htm> (<http://www.in.gov/isdh/regsvcs/radhealth/radon.htm>), <http://www.in.gov/idem/4145.htm> (<http://www.in.gov/idem/4145.htm>), or <http://www.epa.gov/radon/index.html> (<http://www.epa.gov/radon/index.html>).

3. With respect to asbestos removal: all facilities slated for renovation or demolition (except residential buildings that have (4) four or fewer dwelling units and which will not be used for commercial purposes) must be inspected by an Indiana-licensed asbestos inspector prior to the commencement of any renovation or demolition activities. If regulated asbestos-containing material (RACM) that may become airborne is found, any subsequent demolition, renovation, or asbestos removal activities must be performed in accordance with the proper notification and emission control requirements.

If no asbestos is found where a renovation activity will occur, or if the renovation involves removal of less than 260 linear feet of RACM off of pipes, less than 160 square feet of RACM off of other facility components, or less than 35 cubic feet of RACM off of all facility components, the owner or operator of the project does not need to notify IDEM before beginning the renovation activity.

For questions on asbestos demolition and renovation activities, you can also call IDEM's Lead/Asbestos section at 1-888-574-8150.

However, in all cases where a demolition activity will occur (even if no asbestos is found), the owner or operator must still notify IDEM 10 working days prior to the demolition, using the form found at <http://www.in.gov/icpr/webfile/formsdiv/44593.pdf> (<http://www.in.gov/icpr/webfile/formsdiv/44593.pdf>).

Anyone submitting a renovation/demolition notification form will be billed a notification fee based upon the amount of friable asbestos containing material to be removed or demolished. Projects that involve the removal of more than 2,600 linear feet of friable asbestos containing materials on pipes, or 1,600 square feet or 400 cubic feet of friable asbestos containing material on other facility components, will be billed a fee of \$150 per project; projects below these amounts will be billed a fee of \$50 per project. All notification remitters will be billed on a quarterly basis.

For more information about IDEM policy regarding asbestos removal and disposal, visit:

<http://www.in.gov/idem/4983.htm> (<http://www.in.gov/idem/4983.htm>).

4. With respect to lead-based paint removal: IDEM encourages all efforts to minimize human exposure to lead-based paint chips and dust. IDEM is particularly concerned that young children exposed to lead can suffer from learning disabilities. Although lead-based paint abatement efforts are not mandatory, any abatement that is conducted within housing built before January 1, 1978, or a child-occupied facility is required to comply with all lead-based paint work practice standards, licensing and notification requirements. For more information about lead-based paint removal visit: <http://www.in.gov/isdh/19131.htm> (<http://www.in.gov/isdh/19131.htm>).
5. Ensure that asphalt paving plants are permitted and operate properly. The use of cutback asphalt, or asphalt emulsion containing more than seven percent (7%) oil distillate, is prohibited during the months April through October. See 326 IAC 8-5-2, Asphalt Paving Rule

(<http://www.ai.org/legislative/iac/T03260/A00080.PDF>
(<http://www.ai.org/legislative/iac/T03260/A00080.PDF>)).

6. If your project involves the construction of a new source of air emissions or the modification of an existing source of air emissions or air pollution control equipment, it will need to be reviewed by the IDEM Office of Air Quality (OAQ). A registration or permit may be required under 326 IAC 2 (View at: www.ai.org/legislative/iac/t03260/a00020.pdf (<http://www.ai.org/legislative/iac/t03260/a00020.pdf>)). New sources that use or emit hazardous air pollutants may be subject to Section 112 of the Clean Air Act and corresponding state air regulations governing hazardous air pollutants.
7. For more information on air permits visit: <http://www.in.gov/idem/4223.htm> (<http://www.in.gov/idem/4223.htm>), or to initiate the IDEM air permitting process, please contact the Office of Air Quality Permit Reviewer of the Day at (317) 233-0178 or OAMPROD at adem.state.in.us.

LAND QUALITY

In order to maintain compliance with all applicable laws regarding contamination and/or proper waste disposal, IDEM recommends that:

1. If the site is found to contain any areas used to dispose of solid or hazardous waste, you need to contact the Office of Land Quality (OLQ) at 317-308-3103.
2. All solid wastes generated by the project, or removed from the project site, need to be taken to a properly permitted solid waste processing or disposal facility. For more information, visit <http://www.in.gov/idem/4998.htm> (<http://www.in.gov/idem/4998.htm>).
3. If any contaminated soils are discovered during this project, they may be subject to disposal as hazardous waste. Please contact the OLQ at 317-308-3103 to obtain information on proper disposal procedures.
4. If PCBs are found at this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding management of any PCB wastes from this site.
5. If there are any asbestos disposal issues related to this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding the management of asbestos wastes (Asbestos removal is addressed above, under Air Quality).
6. If the project involves the installation or removal of an underground storage tank, or involves contamination from an underground storage tank, you must contact the IDEM Underground Storage Tank program at 317/308-3039. See: <http://www.in.gov/idem/4999.htm> (<http://www.in.gov/idem/4999.htm>).

FINAL REMARKS

Should you need to obtain any environmental permits in association with this proposed project, please be mindful that IC 13-15-8 requires that you notify all adjoining property owners and/or occupants within ten days your submittal of each permit application. However, if you are seeking multiple permits, you can still meet the notification requirement with a single notice if all required permit applications are submitted with the same ten day period.

Should the scope of the proposed project be expanded to the extent that a National Environmental Policy Act Environmental Assessment (EA) or Environmental Impact Statement (EIS) is required, IDEM will actively participate in any early interagency coordination review of the project.

Meanwhile, please note that this letter does not constitute a permit, license, endorsement or any other form of approval on the part of the Indiana Department of Environmental Management regarding any project for which a copy of this letter is used. Also note that it is the responsibility of the project engineer or consultant using this letter to ensure that the most current draft of this document, which is located at <http://www.in.gov/idem/5284.htm> (<http://www.in.gov/idem/5284.htm>), is used.

Signature(s) of the Applicant

I acknowledge that the following proposed roadway project will be financed in part, or in whole, by public monies.

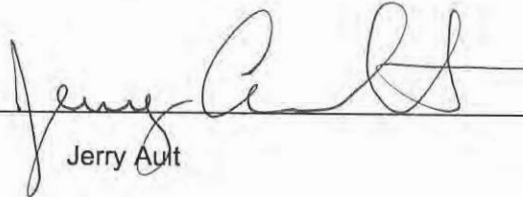
Project Description

Des. No. 1703020, Bridge Project, Bridge No. 36-00154, East County Road (CR) 300 South over Rider Ditch, 0.82 mile East of CR 840 East, Washington Township, Jackson County, Indiana. The current proposed project would be to replace the existing bridge #36-00154. The existing truss bridge would be removed and relocated if a party comes forward to finance the bridge relocation or will be demolished in place. The new bridge would be a continuous three-span reinforced concrete slab bridge on the same alignment. It is anticipated that less than 0.5 acre of additional right-of-way will be required for this project. The amount of right-of-way will be defined as the design process advances.

With my signature, I do hereby affirm that I have read the letter from the Indiana Department of Environment that appears directly above. In addition, I understand that in order to complete that project in which I am interested, with a minimum of impact to the environment, I must consider all the issues addressed in the aforementioned letter, and further, that I must obtain any required permits.

Date: 7-28-2020

Signature of the INDOT
Project Engineer or Other Responsible Agent


Jerry Ault

Date: 7-27-2020

Signature of the
For Hire Consultant

Susan Castle

Susan Castle

Susan Castle

Subject: FW: [CAUTION: Suspicious Link]Agency Early Coordination Letter, Des. No. 1703020, Bridge Project E CR 300 South over Rider Ditch, 0.82 mile east of CR 840 E, Washington Township, Jackson County, Indiana

From: Amick, Kevin R -FS <kevin.amick@usda.gov>

Sent: Tuesday, May 19, 2020 5:54 PM

To: Susan Castle <susanc@metricenv.com>

Subject: RE: [CAUTION: Suspicious Link]Agency Early Coordination Letter, Des. No. 1703020, Bridge Project E CR 300 South over Rider Ditch, 0.82 mile east of CR 840 E, Washington Township, Jackson County, Indiana

Susan,

This project isn't near the Hoosier National Forest; thus, we have no issues or comments. Thanks.



Kevin Amick
Environmental Coordinator
Forest Service
Hoosier National Forest

p: 812-276-4746

f: 812-279-3423

kevin.amick@usda.gov

811 Constitution Ave.

Bedford, IN 47421

www.fs.fed.us



Caring for the land and serving people

Susan Castle

Subject: FW: [EXTERNAL] Agency Early Coordination Letter, Des. No. 1703020, Bridge Project E CR 300 South over Rider Ditch, 0.82 mile east of CR 840 E, Washington Township, Jackson County, Indiana

From: McWilliams, Robin <robin_mcwilliams@fws.gov>

Sent: Wednesday, June 10, 2020 11:46 AM

To: Susan Castle <susanc@metricenv.com>

Subject: Re: [EXTERNAL] Agency Early Coordination Letter, Des. No. 1703020, Bridge Project E CR 300 South over Rider Ditch, 0.82 mile east of CR 840 E, Washington Township, Jackson County, Indiana

Dear Susan,

This responds to your recent letter requesting our comments on the aforementioned project.

These comments have been prepared under the authority of the Fish and Wildlife Coordination Act (16 U.S.C. 661 et. seq.) and are consistent with the intent of the National Environmental Policy Act of 1969, the Endangered Species Act of 1973, and the U. S. Fish and Wildlife Service's Mitigation Policy.

The project is within the range of the Indiana bat (*Myotis sodalis*) and northern long-eared bat (*Myotis septentrionalis*) and should follow the new Indiana bat/northern long-eared bat programmatic consultation process, if applicable (i.e. a federal transportation nexus is established). The Service has 14 days after a "Not Likely to Adversely Affect" determination letter is generated to review the project and provide additional comments or request additional information; if you do not receive a response from us within 14 days, we have no additional comments.

Based on a review of the information you provided, the U.S. Fish and Wildlife Service has no other comments on the project as currently proposed. However, should new information arise pertaining to project plans or a revised species list be published, it will be necessary for the Federal agency to reinitiate consultation. Standard recommendations are provided below.

We appreciate the opportunity to comment at this early stage of project planning. If you have any questions about our recommendations, please call (812) 334-4261 x. 207.

Sincerely,
Robin McWilliams Munson

Standard Recommendations:

1. Do not clear trees or understory vegetation outside the construction zone boundaries. **(This restriction is not related to the "tree clearing" restriction for potential Indiana Bat habitat.)**
2. Restrict below low-water work in streams to placement of culverts, piers, pilings and/or footings, shaping of the spill slopes around the bridge abutments, and placement of riprap. Culverts should span the active stream channel, should be either embedded or a 3-sided or open-arch culvert, and be installed where practicable on an essentially flat slope. When an open-bottom culvert or arch is used in a stream, which has a good natural bottom substrate, such as gravel, cobbles and boulders, the existing substrate should be left undisturbed beneath the culvert to provide natural habitat for the aquatic community.

3. Restrict channel work and vegetation clearing to the minimum necessary for installation of the stream crossing structure.
4. Minimize the extent of hard armor (riprap) in bank stabilization by using bioengineering techniques whenever possible. If riprap is utilized for bank stabilization, extend it below low-water elevation to provide aquatic habitat.
5. Implement temporary erosion and sediment control methods within areas of disturbed soil. All disturbed soil areas upon project completion will be vegetated following INDOT's standard specifications.
6. Avoid all work within the inundated part of the stream channel (in perennial streams and larger intermittent streams) during the fish spawning season (April 1 through June 30), except for work within sealed structures such as caissons or cofferdams that were installed prior to the spawning season. No equipment shall be operated below Ordinary High Water Mark during this time unless the machinery is within the caissons or on the cofferdams.
7. Evaluate wildlife crossings under bridge/culverts projects in appropriate situations. Suitable crossings include flat areas below bridge abutments with suitable ground cover, high water shelves in culverts, amphibian tunnels and diversion fencing

Robin McWilliams Munson
Fish and Wildlife Biologist
U.S. Fish and Wildlife Service
620 South Walker Street
Bloomington, IN 46142
812-334-4261

Mon-Tues 8-3:30p
Wed-Thurs 8:30-3p Telework

From: Susan Castle <susanc@metricenv.com>
Sent: Tuesday, May 19, 2020 4:02 PM
To: 'michelle.allen@dot.gov' <michelle.allen@dot.gov>; 'erica.tait@dot.gov' <erica.tait@dot.gov>; environmentalreview@dnr.in.gov <environmentalreview@dnr.in.gov>; Clark, Rickie <RCLARK@indot.IN.gov>; 'paul.j.lehmann@hud.gov' <paul.j.lehmann@hud.gov>; Santiago, Hector R <Hector_Santiago@nps.gov>; David Dye (ddye@indot.in.gov) <ddye@indot.in.gov>; McWilliams, Robin <robin_mcwilliams@fws.gov>; 'kamick@fs.fed.us' <kamick@fs.fed.us>; 'rick.neilson@in.usda.gov' <rick.neilson@in.usda.gov>; CELRL.Door.To.The.Corps@USACE.army.mil <CELRL.Door.To.The.Corps@USACE.army.mil>; 'dblann@jacksoncounty.in.gov' <dblann@jacksoncounty.in.gov>; 'jault@jacksoncounty.in.gov' <jault@jacksoncounty.in.gov>; ema@jacksoncounty.in.gov <ema@jacksoncounty.in.gov>; 'drew@drewmarkel.com' <drew@drewmarkel.com>; 'auditor@jacksoncounty.in.gov' <auditor@jacksoncounty.in.gov>
Cc: Matern, Jeff <JMatern@jsengr.com>
Subject: [EXTERNAL] Agency Early Coordination Letter, Des. No. 1703020, Bridge Project E CR 300 South over Rider Ditch, 0.82 mile east of CR 840 E, Washington Township, Jackson County, Indiana

Dear Interested Agency,

Metric Environmental is preparing the Categorical Exclusion document for the above referenced project.

The attached letter is part of the early coordination phase of the environmental review process. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Should we not receive your response within thirty (30) calendar days from the date of this letter, it will be assumed that your agency believes that there will be no adverse effects incurred as a result of the proposed project.

Thank you very much

APPENDIX D
**Section 106 of the National Historic
Preservation Act**

**FEDERAL HIGHWAY ADMINISTRATION'S
SECTION 4(F) COMPLIANCE REQUIREMENTS (for historic properties) AND
SECTION 106 FINDINGS AND DETERMINATIONS
AREA OF POTENTIAL EFFECT
ELIGIBILITY DETERMINATIONS
EFFECT FINDING
JACKSON COUNTY BRIDGE 154 CARRYING CR 300S OVER RIDER DITCH
WASHINGTON TOWNSHIP, JACKSON COUNTY, INDIANA
DES NO.: 1703020**

AREA OF POTENTIAL EFFECT

(Pursuant to 36 CFR 800.4(a)(1))

The Area of Potential Effects (APE) is the area in which an undertaking may cause direct or indirect changes in the character or use of an historic property. The APE was developed with regard to the potential scope of the project, which concerns Jackson County Bridge 154 (Indiana Historic Bridge #0742/NBI #3600099). The established APE consists of a 0.25 (1,350 feet) mile radius around the bridge. Please see Appendix B for a map of the APE.

ELIGIBILITY DETERMINATIONS

(Pursuant to 36 CFR 800.4(c)(2))

There are no historic properties listed in the National Register of Historic Places (NRHP) within the APE. The APE contains one NRHP-eligible property:

Jackson County Bridge 154 (Indiana Historic Bridge #0742/NBI #3600099):

This single-span steel Pratt pony truss bridge is 90 feet long with a roadway width of 15.4 feet. It was built in 1910 with repairs made to it in 1987, and it was rehabilitated in 1992 and 2008. The bridge was determined eligible for inclusion in the NRHP by the Indiana Department of Transportation's (INDOT) *Indiana Historic Bridge Inventory* (IHBI) under Criterion C as an early or distinctive phase in bridge construction, design, or engineering. Based upon the methodology used by the IHBI, however, the bridge was determined not to be an excellent example of its type and/or suitable for preservation, and therefore it was listed as a "Non-Select" bridge in the IHBI.

No other resources within the APE are recommended NRHP eligible.

EFFECT FINDING

Per the terms of the "Programmatic Agreement Regarding Management and Preservation of Indiana's Historic Bridges" (Historic Bridges PA), the Federal Highway Administration—Indiana Division (FHWA) will satisfy its Section 106 responsibilities involving "Select" and "Non-Select" bridges through the Project Development Process (PDP) of the Historic Bridges PA (Stipulation III). Jackson County Bridge 154 has been classified as a Non-Select Bridge by the INDOT Historic Bridge Inventory, and thus, the procedures outlined in Stipulation III.B of the Historic Bridges PA will be followed to fulfill FHWA's Section 106 responsibilities for the bridge.

Therefore, the finding for this project only applies to other resources located within the APE and not Jackson County Bridge 154. This document will satisfy the Section 106 responsibilities for other resources located in the APE.

Regarding other resources located in the project area, INDOT, acting on FHWA's behalf, has determined a "No Historic Properties Affected" finding is appropriate for this undertaking. INDOT respectfully requests the Indiana State Historic Preservation Officer (IN SHPO) provide written concurrence with the Section 106 determination of effect.

SECTION 4(f) COMPLIANCE REQUIREMENTS (for historic properties)

Jackson County Bridge 154 - This resource is used for transportation purposes. Jackson County Bridge 154 will be evaluated through the *Programmatic Section 4(f) Evaluation and Approval for FHWA Projects that Necessitate the Use of Historic Bridges*.

Anuradha V. Kumar

Anuradha V. Kumar, for FHWA
Manager
INDOT Cultural Resources Office

05/26/2020

Approved Date

**FEDERAL HIGHWAY ADMINISTRATION'S
DOCUMENTATION OF SECTION 106 FINDINGS OF
NO HISTORIC PROPERTIES AFFECTED
SUBMITTED TO THE STATE HISTORIC PRESERVATION OFFICER
PURSUANT TO 36 CFR 800.4(d)(1)
JACKSON COUNTY BRIDGE 154 CARRYING CR 300S OVER RIDER DITCH
WASHINGTON TOWNSHIP, JACKSON COUNTY, INDIANA
DES NO.: 1703020**

1. DESCRIPTION OF THE UNDERTAKING

Jackson County Commissioners, with funding from the Federal Highway Administration (FHWA) and administration from the Indiana Department of Transportation (INDOT) are proposing to utilize federal funding for a project for Jackson County Bridge 154 (Indiana Historic Bridge #0742/NBI #3600099). The project is located in Jackson County near the town of Crothersville in Washington Township on County Road (CR) 300 South (S) over Rider Ditch. It can be found on the *Crothersville, Indiana* USGS Topographic Quadrangle maps in Township 5 North, Range 6 East, in Sections 28 and 29.

The need for this project is due to advanced deterioration and vandalization of Jackson County Bridge #154. The latest bridge inspection report (March 2019) rated the bridge as structurally deficient and therefore unsafe. Per the bridge inspection report, the bridge has suffered severe corrosion and up to 85% section loss on the plates connecting the intermediate vertical posts and floor beams, including one location where the plate is completely corroded through. On the bridge's west approach vandals have removed the bottom chord I-bar along one panel. Temporary repairs were made at that time to the bridge. Due to the advanced section loss, the bridge was closed in March 2019.

The primary purpose of this project is to restore service to East County Road 300 South over Rider Ditch.

Multiple alternatives are under consideration, including a no build alternative; two rehabilitation options to the bridge for continued vehicular use; two rehabilitation options that would rehabilitate the bridge for non-vehicular use and build a bypass adjacent to it; a relocation alternative, which would require a third party to adopt the bridge and move it at their cost; and replacement of the bridge and construction of a new one on the same alignment.

The project terminus points would be 408 feet west of the bridge and 616 east of the bridge. Acquisition of land for permanent right-of-way is anticipated to be 0.5 acre or less throughout the project limits. County Road 300S, which has been closed to traffic in the project area since March 2019, will continue to be closed to traffic during construction.

Area of Potential Effects (APE): This is a federally funded project that requires coordination with the FHWA as required by the Section 106 process. Per 36 CFR 800.9(a), the APE is defined as the "geographic area or areas within which an undertaking may directly or indirectly cause alterations in the character or use of historic properties, if any such properties exist." The APE boundary is a

circle centered upon the subject bridge from which the radii extend no less than 0.25 mile (1350 feet) in every direction. A map illustrating the APE limits as described is provided in Appendix B.

2. EFFORTS TO IDENTIFY HISTORIC PROPERTIES

A) Historic Properties Report

Pursuant to 36 CFR 800.4(b), Sue Becher Gilliam from Metric Environmental, LLC initiated identification efforts in January 2020 by reviewing the National Register of Historic Places (NRHP), the Indiana Historic Sites and Structures Inventory (IHSSI), the Indiana State Historic Architectural and Archaeological Research Database (SHAARD) and the Indiana Historic Buildings, Bridges, and Cemeteries Map, the *Indiana Historic Bridge Inventory*, the Indiana Historical Bureau's Historical Markers Database, and the 1988 *Jackson County Interim Historic Sites and Structures Inventory* (IHSSI) for previously-identified properties. Primary and secondary documentary research included numerous published county and local histories, historical and current atlases and maps, and online resources. Additionally in January 2020 Ms. Becher Gilliam conducted a field survey by walking all the areas within the APE and taking photographs in an effort to identify and evaluate any historic resources present. Ms. Becher Gilliam then completed a Historic Property Short Report (Becher Gilliam, 2/21/2020) and provided recommendations concerning the historic significance of the properties within the APE. As a result of identification and evaluation efforts for this project, no properties within the project APE were recommended eligible for listing in the NRHP. The HPSR was submitted to the CRO for review on January 31, 2010, and on February 24, 2020 their office released the document for consulting party review. Please refer to Appendix A: Project Site Photographs and Appendix C: Report Summaries.

B) Archaeological Survey

Archaeologist Samuel Snell from Metric Environmental, LLC conducted a Phase IA archaeological reconnaissance in January 2020. Their survey area encompassed 2.4 acres, and included shovel probes, a pedestrian walkover survey and visual inspections. No artifacts were recorded in the shovel probes excavated. The archaeologists also conducted a literature review at the Department of Historic Preservation and Archaeology (DHPA) to review previously recorded sites. No such sites were recorded adjacent to the project area. The archaeologist prepared a Phase Ia Archaeological Records and Reconnaissance Survey report (Snell, 1/31/2020) that noted no archaeological resources are in the project area, and recommended the project proceed as planned. The archaeology report was submitted to the CRO for review on January 31, 2010, and on February 24, 2020 their office released the document for consulting party review. Please refer to Appendix C: Report Summaries.

C) Consultation

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. In accordance with 36 CFR 800.2(c), individuals and groups with a demonstrated interest in the undertaking were invited to participate in efforts to identify historic properties potentially affected by the undertaking, assess its effects, and seek ways to avoid, minimize or mitigate any adverse effects on historic properties.

On November 7, 2019 the following individuals and groups listed in the table below were sent an email on behalf of INDOT requesting them to act as a consulting party for the undertaking. They were also advised that the Early Coordination Letter was available for review at the INDOT’s Section 106 Consultation and Outreach Portal Enterprise, known as INSCOPE. The invitees were requested to respond within 30 days indicating whether the agency agreed or did not agree to participate as a consulting party. Also on November 7, 2019 the INDOT Cultural Resources Office emailed the Native American Tribes listed in the table to invite them to be consulting parties, and to direct them to the documents available for review on INSCOPE.

It was noted in the email correspondence that if no response was provided, the individual or group would not be considered a consulting party and would not receive further information about the undertaking unless the scope changed.

| Invited Section 106 Consulting Parties | Status |
|--|------------------------------|
| Indiana Landmarks, Southern Regional Office | No Response - Declined |
| Jackson County Historical Center | No Response - Declined |
| Bob Gillaspy, Jackson County Commissioner | No Response - Declined |
| Drew Markel, Jackson County Commissioner | No Response - Declined |
| Matt Reedy, Jackson County Commissioner | No Response - Declined |
| Bill Day, Jackson County Historian | No Response - Declined |
| Paul Brandenburg, Historic Spans Task Force | No Response - Declined |
| Dr. James Cooper | No Response - Declined |
| Tony Dillon, Historic Hoosier Bridges | No Response - Declined |
| Nathan Holth, Historicbridges.org | No Response - Declined |
| Kitty Henderson, Historic Bridge Foundation | No Response - Declined |
| Fleeta Arthur, Jackson County Parks and Recreation | No Response - Declined |
| Eastern Shawnee Tribe of Oklahoma | No Response - Declined |
| Miami Tribe of Oklahoma | Accepted – December 10, 2019 |
| Peoria Tribe of Indians of Oklahoma | No Response - Declined |
| Pokagon Band of Potawatomi Indians | No Response - Declined |
| Delaware Tribe of Indians, Oklahoma | No Response - Declined |

The Indiana Department of Natural Resources, Division of Archaeology and Historic Preservation (SHPO) is automatically considered a consulting party for federally funded transportation projects due to its mandatory or designated role as specified in 36 C.F.R. § 800.2. The SHPO was sent a hard copy of all materials on November 12, 2019.

In an email dated December 10, 2019 the Miami Tribe of Oklahoma accepted the consulting party invitation. They also stated they were not aware of existing documentation directly linking Miami culture or historic sites to the project site, but if any human remains or Native American cultural items falling under the Native American Graves Protection and Repatriation Act (NAGPRA) or archaeological evidence was discovered during any phase of the project, they asked to be immediately consulted.

In a letter dated December 16, 2019 the SHPO commented upon the submitted materials by stating they were not aware of any other parties who should be invited to participate in this Section 106 consultation.

In January 2020, Fleeta Arthur of Jackson County Parks and Recreation made a phone inquiry to SHPO staff regarding truss bridges in Jackson County that may be proposed for replacement. The inquiry was forwarded to INDOT Cultural Resources Office staff, who responded to Ms. Arthur via email on January 10, 2020. On the same date, SHPO staff also encouraged Ms. Arthur via email to let INDOT know if she wanted to become a consulting party for the Des. No. 1703020 project. No response was received from Ms. Arthur.

On February 25, 2020 the Miami Tribe of Oklahoma and the SHPO -- consulting parties who had accepted the invitation to participate -- were sent a letter to notify them that the Historic Property Short Report and the Archaeology Report were available for review on INSCOPE. They were asked to reply with comments within 30 days.

On March 30, 2020 the SHPO agreed with the size of the APE for the project, and stated they agreed with the recommendations of the Historic Property Short Report that Jackson County Bridge 154 (NBI #3600099) is the only above ground NRHP eligible property, but that it is listed as a “Non-Select” bridge. The SHPO also agreed with the archaeologist’s recommendation that no further archaeological investigations appear to be necessary. Consulting party correspondence is presented in Appendix D.

Per the terms of the “Programmatic Agreement Regarding Management and Preservation of Indiana’s Historic Bridges” (Historic Bridges PA), the FHWA will satisfy its Section 106 responsibilities involving “Select” and “Non-Select” bridges through the Project Development Process (PDP) of the Historic Bridges PA (Stipulation III). Jackson County Bridge 154 has been classified as a “Non-Select” bridge in the Indiana Historic Bridge Inventory and, thus, the procedures outlined in Stipulation III.B of the Historic Bridges PA will be followed to fulfill FHWA’s Section 106 responsibilities for the bridge. The standard treatment approach described in Attachment B of the Historic Bridges PA (Standard Treatment Approach for Historic Bridges) will be followed.

As per the Historic Bridges PA, a Historic Bridge Alternatives Analysis (HBAA) will be completed and submitted to INDOT-CRO. Once INDOT-CRO approves the HBAA to be released for review it will be distributed to the consulting parties.

The bridge is being marketed for rehabilitation and reuse, or for the salvage of elements of the bridge by an interested party, in accordance with the Historic Bridges PA. An advertisement was placed in the *Seymour Tribune* newspaper and on the INDOT Historic Bridges Marketing Program website on November 15, 2019. The bridge advertisement notification signs were posted on November 19, 2019. An advertisement was placed in the *Indianapolis Star* newspaper on May 1, 2020. To date no interested parties have come forward to take ownership of Jackson County Bridge 154. The marketing period will end when the public hearing comment period ends (see Appendix E for marketing documentation). Depending on the preferred scope of the project from the HBAA, Jackson County will coordinate with the SHPO as appropriate to determine if any photo

documentation of the bridge will be required per the Historic Bridges PA.

Per Stipulation III of the Historic Bridges PA, the project sponsor will hold a public hearing for the project prior to completion of National Environmental Policy Act (NEPA) studies and all consulting parties will be notified of the public hearing.

3. BASIS FOR FINDING

The APE contains one property previously determined eligible for the NRHP: Jackson County Bridge 154 carrying CR 300S over Rider Ditch. However, as mentioned above, the FHWA will satisfy its Section 106 responsibilities involving “Select” and “Non-Select” bridges by following the PDP of the Historic Bridges PA (Stipulation III). The finding for this project only applies to other resources located within the APE and not Jackson County Bridge 154.

Because no historic properties were identified within the project’s APE, a Finding of “No Historic Properties Affected” has been made for this undertaking.

A) Continued Consultation

INDOT’s Findings, made on behalf of FHWA, and supporting Section 800.11(d) documentation are hereby provided to the SHPO and consulting parties for a final 30-day consultation/comment period. Views of the public are concurrently being sought through publication of the findings in a locally available widely circulated newspaper.

APPENDICES

Appendix A: Key Map and Project Site Photographs

Appendix B: Project Location Maps and APE

Appendix C: Report Summaries

Appendix D: Consulting Parties’ Correspondence

Appendix E: Bridge Marketing Documentation

Appendix A: Key Map and Project Site Photographs

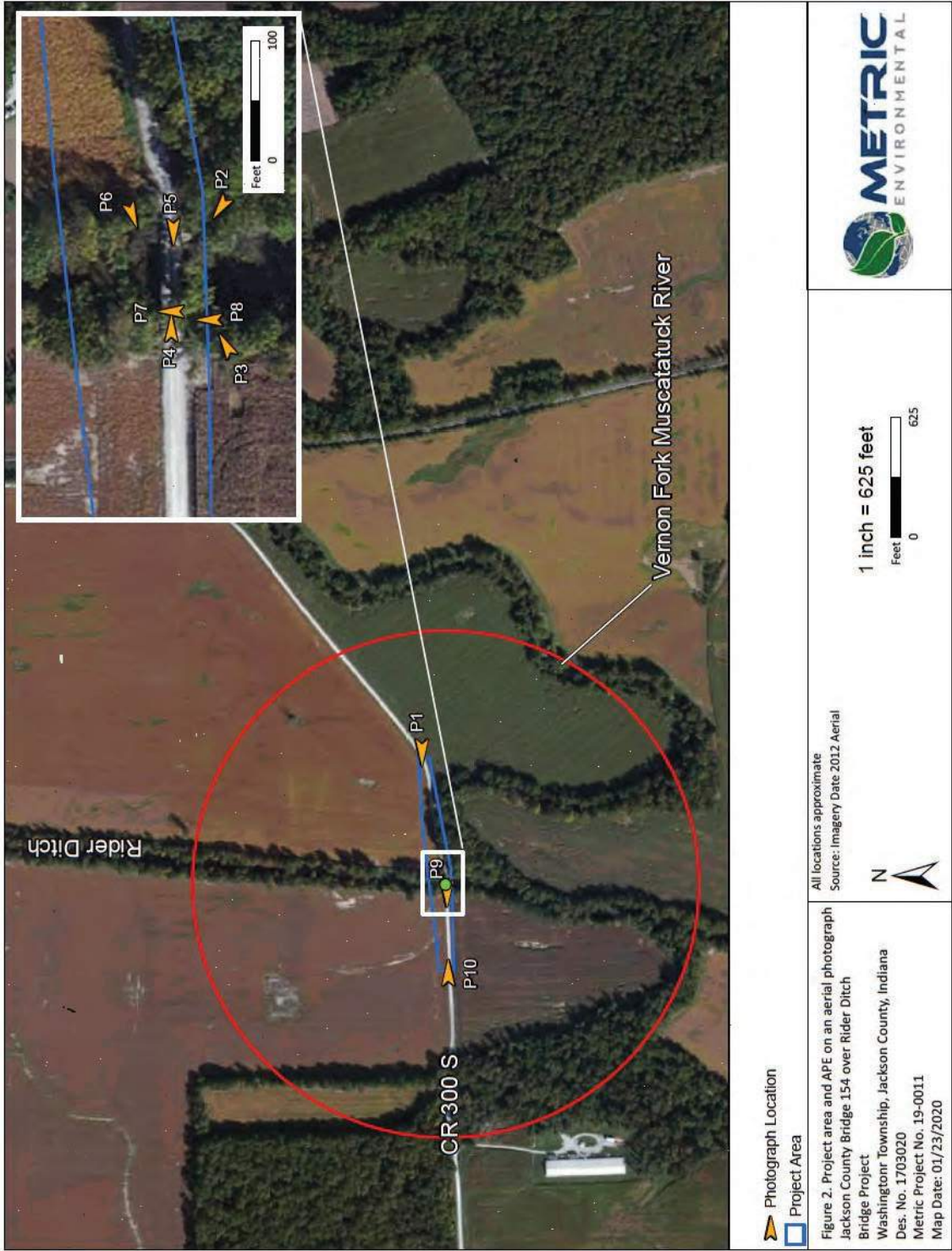




Photo 1. CR 300 South, camera facing west towards Jackson County Bridge 154.



Photo 2. Jackson County Bridge 154, camera facing northwest.



Photo 3. Jackson County Bridge 154, camera facing northeast.



Photo 4. Jackson County Bridge 154, camera facing east.



Photo 5. Jackson County Bridge 154, camera facing west.



Photo 6. Jackson County Bridge 154, camera facing southwest.



Photo 7. Detail of Jackson County Bridge 154.



Photo 8. Detail of Jackson County Bridge 154.

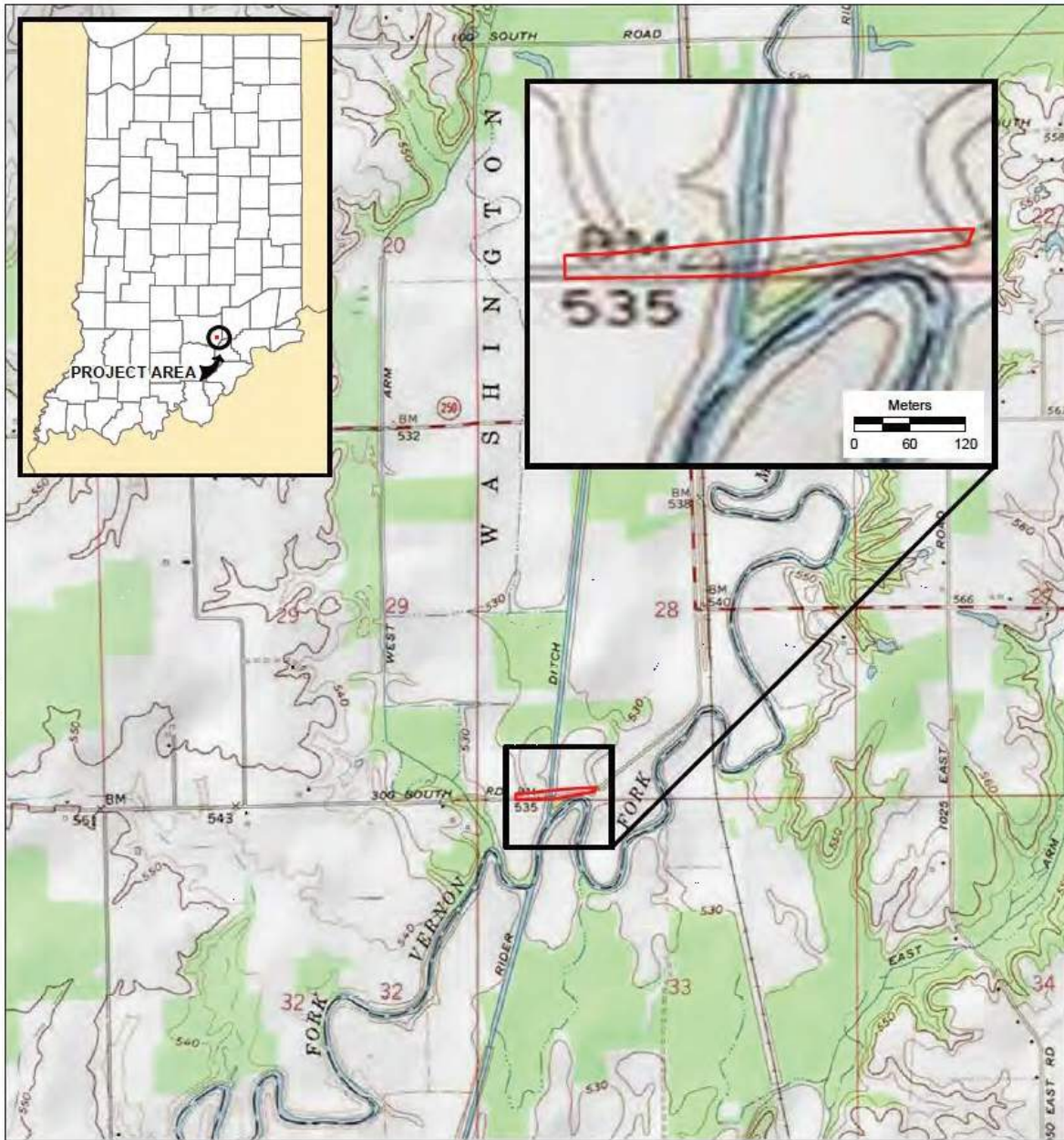


Photo 9. CR 300 South, camera facing west.



Photo 10. CR 300 South, camera facing east toward Jackson County Bridge 154.

Appendix B: Project Locations Maps and APE




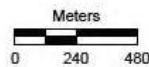
 Project Area

Figure 1. Project area on a portion of the
1993 Crothersville, IN, 7.5 Minute Quadrangle
Jackson County Bridge 154 over Rider Ditch
Bridge Project
Washington Township, Jackson County, Indiana
Des. No. 1703020
Metric Project No. 19-0011
Map Date: 10/29/2019

All Locations Approximate
1993 Basemap



1 cm = 240 m





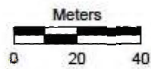
 Project Area

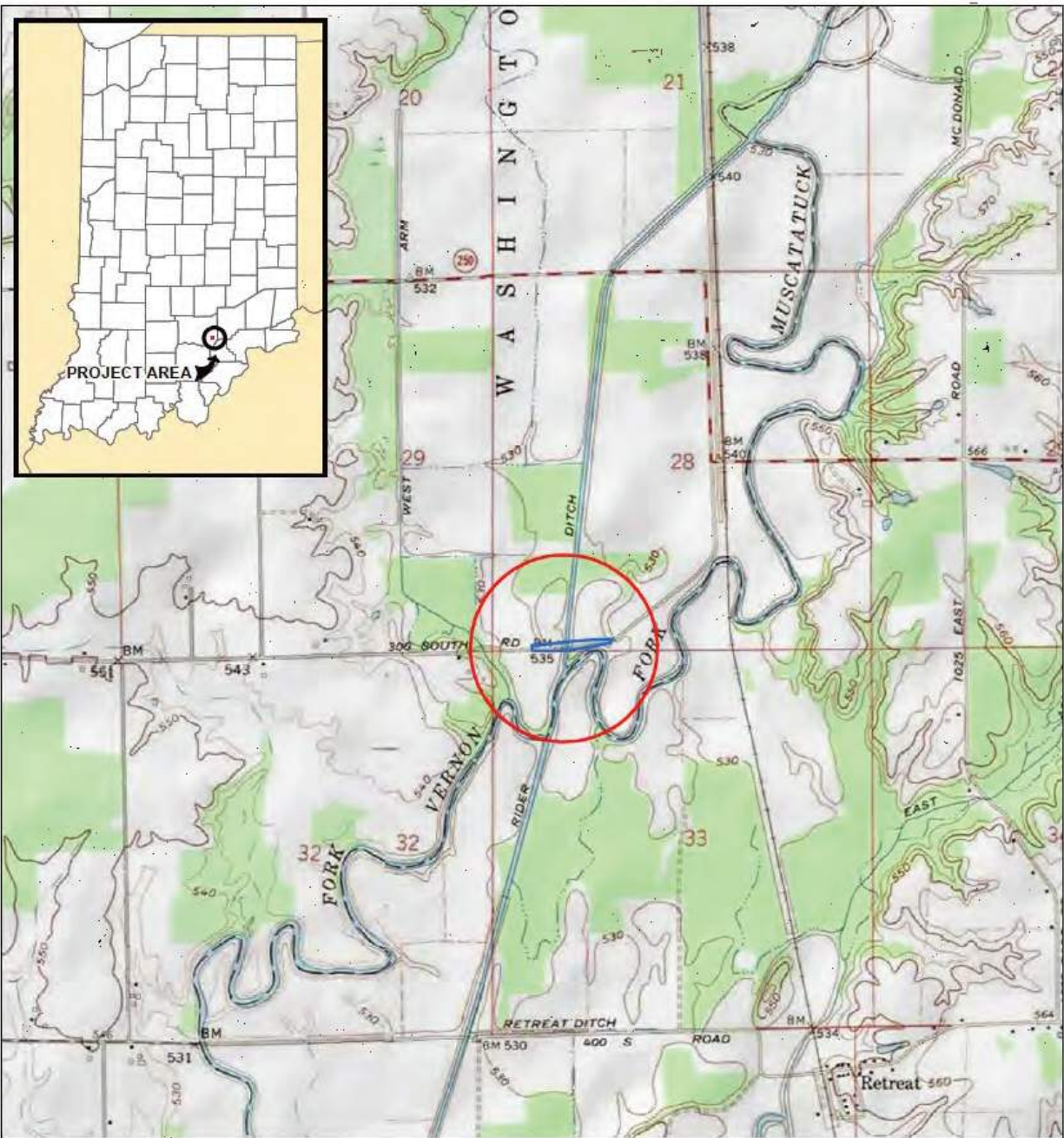
Figure 2. Project area on an aerial photograph
 Jackson County Bridge 154 over Rider Ditch
 Bridge Project
 Washington Township, Jackson County, Indiana
 Des. No. 1703020
 Metric Project No. 19-0011
 Map Date: 10/29/2019

All Locations Approximate
 2013 Basemap
 Service Layer Credits: Source: Esri, DigitalGlobe, GeoEye,
 Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AeroGRID,
 IGN, and the GIS User Community



1 cm = 20 m





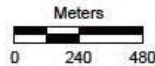
- Project Area
- APE

Figure 1. Project area and APE on a portion of the 1992 Crothersville, IN, 7.5 Minute Quadrangle
 Jackson County Bridge 154 over Rider Ditch
 Bridge Project
 Washington Township, Jackson County, Indiana
 Des. No. 1703020
 Metric Project No. 19-0011
 Map Date: 10/29/2019

All Locations Approximate
 1992 Basemap



1 cm = 240 m



Appendix C: Report Summaries

**Please note: the entire HPSR can be downloaded from INSCOPE*

HISTORIC PROPERTY SHORT REPORT

JACKSON COUNTY BRIDGE 154 (NBI# 3600099) CARRYING
CR 300 SOUTH OVER RIDER DITCH BRIDGE PROJECT DES: 1703020
WASHINGTON TOWNSHIP, JACKSON COUNTY, INDIANA

PREPARED FOR:

JANSSEN & SPAANS ENGINEERING, INC.

LEAD AGENCY:

FEDERAL HIGHWAY ADMINISTRATION

Prepared by:



Complex Environment. Creative Solutions.

6971 Hillsdale Court
Indianapolis, IN 46256
Telephone: 317.400.1633
www.metricenv.com

Sue Becher Gilliam, M.S.H.P.
susang@metricenv.com

February 21, 2020

MANAGEMENT SUMMARY

This report documents the identification and evaluation efforts for properties included in the Area of Potential Effects (APE) for the proposed Jackson County Bridge 154 (NBI# 3600099) carrying CR 300 South over Rider Ditch bridge project in Washington Township, Jackson County, Indiana. Aboveground resources located within the project APE were identified and evaluated in accordance with Section 106, National Historic Preservation Act (NHPA) of 1966, as amended, and the regulations implementing Section 106 (36 CFR Part 800).

As a result of the NHPA, as amended, and CFR Part 800, federal agencies are required to take into account the impact of federal undertakings upon historic properties in the area of the undertaking. Historic properties include buildings, structures, sites, objects, and/or districts that are eligible for or listed in the National Register of Historic Places (National Register). As this project is receiving funding from the Federal Highway Administration (FHWA), it is subject to a Section 106 review.

The APE contains no properties listed in the National Register. One National Register -eligible resource is situated within the project APE, Jackson County Bridge 154 (NBI# 3600099), which was determined eligible for the NRHP per the *Indiana Historic Bridge Inventory*. As stated in the *Indiana Historic Bridge Inventory* this bridge is eligible under Criterion C because it represents an early or distinctive phase in bridge construction, design, or engineering, and it retains historic integrity necessary to convey its engineering significance. Per the terms of the "Programmatic Agreement Regarding Management and Preservation of Indiana's Historic Bridges" (Historic Bridges PA), the FHWA will satisfy its Section 106 responsibilities involving "Select" and "Non-Select" bridges through the Project Development Process (PDP) of the Historic Bridges PA (Stipulation III). Because Jackson County Bridge 154 is a "Non-Select" bridge, the procedures outlined in Stipulation III.B of the Historic Bridges PA will be followed. Per Stipulation III.B. a Historic Bridge Alternatives Analysis will be prepared for the project. There are no other resources listed on, or eligible for listing on, the National Register or the Indiana Register of Historic Sites and Structures (State Register) within the APE of this project. Furthermore, the APE contains no properties that are recommended eligible for inclusion in the National Register.

5.0 CONCLUSIONS

The APE contains no properties listed in the NRHP.

As a result of identification and evaluation efforts for this project, Jackson County Bridge 154 has been determined eligible for inclusion in the NRHP and classified as a Non-Select bridge.

ARCHAEOLOGICAL SHORT REPORT

PHASE IA ARCHEAOLOGICAL SURVEY FOR THE
JACKSON COUNTY BRIDGE 154 OVER RIDER DITCH BRIDGE
PROJECT, DES. NO. 1703020, WASHINGTON TOWNSHIP,
JACKSON COUNTY, INDIANA

PREPARED FOR:

JANSSEN & SPAANS ENGINEERING, INC.

LEAD AGENCY:

JACKSON COUNTY

JANUARY 31, 2020

Prepared by:



Complex Environment. Creative Solutions.

6971 Hillsdale Court
Indianapolis, IN 46256
Telephone: 317.400.1633
www.metricenv.com

A handwritten signature in black ink that reads "Samuel P. Snell".

Samuel P. Snell, MS, RPA
Archaeological Principal Investigator
sams@metricenv.com
January 31, 2020



INDIANA ARCHAEOLOGICAL SHORT REPORT

State Form 54566 (1-11)

INDIANA DEPARTMENT OF NATURAL RESOURCES DIVISION OF HISTORIC PRESERVATION AND ARCHAEOLOGY

402 West Washington Street, Room W274
Indianapolis, Indiana 46204-2739
Telephone Number: (317) 232-1646
Fax Number: (317) 232-0693
E-mail: dhpa@dnr.IN.gov

Where applicable, the use of this form is recommended but not required by the Division of Historic Preservation and Archaeology.

Author: Samuel P. Snell, MS, RPA

Date (month, day, year): January 31, 2020

Project Title: Phase Ia Archaeology Survey for the Jackson County Bridge 154 over Rider Ditch Bridge Project, Des. No. 1703020, Washington Township, Jackson County, Indiana

PROJECT OVERVIEW

Jackson County proposes to proceed with the Jackson County Bridge 154 over Rider Ditch bridge project in Washington Township, Jackson County, Indiana (Des. No. 17003020). The project is in Section 28, Township 5 North, Range 6 East on the 7.5-minute Crothresville, Indiana, United States Geological Survey (USGS) topographic map.

Project Description:

The current bridge is a steel Pratt pony truss which carries East County Road (CR) 300 South across Rider Ditch. Jackson County Bridge 154 has been determined as eligible for the National Register of Historic Places by the Indiana Historic Bridges Inventory. The bridge is structurally deficient. On the south side of the structure, at the west end, vandals have removed the bottom chord I-bar along one panel (date unknown). A temporary repair was completed (date unknown) consisting of wire rope and turnbuckles used to stabilize the structure. Due to advanced section loss, the bridge was closed in March 2019. The preferred alternative is to replace the bridge; however, if the bridge remains in place, then CR 300 S will be realigned to the north of the existing bridge and new right-of-way will be required.

The survey area is roughly rectangular in shape with a length of approximately 340.0 m (1,115.5 ft) and a maximum width of 37.3 m (122.4 ft). The project encompasses 1.0 ha (2.4 ac).

INDOT Designation Number/ Contract Number: 1703020 Project Number:

DHPA Number: 24684 Approved DHPA Plan Number:

Prepared For: Janssens & Spaans Engineering, Inc.

Contact Person: Brad Isaacs

Address: 5921 Stratton Circle

City: Columbus State: IN ZIP Code: 47203

Telephone Number: 812.372.6573 Email Address: BIsaacs@jsengr.com

Principal Investigator: Samuel P. Snell, MS, RPA

Signature:

Company/Institution: Metric Environmental

Recommendation

- The archaeological records check has determined that the project area has the potential to contain archaeological resources and a Phase Ia archaeological reconnaissance is recommended.
- The archaeological records check has determined that the project area does not have the potential to contain archaeological resources and no further work is recommended before the project is allowed to proceed.
- The Phase Ia archaeological reconnaissance has located no archaeological sites within the project area and it is recommended that the project be allowed to proceed as planned.
- The Phase Ia archaeological reconnaissance has determined that the project area includes landforms which have the potential to contain buried archaeological deposits. It is recommended that Phase Ic archaeological subsurface reconnaissance be conducted before the project is allowed to proceed.

Appendix D: Consulting Parties' Correspondence

| Organization | Contact/Title | Address | City | State | ZIP | Phone | Email address | Accept CP Status (Y/N) |
|---|-------------------------------------|----------------------------------|--------------|-------|-------|--------------|------------------------------------|------------------------|
| State Historic Preservation Office | Chad Slider | 402 West Washington Street, W274 | Indianapolis | IN | 46204 | 317.232.3492 | cslider@dnr.in.gov | Y |
| Indiana Landmarks, Southern Regional Office | Greg Sekula, Director | 911 State Street | New Albany | IN | 47150 | 812.284.4534 | gsekula@indianalandmarks.org | N |
| Jackson County Historical Center | Richard Rumph, President | 105 N. Sugar Street | Brownstown | IN | 47220 | N/A | jhc@frontier.com | N |
| Jackson County Commissioner | Bob Gillaspay | 8955 Elizabeth Way | Seymour | IN | 47274 | 812.525.8159 | bgillaspay@tcjobsite.com | N |
| Jackson County Commissioner | Drew Markel | 5269 East County Road 400 S | Seymour | IN | 47274 | 812.569.1110 | drew@drewmarkel.com | N |
| Jackson County Commissioner | Matt Reedy | 6384 N. County Road 450 W | Freetown | IN | 47235 | 812.525.8963 | Mreedy@jereedyinc.com | N |
| Jackson County Historian | Bill Day | 808 W. Spring Street | Brownstown | IN | 47220 | 812.358.5170 | Bdday2@frontier.com | N |
| Historic Spans Taskforce | Paul Brandenburg, Chair | 5868 Croton Circle | Indianapolis | IN | 46256 | | indianabridges@sbcglobal.com | N |
| | Dr. James L. Cooper, Professor | 629 East Seminary Street | Indianapolis | IN | 46135 | | ljcooper@ccrtc.com | N |
| Historic Hoosier Bridges | Tony Dillon | 208 N 17th St. | New Castle | IN | 47362 | 765.624.6558 | spansaver@hotmail.com | N |
| Historicbridges.org | Nathan Holth | 2767 Eastway Drive | Okemos | MI | 48864 | 269.290.2593 | nathan@historicbridges.org | N |
| Historic Bridge Foundation | Kitty Henderson, Executive Director | P.O. Box 66245 | Austin | TX | 78766 | 512.407.8898 | Kitty@historicbridgefoundation.com | N |
| Eastern Shawnee Tribe of Oklahoma | | | | | | | | N |
| Miami Tribe of Oklahoma | | | | | | | | Y |
| Peoria Tribe of Indians of Oklahoma | | | | | | | | N |
| Pokagon Band of Potawatomi Indians | | | | | | | | N |
| Delaware Tribe of Indians, Oklahoma | | | | | | | | N |
| Jackson Co Parks & Rec | Fleeta Arthur | 220 E Walnut St | Brownstown | IN | 47220 | | yakfleet@gmail.com | Y |

Sam Snell @

November 12, 2019 at 3:30 PM



FHWA Project: Des. No. 1703020; Jackson County Bridge 154 on CR 300 S over Rider Ditch Bridge Project, Washington Township, Jackson Co... Details

To: gsekula@indianalandmarks.org, jchc@frontier.com, bgillaspy@tcjobsite.com, drew@drewmarkel.com & 9 more

Des. No.: 1703020

Project Description: Jackson County Bridge 154 on CR 300 S over Rider Ditch Bridge Project

Location: Washington Township, Jackson County, Indiana

Jackson County, with funding from the Federal Highway Administration and administrative oversight from the Indiana Department of Transportation, proposes to proceed with the Jackson County Bridge 154 on CR 300 S over Rider Ditch Bridge Project, Des. No. 1703020.

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. The following agencies/individuals are being invited to become consulting parties:

| | |
|--|---|
| State Historic Preservation Office | Indiana Landmarks, Southern Regional Office |
| Jackson County Historical Center | Jackson County Historian |
| Jackson County Commissioners | Historic Spans Taskforce |
| Dr. James L. Cooper | Historic Hoosier Bridges |
| Historicbridges.com | Historic Bridge Foundation |
| Jackson County Parks and Recreation Board | Eastern Shawnee Tribe of Oklahoma |
| Miami Tribe of Oklahoma | Peoria Tribe of Indians of Oklahoma |
| Pokagon Band of Potawatomi Indians | Delaware Tribe of Indians, Oklahoma |

This letter is part of the early coordination phase of the environmental review process requesting comments associated with this project. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

Please review the attached letter, which is also located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with your comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comments. If we do not receive a response from an invited consulting party within the time allotted, the project will proceed consistent with the proposed design. **Therefore, if we do not receive a response within thirty (30) days, your agency or organization will not receive any further information on the project unless the scope of work changes.**

Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344. Thank you in advance for your input,

Samuel P. Snell, MS, RPA
Archaeological Principal Investigator



Phone: 317.912.3499 Email: sams@metricenv.com
6971 Hillsdale Court, Indianapolis, IN 46250

www.metricenv.com
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Certified DBE/MBE/SBE INDIANAPOLIS | GARY | CINCINNATI

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CR300overRider
Ditch_...-12.pdf

From: Kennedy, Mary
To: "thpo@estoo.net"; "Diane Hunter"; "lpappenfort@peoriatribe.com"; Matthew.Bussler@pokaqonband-nsn.gov; lheady@delawaretribe.org
Cc: Miller, Shaun (INDOT); "Allen, Michelle (FHWA)"; Sam Spell; Dhpacommentsfromcro, Dnr
Subject: FHWA Project: Des. No. 1703020; Jackson County Bridge 154 Project, Washington Township, Jackson County, Ind.
Date: Tuesday, November 12, 2019 3:46:03 PM
Attachments: [image006.png](#)
[image007.png](#)
[image008.png](#)
[image010.png](#)

External Message: *This message originated outside of Metric Environmental.
Do not click links or open attachments unless you recognize the sender and know the content is safe.*

Des. No.: 1703020

Project Description: Jackson County Bridge 154 on CR 300 S over Rider Ditch Bridge Project

Location: Washington Township, Jackson County, Indiana

Jackson County, with funding from the Federal Highway Administration and administrative oversight from the Indiana Department of Transportation, proposes to proceed with the Jackson County Bridge 154 on CR 300 S over Rider Ditch Bridge Project, Des. No. 1703020.

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. The following agencies/individuals are being invited to become consulting parties:

| | |
|---|---|
| State Historic Preservation Office | Indiana Landmarks, Southern Regional Office |
| Jackson County Historical Center | Jackson County Historian |
| Jackson County Commissioners | Historic Spans Taskforce |
| Dr. James L. Cooper | Historic Hoosier Bridges |
| Historicbridges.com | Historic Bridge Foundation |
| Jackson County Parks and Recreation Board | Eastern Shawnee Tribe of Oklahoma |
| Miami Tribe of Oklahoma | Peoria Tribe of Indians of Oklahoma |
| Pokagon Band of Potawatomi Indians | Delaware Tribe of Indians, Oklahoma |

This letter is part of the early coordination phase of the environmental review process requesting comments associated with this project. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

Please review the letter located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with your comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comments. If we do not receive a response from an invited consulting party within the time allotted, the project will proceed consistent with the proposed design. **Therefore, if we do not receive a response within thirty (30) days, your agency or organization will not receive any further information on the project unless the scope of work changes.**

Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Mary E. Kennedy

Historic Bridge Specialist

100 N. Senate Ave., Room N642-ES

Indianapolis, IN 46204

Office: (317) 232-5215

Email: mkennedy@indot.in.gov



****Updated guidance for historic bridge projects can be found in the links below:**

[Overview-Indiana Historic Bridges Program](#)

[Historic Bridge Project Development Process](#)

[Procedures for Public Hearings under the Historic Bridges PA](#)

***For the latest updates from INDOT's Cultural Resources Office, subscribe to the Environmental Services listserv:**

<https://www.in.gov/indot/3217.htm>



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204

PHONE: (317) 234-5168

Eric Holcomb, Governor
Joe McGuinness, Commissioner

November 12, 2019

This letter was sent to the listed parties.

RE: Jackson County Bridge 154 (NBI# 3600099) on CR 300 S over Rider Ditch Bridge Project, Washington Township, Jackson County, Indiana, Des. No. 1703020.

Dear Consulting Party (see attached list),

Jackson County, with funding from the Federal Highway Administration and administrative oversight from the Indiana Department of Transportation (INDOT), proposes to proceed with the Jackson County Bridge 154 on CR 300 S over Rider Ditch Bridge Project, Des. No. 1703020. Metric Environmental, LLC. is under contract with Janssen & Spaans Engineering, Inc., on behalf of Jackson County to advance the environmental documentation for the referenced project.

The proposed undertaking is on East County Road 300 South over Rider Ditch in Jackson, Indiana. It is within Washington Township, Crothersville, IN, in 28 and 29, 5N, 6E.

The purpose of this project is to provide a structurally sufficient and scour resistant crossing of Rider Ditch, to maintain connectivity and access for local residents and farmers. The need for this project is due to advanced deterioration and vandalism of Jackson County Bridge #154, a steel Pratt pony truss which carries East County Road (CR) 300 South across Rider Ditch. The bridge is structurally deficient. On the south side of the structure, at the west end, vandals have removed the bottom chord I-bar along one panel (date unknown). A temporary repair was completed (date unknown) consisting of wire rope and turnbuckles used to stabilize the structure. Due to advanced section loss, the bridge was closed in March 2019. It is anticipated that the project will require acquisition of permanent right-of-way. Although exact amounts are not known at this time, it is expected to be 0.5 acre or less. Jackson County Bridge 154 has been determined as eligible for the National Register of Historic Places by the Indiana Historic Bridges Inventory.

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic and archaeological properties. In accordance with 36 CFR 800.2 (c), you are hereby requested to be a consulting party to participate in the Section 106 process. Entities that have been invited to participate in the Section 106 consultation process for this project are identified in the attached list. Per 36 CFR 800.3(f), we hereby request that the Indiana State Historic Preservation Officer (SHPO) notify this office if the SHPO staff is aware of any other parties that may be entitled to be consulting parties or should be contacted as potential consulting parties for the project.

The Section 106 process involves efforts to identify historic properties potentially affected by the undertaking, assess its effects and seek ways to avoid, minimize or mitigate any adverse effects on historic properties. For more information regarding the protection of historic resources, please see the Advisory Council on Historic Preservation's guide: *Protecting Historic Properties: A Citizen's Guide to Section 106 Review* available online at <https://www.achp.gov/sites/default/files/documents/2017-01/CitizenGuide.pdf>.

Per the terms of the "Programmatic Agreement Regarding Management and Preservation of Indiana's Historic Bridges" (Historic Bridges PA), the FHWA-Indiana Division will satisfy its Section 106 responsibilities involving "Select" and "Non-Select" bridges through the Project Development Process (PDP) of the Historic Bridges PA (Stipulation III). Because Jackson County Bridge 154 is a "Non-Select" bridge, the procedures outlined in Stipulation III.B. of the Historic Bridges PA will be followed to fulfill FHWA's Section 106 responsibilities for the project. (A copy of the Historic Bridges PA can be downloaded here: <http://www.in.gov/indot/2530.htm>).

The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. At this time, no cultural resource investigations have occurred; however, the results of cultural resource identification and

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evaluation efforts, both above-ground and archaeological, will be forthcoming. Consulting parties will receive notification when these reports are completed.

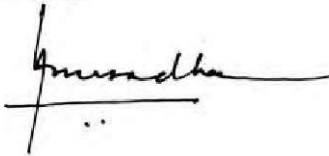
Please review the information and comment within thirty (30) calendar days of receipt. If you indicate that you do not desire to be a consulting party, or if you do not respond, you will not be included on the list of consulting parties for this project. If we do not receive your response in the time allotted, the project will proceed consistent with the proposed design and you will not receive further information about the project unless the design changes.

For questions concerning specific project details, you may contact Samuel P. Snell of Metric Environmental, LLC., at 317-912-3499 or sams@Metricenv.com. All future responses regarding the proposed project should be forwarded to Metric Environmental, LLC. at the following address:

Samuel P. Snell
Archaeological Principal Investigator
Metric Environmental, LLC.
6971 Hillsdale Court
Indianapolis, Indiana 46250
sams@metricenv.com

Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Sincerely,



Anuradha V. Kumar, Manager
Cultural Resources Office
Environmental Services

Enclosures: ←
Topographic project location
Project Area on an aerial photograph

Note: These enclosures are included elsewhere in this document

Distribution List:
State Historic Preservation Office
Indiana Landmarks, Southern Regional Office
Jackson County Historical Center
Jackson County Historian
Jackson County Commissioners
Historic Spans Taskforce
Dr. James L. Cooper
Historic Hoosier Bridges
Jackson County Parks and Recreation Board
Historicbridges.com
Historic Bridge Foundation
Eastern Shawnee Tribe of Oklahoma
Miami Tribe of Oklahoma
Peoria Tribe of Indians of Oklahoma
Pokagon Band of Potawatomi Indians
Delaware Tribe of Indians, Oklahoma

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Miami Tribe of Oklahoma

3410 P St. NW, Miami, OK 74354 • P.O. Box 1326, Miami, OK 74355
Ph: (918) 541-1300 • Fax: (918) 542-7260
www.miamination.com



Via email: smiller@indot.IN.gov

December 10, 2019

Shaun Miller
Archaeological Team Lead
Cultural Resources Office, Indiana DOT
575 North Pennsylvania Street
Indianapolis, IN 46204

Re: Des. No. 1703020; Jackson County Bridge 154 Project, Washington Township, Jackson County, Indiana – Comments of the Miami Tribe of Oklahoma

Dear Mr. Miller:

Aya, kikwehsitoole – I show you respect. My name is Diane Hunter, and I am the Tribal Historic Preservation Officer for the Federally Recognized Miami Tribe of Oklahoma. In this capacity, I am the Miami Tribe's point of contact for all Section 106 issues.

The Miami Tribe offers no objection to the above-mentioned project at this time, as we are not currently aware of existing documentation directly linking a specific Miami cultural or historic site to the project site. However, as this project is within the aboriginal homelands of the Miami Tribe, if any human remains or Native American cultural items falling under the Native American Graves Protection and Repatriation Act (NAGPRA) or archaeological evidence is discovered during any phase of this project, the Miami Tribe requests immediate consultation with the entity of jurisdiction for the location of discovery. In such a case, please contact me at 918-541-8966 or by email at dhunter@miamination.com to initiate consultation.

The Miami Tribe accepts the invitation to serve as a consulting party to the proposed project. In my capacity as Tribal Historic Preservation Officer I am the point of contact for consultation.

Respectfully,

Diane Hunter
Tribal Historic Preservation Officer



December 16, 2019

Samuel P. Snell
Archaeological Principal Investigator
Metric Environmental, LLC
6971 Hillside Court
Indianapolis, Indiana 46250

Federal Agency: Indiana Department of Transportation (“INDOT”),
on behalf of Federal Highway Administration, Indiana Division (“FHWA”)

Re: Early coordination letter for the Jackson County Bridge 154 carrying CR 300S over Rider
Ditch bridge project in Washington Township, Jackson County, Indiana (Des. No. 1703020;
DHPA No. 24684)

Dear Mr. Snell:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), implementing regulations at 36 C.F.R. Part 800, the “Programmatic Agreement Among the Federal Highway Administration, the Indiana Department of Transportation, the Indiana State Historic Preservation Officer, and the Advisory Council on Historic Preservation Regarding the Management and Preservation of Indiana’s Historic Bridges” (“Indiana Historic Bridges PA”), and the “Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana,” the staff of the Indiana State Historic Preservation Officer (“Indiana SHPO” or “INDNR-DHPA”) has reviewed INDOT’s November 11, 2019 early coordination letter, which we received November 25, 2019 for the aforementioned project.

Thank you for providing a list of the invited consulting parties and their contact information. We are not aware of any parties who should be invited to participate in the Section 106 consultation on this federal undertaking, beyond those whom INDOT already has invited. In your next regular correspondence on this project, please advise us as to which of the invited consulting parties has accepted the invitation.

We note that this c. 1910, single-span steel Pratt pony truss was evaluated as Eligible for inclusion in the National Register of Historic Places but rated Non-Select in the *Indiana Historic Bridges Inventory*.

We look forward to reviewing the proposed area of potential effects and the reports on investigations of above-ground cultural resources and archaeological resources that the early coordination letter indicated will be forthcoming.

The Indiana SHPO staff’s archaeological reviewer for this project is Wade T. Tharp, and the structures reviewers are John Carr and Danielle Kauffmann. However, if you have a question about the Section 106 process, please contact initially the INDOT Cultural Resources staff members who are assigned to this project.

Samuel P. Snell
December 16, 2019
Page 2

In all future correspondence about the Jackson County Bridge 154 on CR 300 S over Rider Ditch bridge Project in Washington Township, Jackson County, Indiana (Des. No. 1703020), please refer to DHPA No. 24684.

Very truly yours,



Beth K. McCord
Deputy State Historic Preservation Officer

BKM:DMK:dmk

emc: Michelle Allen, FHWA
Erica Tait, FHWA
Anuradha Kumar, INDOT
Shaun Miller, INDOT
Susan Branigin, INDOT
Shirley Clark, INDOT
Mary Kennedy, INDOT
Samuel P. Snell, Metric Environmental, LLC
Wade T. Sharp, INDNR-DHPA
John Carr, INDNR-DHPA
Danielle Kauffmann, INDNR-DHPA

From: Carr, John
Sent: Friday, January 10, 2020 2:02 PM
To: Kennedy, Mary; Fleeta Arthur
Cc: Slider, Chad (DNR); Branigin, Susan; Kumar, Anuradha; Kauffmann, Danielle M
Subject: RE: Jackson County Bridges

Mary, thank you for looking into Fleeta's inquiry and for responding to her with the information available to you.

Fleeta, I would encourage you to let Mary know if you decide you *do* want to become a consulting party on the Des. No. 1703020 project (Jackson Co. Br No. 154 on CR 300S over Rider Ditch).

John

John L. Carr
Team Leader for Historic Structures Review
Division of Historic Preservation and Archaeology
Indiana Department of Natural Resources
402 West Washington Street, Room W274
Indianapolis, Indiana 46204
317-233-1949
jcarr@dnr.IN.gov
www.dnr.IN.gov/historic

* Please let us know about the quality of our service by taking this brief [customer survey](#).

From: Kennedy, Mary
Sent: Friday, January 10, 2020 1:39 PM
To: Fleeta Arthur <yakfleet@gmail.com>
Cc: Carr, John <JCarr@dnr.IN.gov>; Slider, Chad (DNR) <CSlider@dnr.IN.gov>; Branigin, Susan <SBranigin@indot.IN.gov>; Kumar, Anuradha <akumar@indot.IN.gov>; Kauffmann, Danielle M <DKauffmann@dnr.IN.gov>
Subject: Jackson County Bridges

Hello Fleeta,

John Carr of the SHPO staff forwarded me an inquiry you made regarding bridges in Jackson County. Your message indicated you have concerns regarding some truss bridges possibly slated for replacement. While I don't know what projects Jackson County may be planning with 100% local funds, we currently have two projects in the INDOT system (FHWA funds) for Jackson County pony trusses:

Jackson County Bridge No. 154 (NBI No. 3600099), CR 300S over Rider Ditch, Jackson County, Des. No. 1703020
Jackson County Bridge No. 197 (NBI No. 3600132), CR 100S over McHargue Ditch, Jackson County, Des. No. 1703018

The early coordination letter to kick off the Section 106 review process for Des. No. 1703020 was sent out via email on November 12, 2019 and you were a recipient. The letter can also be accessed via IN SCOPE by searching for the des. no. (<http://erms.indot.in.gov/Section106Documents/>). The letter indicated that if you did not respond, you would not be on the list for further communications for the project. The only respondents were the SHPO staff and the Miami Tribe of Oklahoma. However, if you would like to become a consulting party for that project and continue to receive emails for it, please let me know. The bridge involved is a 90' Pratt Pony Truss. We don't have a builder listed in our database.

No environmental/Section 106 work has yet commenced for Bridge No. 197/ Des. No. 1703018. You will be on the list of invited consulting parties when the first communication goes out. The bridge involved is a 65' Warren Pony Truss. Our database indicates it was built by a local contractor named Henry Harman.

Your message indicated you'd like to know how many Indiana Bridge Company bridges with "pedestals" are remaining. Based on research INDOT did to evaluate a bridge in Chain O'Lakes State Park, it does not appear that any Indiana Bridge Company Warren Cantilever Bedstead Pony Truss bridges remain except for one in Chain O'Lakes State Park. I have attached the page from the report that lists the known examples based on Jim Cooper's book *Iron Monuments to Distant Posterity* along with the dates they were demolished (when known). The full report on the Chain O'Lakes State Park Bridge can be found through IN SCOPE by searching under Des. No. 0123456.

Regards,

Mary E. Kennedy

Historic Bridge Specialist

100 N. Senate Ave., Room N642-ES

Indianapolis, IN 46204

Office: (317) 232-5215

Email: mkennedy@indot.in.gov



****Updated guidance for historic bridge projects can be found in the links below:**

[Overview-Indiana Historic Bridges Program](#)

[Historic Bridge Project Development Process](#)

[Procedures for Public Hearings under the Historic Bridges PA](#)

***For the latest updates from INDOT's Cultural Resources Office, subscribe to the Environmental Services**

listserv: <https://www.in.gov/indot/3217.htm>

From: Sam Snell [<mailto:sams@metricenv.com>]
Sent: Tuesday, February 25, 2020 9:11 AM
To: Slider, Chad (DNR) <CSlider@dnr.IN.gov>
Cc: Moffatt, Charles D <CMoffatt@indot.IN.gov>; Kennedy, Mary <MKENNEDY@indot.IN.gov>
Subject: FHWA Project: Des. No. 1703020; Jackson County Bridge over Rider Ditch Bridge Project, Washington Township, Jackson County, Indiana (DHPA 24684).

**** This is an EXTERNAL email. Exercise caution. DO NOT open attachments or click links from unknown senders or unexpected email. ****

Des. No.: 1703020

Project Description: Jackson County Bridge over Rider Ditch Bridge Project, Washington Township, Jackson County, Indiana (DHPA 24684).

Jackson County, with funding from the Federal Highway Administration and administrative oversight from the Indiana Department of Transportation, proposes to proceed with the Jackson County Bridge 154 over Rider Ditch; Des. No. 1703020, Washington Township, Jackson County, Indiana. The Section 106 Early Coordination Letter for this project was originally distributed on November 12, 2019.

As part of Section 106 of the National Historic Preservation Act, a Historic Properties Short Report and Archaeological Report have been prepared and are ready for review and comment by consulting parties.

Please review this documentation located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with any comments that you may have. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Thank you in advance for your input,

Samuel P. Snell, MS, RPA
Archaeological Principal Investigator



Phone: 317.912.3499 Email: sams@metricenv.com
6971 Hillside Court, Indianapolis, IN 46250



www.metricenv.com

Complex Environment. Creative Solutions.

Certified DBE/MBE/SBE

INDIANAPOLIS | GARY | CINCINNATI

Sam Snell

Sent: Tuesday, February 25, 2020 9:21 AM
To: 'thpo@estoo.net'; 'dhunter@miamination.com'; 'lpappenfort@peoriatribe.com'; 'Matthew.Bussler@pokagonband-nsn.gov'; lheady@delawaretribe.org
Cc: Miller, Shaun (INDOT); Allen, Michelle (FHWA)
Subject: Jackson County Bridge 154 (NBI# 3600099) on CR 300 S over Rider Ditch Bridge Project, Washington Township, Jackson County, Indiana, Des. No. 1703020.

Des. No.: 1703020

Project Description: Bridge Project,

Location: Washington Township, Jackson County, IN

Jackson County, with funding from the Federal Highway Administration and administrative oversight from the Indiana Department of Transportation (INDOT), proposes to proceed with the Jackson County Bridge 154 on CR 300 S over Rider Ditch Bridge Project, Des. No. 1703020. Metric Environmental, LLC. is under contract with Janssen & Spaans Engineering, Inc., on behalf of Jackson County to advance the environmental documentation for the referenced project. The Section 106 Early Coordination Letter for this project was originally distributed on November 12, 2019.

As part of Section 106 of the National Historic Preservation Act, an Archaeology Report (ASR) and a Historic Property Report (HPR) have been prepared and are ready for review and comment by consulting parties.

Please review the HPR and ASR (Tribes only) in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with any comments that you may have. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Thank you,

David Moffatt
Archaeologist
Environmental Services
Cultural Resources Office
Indiana Department of Transportation
317-233-3703

Samuel P. Snell, MS, RPA
Archaeological Principal Investigator



Phone: 317.912.3499 Email: sams@metricenv.com
6971 Hillisdale Court, Indianapolis, IN 46250

www.metricenv.com
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INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204

PHONE: (317) 234-5168

Eric Holcomb, Governor
Joe McGuinness, Commissioner

February 25, 2020

This letter was sent to the listed parties.

RE: Jackson County Bridge 154 over Rider Ditch bridge project (Des. No. 1703020); Washington Township, Jackson County, Indiana (DHPA 24684).

Dear Consulting Party,

Jackson County, with funding from the Federal Highway Administration and administrative oversight from the Indiana Department of Transportation (INDOT), proposes to proceed with the Jackson County Bridge 154 over Rider Ditch Des. No. 1703020 (DHPA 24684).

This letter is part of the Section 106 review process for this project. Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic and archaeological properties. We are requesting comments from you regarding the possible effects of this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

A Section 106 early coordination letter was distributed on November 12, 2019.

The proposed undertaking is on East County Road 300 South over Rider Ditch in Jackson, Indiana. It is within Washington Township, Crothersville, IN, in 28 and 29, 5N, 6E.

The need for this project is due to the advanced deterioration of Jackson County Bridge 154, a steel Pratt pony truss bridge. The most recent bridge inspection report noted severe corrosion and advance section loss (up to 85%) on the plates connecting the intermediate vertical posts and floor beams. One location had the plate severed through. On the south side of the structure, at the west end, vandals have removed the bottom chord I-bar along one panel (date unknown). A temporary repair was completed (date unknown) consisting of wire rope and turnbuckles used to stabilize the structure. The bridge was closed in March 2019 after the bridge inspection. The primary purpose of this project is to restore service to East County Road 300 South over Rider Ditch. It is anticipated that the project will require acquisition of permanent right-of-way. Although exact amounts are not known at this time, it is expected to be 0.5 acre or less. Jackson County Bridge 154 has been determined as eligible for the National Register of Historic Places by the Indiana Historic Bridges Inventory.

Metric Environmental, LLC is under contract with Janssens and Spaans Engineering, Inc. on behalf of Jackson County to advance the environmental documentation for the referenced project.

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In accordance with 36 CFR 800.2 (c), you were invited to become a consulting party as part of the Section 106 process, or you are hereby invited to become a consulting party as part of the Section 106 process. Entities that have previously accepted consulting party status--as well as additional entities that are currently being invited to become consulting parties--are identified in the attached list.

The Section 106 process involves efforts to identify historic properties potentially affected by the undertaking, to assess the undertaking's effects and to seek ways to avoid, minimize, or mitigate any adverse effects on historic properties. For more information regarding the protection of historic resources, please see the Advisory Council on Historic Preservation's guide: *Protecting Historic Properties: A Citizen's Guide to Section 106 Review* available online at <https://www.achp.gov/sites/default/files/documents/2017-01/CitizenGuide.pdf>.

Per the terms of the "Programmatic Agreement Regarding Management and Preservation of Indiana's Historic Bridges" (Historic Bridges PA), the FHWA-Indiana Division will satisfy its Section 106 responsibilities involving "Select" and "Non-Select" bridges through the Project Development Process (PDP) of the Historic Bridges PA (Stipulation III). Because Jackson County Bridge 154 is a "Non-Select" bridge, the procedures outlined in Stipulation III.B. of the Historic Bridges PA will be followed to fulfill FHWA's Section 106 responsibilities for the project. (A copy of the Historic Bridges PA can be downloaded here: <http://www.in.gov/indot/2530.htm>).

The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. The APE contains no resources listed in the National Register of Historic Places (NRHP).

A historian who meets the Secretary of the Interior's Professional Qualification Standards identified and evaluated above-ground resources within the APE for potential eligibility for the NRHP. As a result of the historic property identification and evaluation efforts, no above-ground resources are recommended as eligible for listing in the NRHP.

With regard to archaeological resources, an archaeologist who meets the Secretary of the Interior's Professional Qualification Standards identified no sites within the project area. As a result of these efforts, no archaeological sites are recommended as eligible for listing in the NRHP and no further work is recommended.

The Archaeology Report (Tribes only) and Historic Property Short Report are available for review in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE). You are invited to review these documents and to respond with comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If you prefer a hard-copy of this material, please respond to this email with your request within seven (7) days.

Please review the information and comment within thirty (30) calendar days of receipt. If you indicate that you do not desire to be a consulting party or if you have not previously accepted consulting party status and you do not respond to this letter, you will not be included on the list of consulting parties for this project and will not receive further information about the project unless the design changes.

For questions concerning specific project details, you may contact Samuel P. Snell of Metric Environmental, LLC., at 317-912-3499 or sams@Metricenv.com. All future responses regarding the proposed project should be forwarded to Metric Environmental, LLC. at the following address:

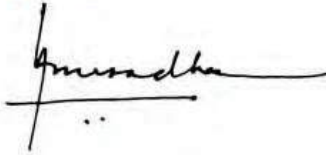
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Samuel P. Snell
Archaeological Principal Investigator
Metric Environmental, LLC.
6971 Hillside Court
Indianapolis, Indiana 46250
sams@metricenv.com

Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Sincerely,



Anuradha V. Kumar, Manager
Cultural Resources Office
Environmental Services

Enclosures:
Historic Properties Short Report
Archaeological Short Report

Distribution List:
Indiana Department of Natural Resources – Division of Historic Preservation and Archaeology
Miami Tribe of Oklahoma



March 30, 2020

Samuel P. Snell
Archaeological Principal Investigator
Metric Environmental, LLC
6971 Hillsdale Court
Indianapolis, Indiana 46250

Federal Agency: Indiana Department of Transportation ("INDOT"),
on behalf of Federal Highway Administration, Indiana Division ("FHWA")

Re: Indiana archaeological short report (Snell, 01/31/2020), and historic property short report
(Becher Gilliam, 2/21/2020), for the Jackson County Bridge 154 carrying CR 300S over Rider
Ditch bridge project, in Washington Township, Jackson County, Indiana (Des. No. 1703020;
DHPA No. 24684)

Dear Mr. Snell:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), implementing regulations at 36 C.F.R. Part 800, the "Programmatic Agreement Among the Federal Highway Administration, the Indiana Department of Transportation, the Indiana State Historic Preservation Officer, and the Advisory Council on Historic Preservation Regarding the Management and Preservation of Indiana's Historic Bridges" ("Indiana Historic Bridges PA"), and the "Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana," the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO" or "INDNR-DHPA") has reviewed your February 25, 2020, review request submittal form, which enclosed the aforementioned reports, all of which we received February 27, 2020.

The proposed area of potential effects ("APE") appears to be of adequate size for this project, to encompass the geographic area in which direct and indirect effects of a project of this nature could occur.

Based on the information in the historic property short report ("HPSR"; Becher Gilliam, 2/21/2020), we agree that Jackson County Bridge 154 is the only above-ground property within the APE that is eligible for inclusion in the National Register of Historic Places ("NRHP"). As previously indicated, we note that the subject bridge, which carries CR 300S over Rider Ditch is identified as eligible for inclusion in the NRHP under Criterion C as an early or distinctive phase in bridge construction, design, or engineering. The single-span, pin-connected, steel Pratt pony truss constructed c. 1910 is listed as a Non-Select Bridge in the *Indiana Historic Bridge Inventory*.

Additionally, based on the submitted information and the documentation available to the staff of the Indiana SHPO, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the NRHP within the proposed project area; and we concur with the opinion of the archaeologist, as expressed in the Indiana archaeological short report (Snell, 01/31/2020), that no further archaeological investigations appear necessary at the proposed project area.

If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29) requires that the discovery be reported to INDNR-DHPA within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana

Samuel P. Snell
March 30, 2020
Page 2

Code 14-21-1-27 and Indiana Code 14-21-1-29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. Part 800.

Because possible effects on this historic but Non-Select Bridge were taken into account in the Indiana Historic Bridges PA, it might now be appropriate for INDOT, on behalf of FHWA, to make a finding for this undertaking.

The Indiana SHPO staff's archaeological reviewer for this project is Wade T. Tharp, and the structures reviewers are John Carr and Danielle Kauffmann. However, if you have a question about the Section 106 process, please contact initially the INDOT Cultural Resources staff members who are assigned to this project.

In all future correspondence about the Jackson County Bridge 154 on CR 300S over Rider Ditch bridge Project in Washington Township, Jackson County, Indiana (Des. No. 1703020), please continue to refer to DHPA No. 24684.

Very truly yours,

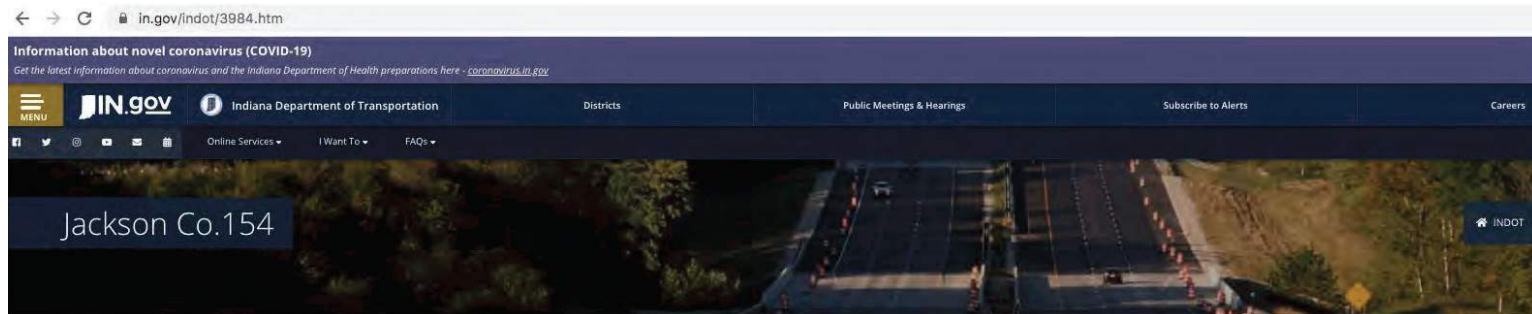


Beth K. McCord
Deputy State Historic Preservation Officer

BKM:DMK:JLC:WTT:wt

emc: Erica Tait, FHWA
Anuradha Kumar, INDOT
Shaun Miller, INDOT
Susan Branigin, INDOT
Shirley Clark, INDOT
Mary Kennedy, INDOT
Samuel P. Snell, Metric Environmental, LLC
Susan Becher Gilliam, Metric Environmental, LLC
Miami Tribe of Oklahoma
Wade T. Tharp, INDNR-DHPA
John Carr, INDNR-DHPA
Danielle Kauffmann, INDNR-DHPA

Appendix E: Bridge Marketing Documentation



| Location: | County | Road | Over | Number | Other Location Information |
|-----------|---------|----------|--------------------|--------|-----------------------------|
| | Jackson | CR 300 S | Rider Ditch Bridge | 154 | 0.82 miles east of CR 840 E |

| | Owner | Length | Width | Year Built | Type |
|--------------------|--|---------|---------|----------------|------------------|
| | Jackson Co. | 90 feet | 16 feet | 1910 | Pratt Pony Truss |
| Builder: | unknown | | | Status: | Pending |
| Statistics: | Reconstructed in 1992. A temporary repair was completed consisting of wire rope and turnbuckles used to stabilize the structure. On the south side of the structure, at the west end, vandals have removed the bottom chord I-bar along one panel. Jackson County Bridge #154, a steel Pratt pony truss which carries East County Road (CR) 300 South across Rider Ditch was built in 1910. The bridge is eligible for the National Register of Historic Places and has been determined "Non-Select" for preservation per the Programmatic Agreement Regarding Management and Preservation of Indiana's Historic Bridges. The status of this bridge is currently "pending," which means that its future is currently unknown as the Section 106 historic review process is on-going. Depending on the outcome of Section 106 consultation, interested parties may be able to utilize the bridge. Jackson County is now accepting proposals for the rehabilitation and reuse, or the storage and future reuse of the bridge. Proposals will also be accepted for the salvage of elements of the bridge. | | | | |

| Contact: | Name | E-mail | Address | Phone |
|----------|--------------------|---------------------|--|----------------|
| | Luella Beth Hillen | bethh@metricenv.com | Metric Enviromental, LLC 6971 Hillsdale Court | (317) 218-4728 |



Public Bridge notice.

The Tribune

Prescribed by State Board of Accounts

General Form No. 99P (Rev. 2009A)

Attn: Accounts Payable
Name Metric Environmental, LLC.
60041247

(Governmental Unit)

The Tribune
100 St Louis Ave
Seymour, IN 47247
Fed ID# 32-0472774

County: Jackson

PUBLISHER'S CLAIM

LINE COUNT

| | |
|---|----|
| Data for computing costs: Number of lines per column..... | 96 |
| Number of Columns..... | 1 |
| Number of Insertions..... | 1 |

COMPUTATION OF CHARGES

| | | | | |
|---|-----------------|-----------|----|-------|
| Lines x columns x insertion rate..... | 0.3355 per line | Flat Rate | \$ | 32.21 |
| Additional charges for notices containing rule or tabular work (50 per cent of above amount) | | | \$ | |
| Charge for extra proofs of publication (\$5.00 for each proof in excess of two) | | | \$ | |
| TOTAL AMOUNT OF CLAIM | | | \$ | 32.21 |

Pursuant to the provisions and penalties of IC 5-11-10-1, I hereby certify that the foregoing account is just and correct, that the amount claimed is legally due, after allowing all just credits, and that no part of the same has been paid. 11/15/2019

PUBLISHER'S AFFIDAVIT

State of Indiana (Jackson County) ss:

I, Sally Clark, Legal Advertising Clerk of The Tribune newspaper of general circulation printed and published in the English language in the (city/town) of Seymour in state and county aforesaid, and that the printed matter attached hereto is a true copy, which dates of publication being as follows:

11/15/2019

Sally Clark

Sally Clark/Legal Advertising Clerk

| | | | | | | |
|---------------------|---|---|---------------------|--------------------|---|-----------------------------|
| Page | : | 1 of 2 | 11/15/2019 09:35:23 | Ad Number | : | 50049571 |
| Order Number | : | 60041247 | | Ad Key | : | |
| PO Number | : | Rhonda Edwards | | Salesperson | : | 28 - Amirtha Sathi Sargunam |
| Customer | : | S11211606 Metric Environmental, LLC. | | Publication | : | Seymour Tribune |
| Contact | : | Rhonda Edwards | | Section | : | 60 Notices |
| Address1 | : | 6971 Hillside Court | | Sub Section | : | 60 Notices |
| Address2 | : | | | Category | : | 6015 Legals |
| City St Zip | : | Indianapolis IN 46250 | | Dates Run | : | 11/15/2019-11/15/2019 |
| Phone | : | (317) 207-4286 | | Days | : | 1 |
| Fax | : | | | Size | : | 1 x 9.31, 96 lines |
| Credit Card | : | | | Words | : | 296 |
| Printed By | : | Sally Rohm | | Ad Rate | : | L-Government |
| Entered By | : | Amirtha Sathi Sargunam | | Ad Price | : | 32.21 |
| Keywords | : | Designation No. 1703020 - Public Notice | | Amount Paid | : | 0.00 |
| Notes | : | 11/14-Emailed Confirmation. AS | | Amount Due | : | 32.21 |
| Zones | : | | | | | |

Legal Advertisement
Public Notice
Designation No. 1703020

Jackson County is offering Jackson County Bridge 154 (NBI# 3600099) carrying County Road 300 South over Rider Ditch in Washington Township, Jackson County to interested responsible parties. The bridge is eligible for the National Register of Historic Places and has been determined "Non-Select" for preservation per the Programmatic Agreement Regarding Management and Preservation of Indiana's Historic Bridges. The status of this bridge is currently "pending," which means that its future is currently unknown as the Section 106 historic review process is on-going. Depending on the outcome of Section 106 consultation, interested parties may be able to utilize the bridge.

The bridge is a single span steel pratt pony truss structure with a wooden deck that is 90 feet long and 16 feet wide. The bridge is in poor condition. A photo and general information about the bridge can be viewed at the following w e b s i t e : <http://www.in.gov/indot/2532.htm> . Additional information about the bridge is available for review by contacting the person listed below.

Jackson County is now accepting proposals for the rehabilitation and reuse, or the storage and future reuse of the bridge. Proposals will also be accepted for the salvage of elements of the bridge. Any proposals should be received within the next six months. Funding of any

rehabilitation, reuse, storage, dismantling, reconstruction, salvage, etc. of this bridge would be the responsibility of the new owner. Interested parties should submit a written proposal for reuse to the address below as soon as possible:

Luella Beth Hillen
Director of NEPA Services
Metric Environmental,
LLC.
6971 Hillsdale Court
Indianapolis, Indiana
46250
bethh@metricenv.com

**This notice is intended to market Non-Select Bridges. If an owner is marketing a Select Bridge, please contact INDOT-CRO for guidance on modifying the template appropriately.
60041274 hspaxip
S: 11/15/19

The Indianapolis Star

130 South Meridian Street
Indianapolis, IN 46225
Marion County, Indiana

METRIC ENVIRONMENTAL

Federal Id: 06-1032273

Account #:INI-62283
Order #:0004172952
of Affidavits: 2

Total Amount of Claim:\$73.17
This is not an invoice

METRIC ENVIRONMENTAL
ATTN Rhonda Edwards
6971 HILLSDALE CT
INDIANAPOLIS, IN 46250

PUBLISHER'S AFFIDAVIT

STATE OF WISCONSIN, }
County Of Brown } **SS:**

Personally appeared before me, a notary public in and for said county and state, the undersigned

I, being duly sworn, say that I am a clerk for THE INDIANAPOLIS NEWSPAPERS a DAILY STAR newspaper of general circulation printed and published in the English language in the city of INDIANAPOLIS in state and county of Marion, and that the printed matter attached hereto is a true copy, which was duly published in said paper for 1 times., the dates of publication being as follows:

The insertion being on the 05/01/2020

Newspaper has a website and this public notice was posted in the same day as it was published in the newspaper.

Pursuant to the provisions and penalties of Ch. 155, Acts 1953,

I hereby certify that the foregoing account is just and correct, that the amount claimed is legally due, after allowing all just credits, and that no part of the same has been paid.

Kathleen Allen

Date: 5-1, 2020 Title: Clerk

Subscribed and sworn to before me this 1 day of May, 2020

Shelly Hora
Notary Public

Notary Expires: 8-25-23

SHELLY HORA
Notary Public
State of Wisconsin

STAR

(Governmental Unit)

County, Indiana

To: INDIANAPOLIS

Indianapolis, IN

58 lines, 2 columns wide equals 116 equivalent lines at \$0.63 per line @ 1 days, \$73.17

Website Publication \$0

Charge for proof(s) of publication \$0.00

TOTAL AMOUNT OF CLAIM \$73.17

Acct #:INI-62283
Ad #: 0004172952

DATA FOR COMPUTING COST
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Claim No. _____ Warrant No. _____
IN FAVOR OF
The Indianapolis Star
Indianapolis, IN
Marion County
130 S. Meridian St. Indianapolis, IN 46225

I have examined the within claim and hereby certify as follows:

That it is in proper form.

This it is duly authenticated as required by law.

That it is based upon statutory authority.

That it is apparently (correct)
(incorrect)

\$ _____

On Account of Appropriation For

FED. ID
#06-1032273

Allowed _____, 20____

In the sum of \$ _____

I certify that the within claim is true and correct; that the services there-in itemized and for which charge is made were ordered by me and were necessary to the public business.

Public Notice
Designation No. 1703020

Jackson County is offering Jackson County Bridge 154 (NB# 3600099) carrying County Road 300 South over Rider Ditch in Washington Township, Jackson County to interested responsible parties. The bridge is eligible for the National Register of Historic Places and has been determined "Non-Select" for preservation per the Programmatic Agreement Regarding Management and Preservation of Indiana's Historic Bridges. The status of this bridge is currently "pending," which means that its future is currently unknown as the Section 106 historic review process is on-going. Depending on the outcome of Section 106 consultation, interested parties may be able to utilize the bridge.

The bridge is a single span steel pratt pony truss structure with a wooden deck that is 90 feet long and 16 feet wide. The bridge is in poor condition. A photo and general information about the bridge can be viewed at the following website: <http://www.in.gov/indot/2532.htm>. Additional information about the bridge is available for review by contacting the person listed below.

Jackson County is now accepting proposals for the rehabilitation and reuse, or the storage and future reuse of the bridge. Proposals will also be accepted for the salvage of elements of the bridge. Any proposals should be received within the next six months. Funding of any rehabilitation, reuse, storage, dismantling, reconstruction, salvage, etc. of this bridge would be the responsibility of the new owner. Interested parties should submit a written proposal for reuse to the address below as soon as possible:

Luella Beth Hillen Director of NEPA Services
Metric Environmental, LLC,
6971 Hillside Court
Indianapolis, Indiana 46250
bethh@metricenv.com

**This notice is intended to market Non-Select Bridges. If an owner is marketing a Select Bridge, please contact INDOT-CRO for guidance on modifying the template appropriately.

(S - 5/1/20 - 0004172952)

hspaxlp

Division of Historic Preservation & Archaeology • 402 W. Washington Street, W274 • Indianapolis, IN 46204-2739
Phone 317-232-1646 • Fax 317-232-0693 • dhpa@dnr.IN.gov • www.IN.gov/dnr/historic



June 8, 2020

Samuel P. Snell
Metric Environmental, LLC
6971 Hillside Court
Indianapolis, Indiana 46250

Federal Agency: Indiana Department of Transportation (“INDOT”),
on behalf of Federal Highway Administration, Indiana Division (“FHWA”)

Re: Indiana Department of Transportation’s finding of “no historic properties affected” on behalf of the Federal Highway Administration for the Jackson County Bridge 154 carrying CR 300S over Rider Ditch bridge project, in Washington Township, Jackson County, Indiana (Des. No. 1703020; DHPA No. 24684)

Dear Mr. Snell:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), implementing regulations at 36 C.F.R. Part 800, the “Programmatic Agreement Among the Federal Highway Administration, the Indiana Department of Transportation, the Indiana State Historic Preservation Office, and the Advisory Council on Historic Preservation Regarding the Management and Preservation of Indiana’s Historic Bridges” (“Indiana Historic Bridges PA”), and the “Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, and the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana,” the staff of the Indiana State Historic Preservation Officer, (“Indiana SHPO” or “INDNR-DHPA”) has reviewed your May 27, 2020 review request submittal form, which enclosed the abovementioned finding and documentation, all of which we received electronically the same day.

As previously indicated, we agree that the Jackson County Bridge 154 is the only above-ground property within the project’s area of potential effects that is eligible for inclusion in the National Register of Historic Places (“NRHP”). Per the terms of the Historic Bridges PA, the FHWA will satisfy its Section 106 responsibilities involving “Select” and “Non-Select” bridges through the Project Development Process (Stipulation III.B). As a result, this finding applies to other resources located within the project’s area of potential effects. There are no other historic properties within the APE.

Also as previously indicated, based on the submitted information and the documentation available to the staff of the Indiana SHPO, we have no identified any currently known archaeological resources listed in or eligible for inclusion in the NRHP within the proposed project area; and we concur with the opinion of the archaeologist, as expressed in the Indiana archaeological short report (Snell, 01/31/2020), that no further archaeological investigations appear necessary at the proposed project area.

If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29) requires that the discovery be reported to INDNR-DHPA within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29 does not obviate the need to adhere to applicable federal statutes and regulations including but not limited to 36 C.F.R. Part 800.

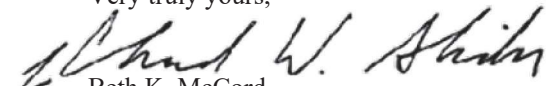
Accordingly, we concur with the INDOT’s May 26, 2020, Section 106 finding, on behalf of FHWA, of “no historic properties affected” for this federal undertaking.

Samuel P. Snell
June 8, 2020
Page 2

The Indiana SHPO Staff's archaeological reviewer for this project is Wade T. Tharp, and the structures reviewers are John Carr and Danielle Kauffmann. However, if you have a question about the Section 106 process, please contact initially the INDOT Cultural Resources staff members who are assigned to this project.

In all future correspondence regarding the Jackson County Bridge 154 on CR 300S over Rider Ditch bridge in Washington Township, Jackson County, Indiana (Des. No. 1703020), please refer to DHPA No. 24684.

Very truly yours,



Beth K. McCord
Deputy State Historic Preservation Officer

BKM:DMK:dmk

emc: Erica Tait, FHWA
Anuradha Kumar, INDOT
Mary Kennedy, INDOT
Shaun Miller, INDOT
Susan Branigin, INDOT
Shirley Clark, INDOT
Samuel P. Snell, Metric Environmental, LLC
Diane Hunter, Miami Tribe of Oklahoma
John Carr, INDNR-DHPA
Danielle Kauffmann, INDNR-DHPA
Wade T. Tharp, INDNR-DHPA

The Tribune

Prescribed by State Board of Accounts

General Form No. 99P (Rev. 2009A)

Attn: Accounts Payable
Name METRIC ENVIRONMENTAL, LLC.
60053330

(Governmental Unit)

The Tribune
100 St Louis Ave
Seymour, IN 47247
Fed ID# 32-0472774

County: Jackson

PUBLISHER'S CLAIM

LINE COUNT

| | |
|---|-----|
| Data for computing costs: Number of lines per column..... | 168 |
| Number of Columns..... | 1 |
| Number of Insertions..... | 1 |

COMPUTATION OF CHARGES

| | | | | |
|---|-----------------|-----------|----|-------|
| Lines x columns x insertion rate..... | 0.3445 per line | | \$ | 57.88 |
| | | Flat Rate | \$ | |
| Additional charges for notices containing rule or tabular work (50 per cent of above amount) | | | \$ | |
| Charge for extra proofs of publication (\$5.00 for each proof in excess of two) | | | \$ | |
| TOTAL AMOUNT OF CLAIM | | | \$ | 57.88 |

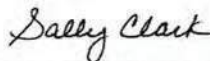
Pursuant to the provisions and penalties of IC 5-11-10-1, I hereby certify that the foregoing account is just and correct, that the amount claimed is legally due, after allowing all just credits, and that no part of the same has been paid. 5/30/2020

PUBLISHER'S AFFIDAVIT

State of Indiana (Jackson County) ss:

I, Sally Clark, Legal Advertising Clerk of The Tribune newspaper of general circulation printed and published in the English language in the (city/town) of Seymour in state and county aforesaid, and that the printed matter attached hereto is a true copy, which dates of publication being as follows:

5/30/2020



Sally Clark/Legal Advertising Clerk

Page : 1 of 3 06/01/2020 09:17:19

Order Number : 60053330
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Contact : RHONDA EDWARDS
Address1 : 6971 HILLSDALE COURT
Address2 :
City St Zip : INDIANAPOLIS IN 46250
Phone : (317) 207-4286
Fax :
Credit Card :
Printed By : Sally Rohm
Entered By : Christy Hubbard
Keywords : Public Notice Jackson County Bridge 154
Notes :
Zones :

Ad Number : 50065780
Ad Key :
Salesperson : 28 - Christy Hubbard
Publication : Seymour Tribune
Section : 60 Notices
Sub Section : 60 Notices
Category : 6015 Legals
Dates Run : 05/30/2020-05/30/2020
Days : 1
Size : 1 x 16.30, 168 lines
Words : 554
Ad Rate : L-Government
Ad Price : 57.88
Amount Paid : 0.00
Amount Due : 57.88

Legal Advertisement
Public Notice

Des. No. 1703020

Jackson County is proposing to undertake the Jackson County Bridge 154 (NBI#3800099) carrying CR 300S over Rider Ditch Project, that is funded in part by the Federal Highway Administration (FHWA). The project is located near the town of Crothersville in Washington Township, Jackson County.

Multiple alternatives are under consideration, including the no-build alternative; six rehabilitation options including three alternatives which the bridge would be rehabilitated to the Secretary of the Interior's Standards (SOIS); a relocation option which would require a third party to adopt the bridge and move it at their cost; and removal of the existing bridge and replacement with a new bridge on the same alignment. The Area of Potential Effects (APE) is the "geographic area or areas within which an undertaking may directly or indirectly cause alterations in the character or use of historic properties, if and such properties exist". The APE for the project was drawn sufficiently large to encompass potential impacts – including visual, physical, and traffic-related impacts – that may result from the undertaking, whichever alternative is selected.

The APE contains one property previously determined eligible for the National Register of Historic Places (NRHP): Jackson County Bridge 154. Per the terms of the "Programmatic Agreement Regarding Management and Preservation of Indiana's Historic

| | | | |
|-----------------------|---|----------------------|-----------------------|
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| PO Number : | Rhonda Edwards | Ad Key : | |
| Customer : | S11211606 METRIC ENVIRONMENTAL, LL | Salesperson : | 28 - Christy Hubbard |
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| Notes : | | | |
| Zones : | | | |

Bridges" (HBPA), FHWA will satisfy its Section 106 responsibilities involving "Select" and "Non-Select" bridges through the Project Development Process (PDP) of the HBPA (Stipulation III). Jackson County Bridge 154 has been classified as a "Non-Select" bridge and, thus, the procedures outlined in Stipulation III.B of the HBPA will be followed to fulfill FHWA's Section 106 responsibilities for the bridge. The standard treatment approach described in Attachment B of the HBPA (Standard Treatment Approach for Historic Bridges) will be followed. Per Stipulation III.B of the HBPA, Jackson County will hold a public hearing for the project prior to completion of the National Environmental Policy Act (NEPA) studies. The hearing will be advertised at a later date.

Regarding other resources in the APE, INDOT, on behalf of the FHWA, has determined a "No Historic Properties Affected" finding is appropriate for the project because no historic properties are present within the APE. In accordance with the National Historic Preservation Act, the views of the public are being sought regarding the effect of the proposed project on the historic elements as per 36 CFR 800.2(d), 800.3(e) and 800.6(a)(4). Pursuant to 36 CFR 800.4(d)(2), the documentation specified in 36 CFR 800.11(d) is available for review electronically by accessing INDOT's Section 106 document posting website IN SCOPE at <http://erms.indot.in.gov/Section106Documents>. Persons with limited in-

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Ad Price : 57.88
Amount Paid : 0.00
Amount Due : 57.88

ternet access may request project information be mailed to them. Please contact Luella Beth Hillen, 317-218-4728 or bethh@metricenv.com, to make such a request.

This documentation serves as the basis for the "No Historic Properties Affected" finding. The views of the public on this effect finding are being sought. Please reply with any comments to Luella Beth Hillen no later than June 29, 2020.

Luella Beth Hillen
 Metric Environmental, LLC
 6971 Hillside Ct.
 Indianapolis, IN 46250
 317-218-4728
 bethh@metricenv.com

In accordance with the "Americans with Disabilities Act", if you have a disability for which Jackson County needs to provide accessibility to the document(s) such as interpreters or readers, please contact Jerry Ault, Jackson Co. Highway Superintendent, at jault@jacksoncounty.in.gov 812-358-0953.

60053330 hspaxlp
 T: 5/30/2020

Public Notice
Des. No. 1703020

Jackson County is proposing to undertake the Jackson County Bridge 154 (NBI#3600099) carrying CR 300S over Rider Ditch Project, that is funded in part by the Federal Highway Administration (FHWA). The project is located near the town of Crothersville in Washington Township, Jackson County.

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Luella Beth Hillen
Metric Environmental, LLC
6971 Hillsdale Ct.
Indianapolis, IN 46250
317-218-4728
bethh@metricenv.com

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HISTORIC BRIDGE ALTERNATIVES ANALYSIS

ADDENDUM 1

DATE: 06/24/2021



BRIDGE NUMBER: 36-00154

DESIGNATION NUMBER: 1703020

ROUTE IDENTIFICATION AND FEATURE CROSSED:

East County Road 300 South Over Rider Ditch

NBI NUMBER: 3600099

COUNTY: Jackson

PROJECT LOCATION: 0.82 Miles East of CR 840 East



PREPARED BY:

Keith D. Echternach, P.E. – JSE, Inc.

REVIEWED BY:

Jeff Matern, P.E. – JSE, Inc.

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I. INTRODUCTION

In the Draft Historic Bridge Alternatives Analysis (HBAA, 5/28/2020) for Jackson County Bridge 154, the Jackson County Highway Department identified severe advanced structural deterioration and corrosion at the lower chord panel points of the truss. These panel points connect the floor beams to the intermediate vertical posts and also serve as pin locations for lower chord and diagonal eyebars. This bridge has been closed to traffic since March 19, 2019. Subsequent to the HBAA approval and at the direction of the Jackson County Highway Department, modifications were made to the proposed replacement bridge and project in order to accommodate their desired goals/secondary needs. These were then incorporated into our bridge design documents.

The project modifications include:

- Widening the proposed replacement structure to provide a 24' clear roadway.
- Adding a substantial quantity of channel protection along the Rider Ditch embankments.
- Providing a larger 50' middle span length to reduce debris collection under normal flow.
- Adjusting the horizontal alignment of the bridge to correct the angle point located to the east of the bridge.

Additionally, in the Level One Design Criteria Checklist the design speed was updated to 35 mph. Just recently it was made known that 35 mph is the (unposted) legal speed on un-paved roads in Jackson County. Our computations have been updated to reflect this .

The widened bridge and modified alignment were provided as accommodations for the anticipated primary use of this crossing in service of agricultural traffic. The proposed effectively 2-lane bridge on this one lane road would match the width of the bridge directly west in this corridor, Jackson County Bridge # 155, which has a clear roadway of 23.8'.

It is due to these changes that INDOT's Environmental and CRO staff have requested that an addendum be prepared in order to re-evaluate the alternatives considered in the analysis. Given that the scope of the bridge project currently includes both scour protection and approach work and that these items are typically included in bridge rehabilitation to address servicability, they are now included in all bridge project alternates for use in the analysis. The scale and quantity of this work has been carefully and equitably considered for each individual bridge alternative and would be included in the final design for any of these alternates should it be determined to be both feasible and prudent.

II. EXISTING STRUCTURE DATA

No changes to the draft Historic Bridge Alternatives Analysis (HBAA, 5/28/2020)

III. EXISTING CONDITIONS

No changes to sections A, C or D of the draft Historic Bridge Alternatives Analysis are being made (HBAA, 5/28/2020).

A second paragraph is being appended to section B as follows:

In addition to the specific superstructure deteriorations as noted, the existing bridge requires special inspection procedures as specified by the National Bridge Inspection Standards (NBIS) item 92A. Item 92A is required for bridges that are determined to be fracture critical. This effectively means that the loss of a primary tension only member can result in the catastrophic failure of the structure as a whole. Per this designation, bridge inspectors are required bi-annually to do a detailed inspection of all primary tension load carrying members and their connections. The inspectors then report on any discovered defects or deteriorations and keep record of any changed/worsening conditions. The most recent inspection report for the Jackson County 154 bridge indicates that a fracture critical inspection is not currently required. This is only due to the fact that the bridge is closed. If this bridge re-opens, fracture critical inspections will resume. See the inspection report pages in the Appendix as record of the above.

Section E will now begin with the below paragraph (the existing will remain and follow):

Rider Ditch at this location carries a drainage area of 378 square miles. Per the 2019 inspection report the adequacy of the existing waterway is rated as 2 for item 71. The cause of this rating is cited as “frequent flooding” in the inspection report. Per the *FHWA Coding Guide*, “frequent” has particular meaning which in this instance indicates overtopping events occurring at a bridge location once every 3 years or less. The *FHWA Coding Guide* gives the rating code description for 2 as: “Occasional or frequent overtopping of bridge deck and roadway approaches with severe traffic delays”. The page of the inspection report that references this criterion, and relevant pages from the *FHWA Coding Guide* are provided in the Appendix.

IV. PURPOSE AND NEED

No changes to the draft Historic Bridge Alternatives Analysis (HBAA, 5/28/2020)

V. ADJUSTMENT TO ALTERNATIVES

The alternatives introduction section is revised. The below replaces the existing section from the draft historic Bridge Alternatives Analysis (HBAA 5/28/2020).

Per the Indiana Design Manual (IDM) Chapter 412 Section 5 in conjunction with the Indiana Department of Cultural Resources-Cultural Resources Office (INDOT-CRO) Manual Part IV, the evaluation of alternatives shall address the prescribed hierarchal list proceeding from A to F. They shall be evaluated based on whether the alternative is feasible and prudent.

Prudence of projects involving Non-Select bridges on low-volume roads should be assessed based on cost-effectiveness and other criteria as noted in IDM 412-5.04(02). If the bridge rehabilitation cost is $\geq 40\%$ of the replacement cost **or** it meets two or more of the following criteria that cannot be economically corrected as part of a rehabilitation project, then replacement is warranted:

- “1. The bridge waterway opening is inadequate (i.e., National Bridge Inventory Item 71 is rated 2 or 3).*
- 2. The bridge has a documented history of catching debris due to inadequate freeboard or due to piers in the stream.*
- 3. The bridge requires special inspection procedures (i.e., the first character of National Bridge Inventory Item 92A or 92C is Y).*
- 4. The bridge is classified as scour-critical (i.e., National Bridge Inventory Item 113 is rated 0, 1, 2, or 3.)*
- 5. The bridge has fatigue-prone welded components that are expected to reach the end of their service lives within the next 20 years. See Section 412-4.03(04) for information on conducting a fatigue analysis.*
- 6. The bridge has a Sufficiency Rating of lower than 35.”*

(List above taken verbatim from Indiana Design Manual Section 412-5.04(02))

The alternatives below are developed with reference to the Guidelines for Geometric Design of Very Low-Volume Local Roads (ADT ≤ 400) (listed as “LV” in the Level One Checklists) set forth by the American Association of State Highway and Transportation Officials (AASHTO), IDM 412-5.03 and Figure 412-2A (design criteria for the treatment of historic bridges on low volume roads), and the INDOT 3R Criteria as noted in Figure 55-3D. As stated in the introduction, various treatments are included in the alternatives analysis to reflect the goals of the Jackson County Highway Department. A summary of the cost estimates is found in Table 3, while a construction cost estimate for each of the alternatives can be found in Appendix D.

A. Do Nothing Alternative

No changes to the draft Historic Bridge Alternatives Analysis (HBAA, 5/28/2020)

B-1. Rehabilitation in Accordance with the Secretary of Interior Standards

For additional discussion of this alternate please see the draft Historic Bridge Alternatives Analysis (HBAA, 5/28/2020)

This alternate as well as B-2 now provides scour protection by armoring the banks of Rider Ditch with riprap similarly to the updated alternates E & F. This is a county desired outcome for this project and is considered necessary for future servicability to the bridge irrespective of which alternative is selected. No additional riprap will be placed directly under the bridge at the east approach as the riprap here is in good condition, riprap will however be added north and south of the structure where bank erosion is present and significant. Along the west embankment, the channel will be cleared to allow a greater waterway opening through the structure during normal flow conditions. Riprap armoring will be placed along this bank similarly to what is provided for the updated alternates E & F.

Guardrail and endtreatments are now included to prevent blunt end vehicular strikes to the pony trusses and errant vehicle entry into Rider Ditch from E CR 300S. This is consistent with current roadside safety design practices and guardrail as provided for the updated alternates E&F.

The required roadway approach work incidental to this bridge rehabilitation and the B-2 alternate includes correcting the sag approach grade and vertical angle point where the bridge floor meets the approach gravel. Additionally the horizontal angle point to the east of the bridge will be relaxed to improve roadway servicability in approach to the bridge and reduce the potential for bridge strikes. The primary intent of these corrections, which are typically considered during bridge rehabilitation, is to improve overall safety for the motorist and provide similar improvements to E CR 300S to those included in the updated alternates E&F.

After adding the above to the existing B-1 cost estimate, the final cost for this alternate was determined to be \$360,000, 33% of the updated replacement alternate F. This alternative does not provide a minimum structural capacity of 15 Tons as required in the purpose and need. Additionally, per the evaluation criteria as noted above, Bridge 154 has an inadequate waterway opening, requires special inspections, and has a sufficiency rating below 35. The first two items will persist following rehabilitation. JSE has reviewed the criteria used to determine a structure's sufficiency rating and believe that following rehabilitation and inspection, this structure may still have a sufficiency close to or below 35.

Alternative B-1 is **feasible**; however due to substandard capacity, it does not meet the project purpose and need, and therefore, is **not prudent**. Additionally two items from IDM 412-5.04(02) will persist after the rehabilitation as described above.

B-2. Rehabilitation Not Meeting the Secretary of Interior's Standards

For additional discussion of this alternate please see the draft Historic Bridge Alternatives Analysis (HBAA, 5/28/2020)

The scour protection, roadside safety elements, and incidental roadway approachwork as noted above for the B-1 alternate applies equivalently to this alternate as well.

After adding these elements to the B-2 cost estimate, the final cost for this alternate (\$460,000) was determined to be 42% of the updated replacement alternate F.

Alternate B-2 improves the structural capacity of the bridge to meet the AASHTO H-15 vehicular load requirement. This alternate however retains the bridge's designation as fracture-critical and it will therefore continue to require these special inspections moving forward in perpetuity.

In addition, although not explicitly stated previously, the member replacements required to provide the 15 Ton structural capacity include:

- All rolled steel stringers. These members were determined substandard to carry the required vehicles in preliminary analysis.
- All diagonal and bottom chord eye-bars. (32 individual pieces in total)
- As noted previously all vertical post bottom gusset plates.
- Steel pins where substantial section loss has occurred (8 estimated)

These replacements constitute a substantial modification and change to the existing bridge superstructure. The in-kind replacements constitute a substantial loss of historic steel elements. From a purely aesthetic point of view, this structure would appear similar to what currently exists. These changes could potentially result in Bridge 154 no longer being eligible for the National Register of Historic Places. That determination would have to be made with SHPO input.

Alternative B-2 is **feasible**. It, however, is **not prudent**. The rehabilitation cost is 43% of the Alternative F replacement cost. This exceeds the 40% replacement cost threshold as noted above and in reference to IDM 412-5.04(02). Additionally, 412-5.04(02) states that if the bridge meets two or more other criteria that cannot be economically corrected as part of a rehabilitation project, then replacement is warranted. As mentioned in the Alternative B-1 discussion, Bridge 154 meets three of those criteria: an inadequate waterway opening, it requires special inspections, and has a sufficiency rating lower than 35. The first two conditions mentioned would persist after the rehabilitation. Furthermore, this alternative severely diminishes the historic integrity of the bridge. Lastly, as a secondary consideration, this bridge will not have sufficient width or load carrying capacity to fully serve the current needs associated with the adjacent properties.

C-1. Rehabilitation in Accordance with Secretary of Interior's Standards (One-way Pair)

Changes to this alternate are in agreement with those provided for the B-1 alternate above as they pertain to the rehabilitation and associated approach work for the existing structure.

Per the introduction, any new bridge that is narrower than 24' would not meet the County's desired project goal. As a result, any alternates that include a new bridge are providing one of equal length, configuration, type, and size to that included in the replacement alternates E and F.

The new structure for this alternate will be a haunched slab bridge with a 50' middle span. It will be located adjacent and parallel to the existing bridge and spaced 10' from the existing bridge allowing for easier simpler construction and inspectability. The new bridge will include concrete bridge railings and reinforced concrete bridge approach slabs. The riprap and geotextile for this alternate includes additional quantity to address

extended limits along Rider Ditch due to the presence of two structures, and modification to embankments to accept the two adjacent structures.

Aggregate pavement approaches for this structure will be tapered back to the existing as quickly and smoothly as possible. Guardrail and OS end treatments will be added to the outside copings of this and the existing bridge. The space between the bridges will be protected with an impact attenuator which will be situated to shield the truss and the blunt end of the concrete barrier railing.

The total cost of this alternate is \$1,374,000 which is 126% of the replacement alternate. This alternate would not improve the load carrying capacity of the existing bridge to the minimum of 15 Tons. The other factors from Section 412-5.04(02) of the IDM mentioned in the B alternatives are also applicable to this alternative.

Alternative C-1 is **feasible**, it however is **not prudent** since it does not meet the purpose and need for load carrying capacity, and the cost of this alternative is significantly higher than the replacement alternative.

| Table 2. Design Criteria for the New Structure for the C, D, E and F Alternatives. (Table Replaces "Table 2" from HBAA, 05/28/2021) | | | | | |
|--|---|-------------------------|----------------------|--------------------|----------------------------|
| Design Criteria | Design Criteria Reference | Minimum Design Criteria | Existing Condition | Proposed Condition | Design Exception Required? |
| Design Speed | Jackson County Legal Speed on Unpaved Roads | 35 mph | un-posted | un-posted | N/A |
| Lane Width/Shoulder Width (Total Road Cross-Section) | *AASHTO Geometric Design of VLVL | Existing ¹ | 11 ft to 15 ft | 11 ft to 24 ft | No |
| Bridge Clear Roadway Width | *AASHTO Geometric Design of VLVL | Existing | 15.4 ft | 24ft | No |
| Structural Capacity | IDM Fig. 55-3D | HL-93 | < 5 Ton ² | HL-93 | No |
| Travel Lane Cross Slope | *AASHTO Geometric Design of VLVL | Existing ³ | 0% | 0% to 2% | No ³ |
| Stopping Sight Distance | IDM Fig. 55-3D | 250 ft | >250' | >250' | No |
| Maximum Grade | IDM Fig. 55-3D | 9% | Flat | Flat | No |
| Bridge Railing Test Level | *AASHTO Geometric Design of VLVL | Existing | N/A | MASH TL-3 | No |
| * AASHTO Guidelines for Geometric Design of Very Low-Volume Local Roads. (ADT ≤ 400 VPD) | | | | | |
| 1 Evidence observed from site visit does not show excessive rutting or skid marks from running off the road. | | | | | |
| 2 Bridge is closed to vehicular traffic. | | | | | |
| 3 Roadway is aggregate pavement and carries a single lane of bi-directional traffic. No cross-slope is warranted since none exists. | | | | | |

C-2. Rehabilitation Not Meeting the Secretary of Interior's Standards (One-way Pair)

Changes to this alternate are in agreement with those provided for the B-2 alternate above as they pertain to the rehabilitation and associated approach work for the existing structure.

Changes to this alternate are also in agreement with those provided for the C-1 alternate above as they pertain to the new bridge, channel protection, associated approach work, and roadside safety elements.

The total cost of this alternate is \$1,475,000. This alternate improves the structural capacity of the bridge to meet the AASHTO H-15 minimum vehicular load requirement. However, the other factors from Section 412-5.04(02) of the IDM mentioned in the B alternatives are also applicable to this alternative.

Alternative C-2 is **feasible**, it however is **not prudent** since the cost of this alternate is 136% of the alternate F. Additionally the shortcomings as noted in alternative B-2 also pertain to this structure with respect to special inspections, waterway opening, diminished integrity, and the needs of the adjacent properties.

D. Rehabilitation of the Existing Structure with a Two-Way Bypass

This alternate is effectively identical to alternate C-1. The main difference is that following rehabilitation, the existing bridge will be closed to vehicular traffic and the adjacent approach will be graded for parking. Due to the severely deteriorated condition of the existing structure, it would need to be rehabilitated in an equivalent manner to the B-1 alternate. Once rehabilitated and permanently closed to highway traffic NBIS inspection requirements would no longer be applicable.

As noted previously in the draft Historic Bridge Alternatives Analysis (HBAA, 5/28/2020), this bridge is in a exclusively rural location that sees few if any pedestrians and has a very low traffic volume. It therefore has almost no potential as a recreational resource to the surrounding community. To date no interested parties have come forward to take ownership of Jackson County Bridge 154 as required for this alternate. (See Minimization and Mitigation section of HBAA, 5/28/2020 for discussion on the marketing of this bridge)

Alternative D is **feasible**. It is **not prudent** since the cost of this alternate is significantly higher than the demolition and replacement alternate F (126% greater) and no willing buyer has yet to come forward to claim the existing structure.

E. Relocation of the Existing Bridge and New Bridge on Current Alignment

No changes to the draft Historic Bridge Alternatives Analysis (HBAA, 5/28/2020) discussion on relocation of the existing structure.

The new structure for this alternate is changed in the following ways:

- The proposed replacement structure provides a 24' clear roadway.
- A substantial quantity of channel protection along the Rider Ditch embankments is being included with this alternate.

- A larger 50' middle span length is included to reduce debris collection under normal flow.
- The horizontal alignment of the bridge is adjusted to correct the angle point located to the east of the bridge. This alignment is situated such that the new structure partially overlaps with the existing bridge footprint and for the purpose of this report should be considered the existing alignment.

The cost of this alternative is \$1,025,000.00 which is 94% of the demolition and replacement alternative F. The bridge has been advertised for the minimum six month marketing period but to date, no responsible parties have come forward. The opportunity to relocate and reuse the bridge will remain viable until the public hearing requirements have concluded. As a result this alternate is still **feasible**. Given that this alternate is cost effective, meets the purpose and need, and achieves all of the county's desired secondary goals, this alternative is also **prudent** if a responsible party comes forward to take ownership of the bridge.

F. Replacement

This alternate diverges from the previously presented alternate F in the draft Historic Bridge Alternatives Analysis (HBAA, 5/28/2020) in the following ways:

- Per the wishes of the county, the horizontal alignment of the bridge is adjusted to correct the angle point located to the east of the bridge. This alignment is situated such that the new structure partially overlaps with the existing bridge footprint and for the purpose of this report should be considered the existing alignment.
- The proposed replacement structure provides a 24' clear roadway.
- A substantial quantity of channel protection along the Rider Ditch embankments is being included with this alternate.
- A larger 50' middle span length is included to reduce debris collection under normal flow.

Discussion related to the demolition of the existing bridge is unchanged from the draft Historic Bridge Alternatives Analysis (HBAA, 5/28/2020).

This alternative fully address the purpose and need statement by providing a cost effective (\$1,090,000) and structurally sufficient bridge (Table 3). This new bridge also meets all of the county's secondary goals and will sufficiently serve nearby agricultural properties. This alternative is determined to be both **feasible** and **prudent**.

VI. MINIMIZATION AND MITIGATION

A. Minimization

No changes to the draft Historic Bridge Alternatives Analysis (HBAA, 5/28/2020)

B. Mitigation

No changes to the draft Historic Bridge Alternatives Analysis (HBAA, 5/28/2020)

| Table 3. Alternative Summary (Table Replaces "Table 3" from HBAA, 05/28/2021) | | | | | | |
|---|-------------------------------|-------------------|-------------------|-------------|--|---------------------------------|
| Alternative | Meets Project Purpose & Need? | Construction Cost | ROW Amount & Cost | Total Cost | Other Factors | Feasible & Prudent? |
| A-No-Build | No | N/A | N/A | N/A | None | Feasible: Yes. Prudent: No. |
| B-1-Rehabilitation for Continued Vehicular Use (one-way option) | No | \$360,000 | N/A | \$360,000 | Does not meet required load capacity, fracture critical, inadequate waterway opening | Feasible: Yes. Prudent: No. |
| B-2-Rehabilitation for Continued Vehicular Use (one-way option) | No | \$460,000 | N/A | \$460,000 | Fracture critical, inadequate waterway opening, and severely diminished historic integrity | Feasible: Yes. Prudent: No. |
| C-1-Rehabilitation for Continued Vehicular Use (one-way pair option) | No | \$1,359,000 | 1 acre, \$15,000 | \$1,374,000 | Does not meet required load capacity, fracture critical, inadequate waterway opening | Feasible: Yes. Prudent: No. |
| C-2-Rehabilitation for Continued Vehicular Use (one-way pair option) | Yes | \$1,460,000 | 1 acre, \$15,000 | \$1,475,000 | Fracture critical, inadequate waterway opening and severely diminished historic integrity | Feasible: Yes. Prudent: No. |
| D - Bypass (non-vehicular use rehabilitation) | Yes | \$1,359,000 | 1 acre, \$15,000 | \$1,374,000 | No new owner has come forward to date. | Feasible: Yes. Prudent: No. |
| E - Relocate | Yes | \$1,010,000 | 1 acre, \$15,000 | \$1,025,000 | No new owner has come forward to date. | Feasible: Yes. Prudent: ** |
| F - Replacement | Yes | \$1,075,000 | 1 acre, \$15,000 | \$1,090,000 | None | Feasible: Yes. Prudent: Yes. |

*Right of Way Cost Based on \$15,000 budgeted

**Yes if a new owner comes forward

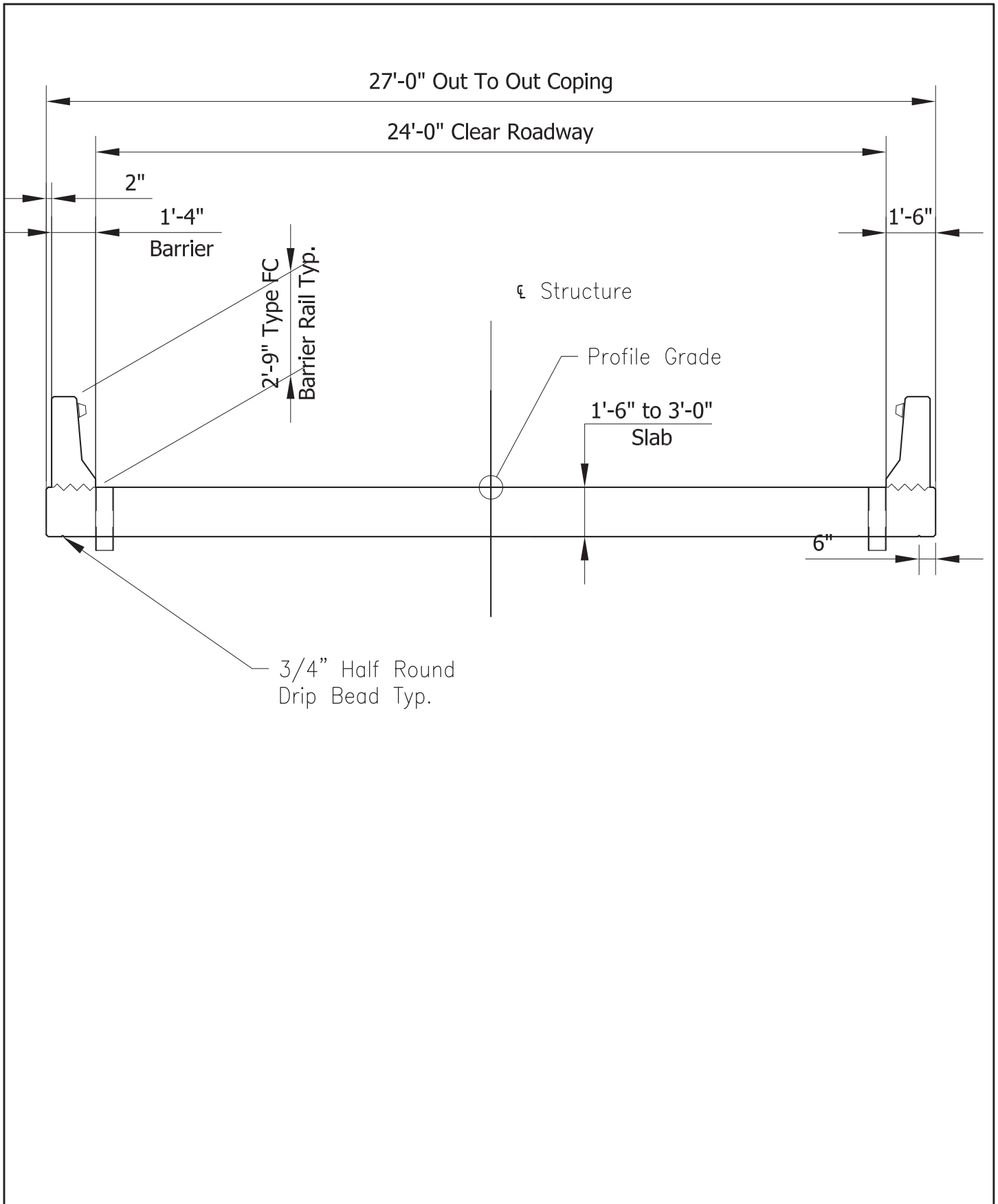
VII. PRELIMINARY PREFERRED ALTERNATE

Alternate E is the preferred alternative because it is **prudent** and **feasible** and provides and opportunity to preserve the bridge. At this time, the relocation and reuse of the bridge has been advertised to the public for the required six month minimum. To date, no responsible party has claimed the bridge. The opportunity to relocate and reuse the bridge will continue to be offered until the public hearing requirements have concluded. Unless a responsible party comes forward to claim the bridge by the time the public hearing requirements conclude, this alternative will be considered **not prudent**. As a result, it is **expected** that **Alternative F** will be the **preferred alternative** that is executed.

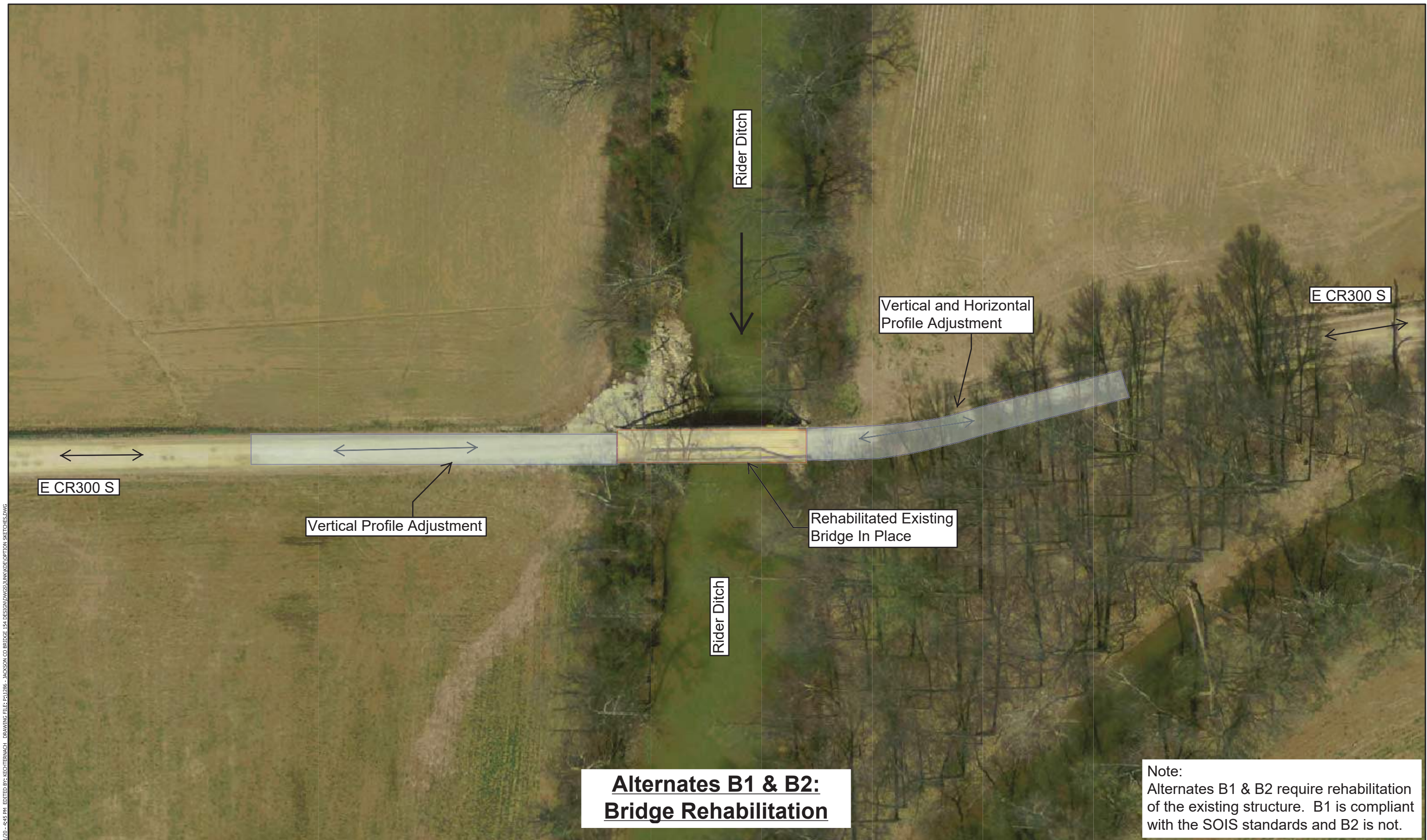
VIII. ADDENDUM APPENDICIES

Appendices as provided with this Addendum are intended to replace in-kind similar appendix pages/sections from the draft Historic Bridge Alternatives Analysis, (HBAA, 5/28/2020) or to supplement/support new information as noted also in the Addendum.

PLANS AND DESIGN CRITERIA



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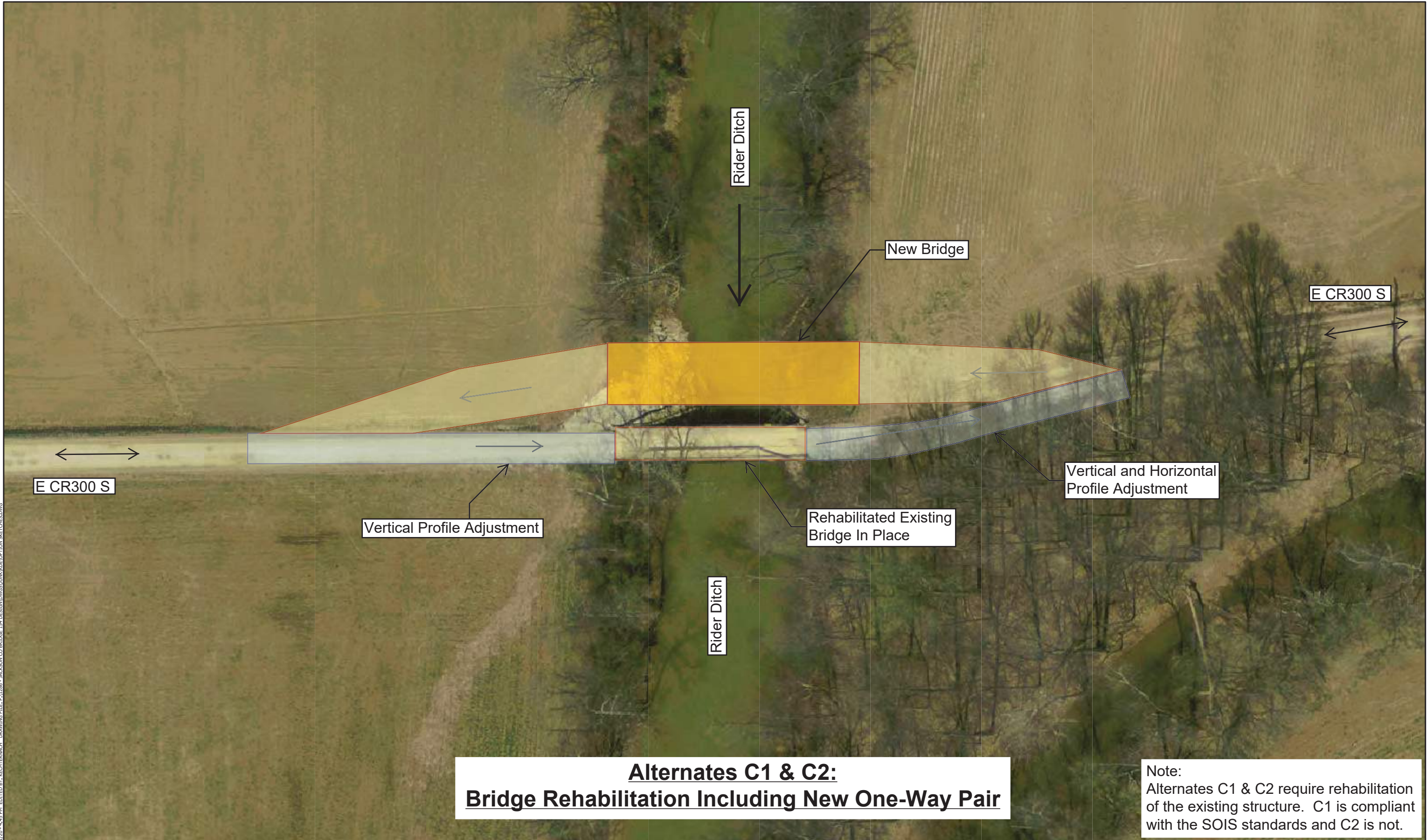


**Alternates B1 & B2:
Bridge Rehabilitation**

Note:
 Alternates B1 & B2 require rehabilitation of the existing structure. B1 is compliant with the SOIS standards and B2 is not.

| | | | |
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| Page 14 of 120 | RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER _____ DATE _____ | INDIANA DEPARTMENT OF TRANSPORTATION | HORIZONTAL SCALE _____ BRIDGE FILE _____ |
| | DESIGNED: _____ DRAWN: _____ | ALTERNATE B (1&2) OPTION LAYOUT | VERTICAL SCALE _____ DESIGNATION _____ |
| | CHECKED: _____ CHECKED: _____ | | NONE |
| | | | SURVEY BOOK _____ SHEETS _____ of _____ CONTRACT _____ PROJECT _____ |

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Alternates C1 & C2:
Bridge Rehabilitation Including New One-Way Pair

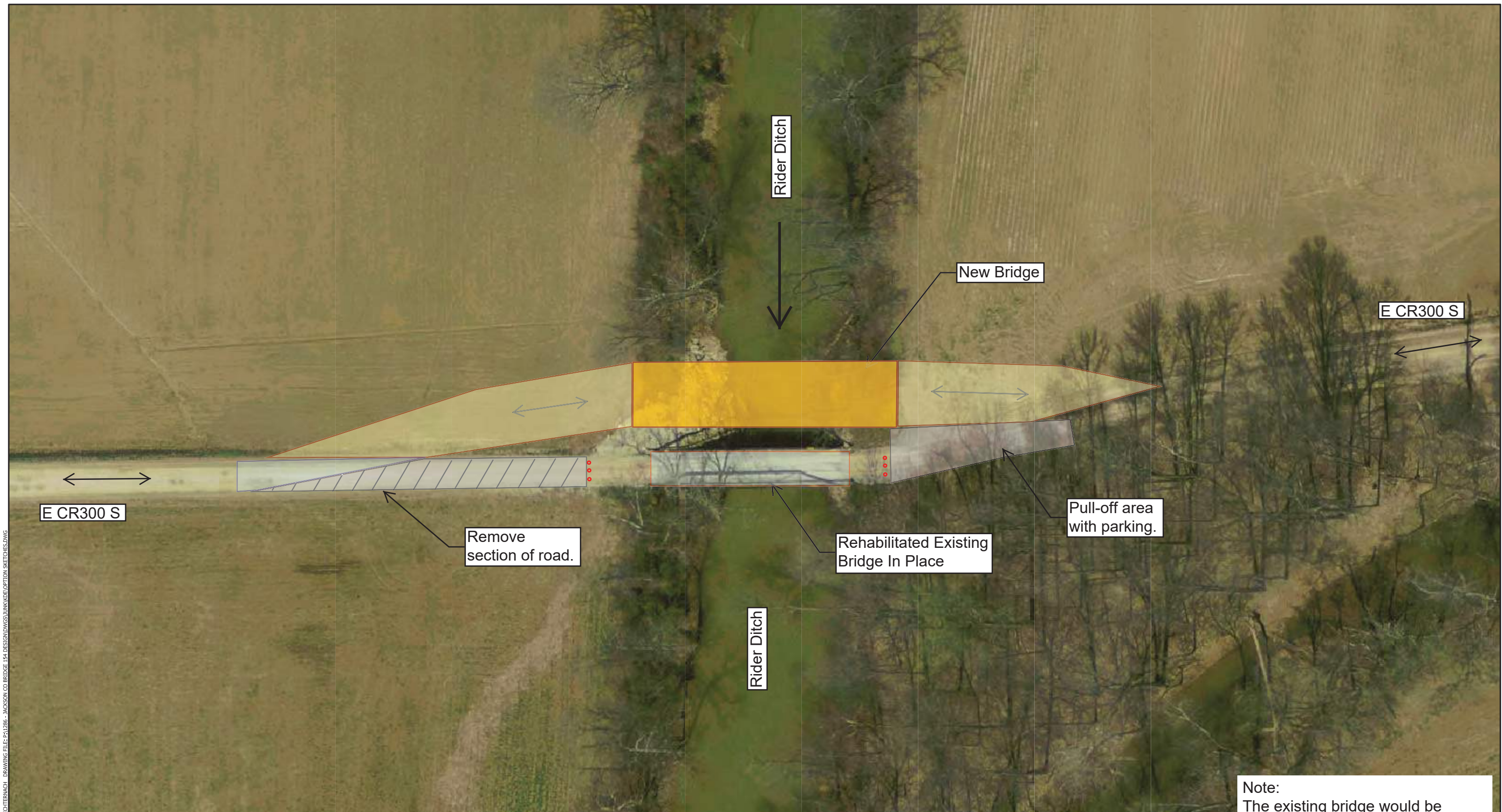
Note:
 Alternates C1 & C2 require rehabilitation of the existing structure. C1 is compliant with the SOIS standards and C2 is not.

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| INDIANA DEPARTMENT OF TRANSPORTATION |
| ALTERNATE C (1&2) OPTION LAYOUT |

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Alternate D:
Bypass Bridge Option with Rehabilitation for Pedestrians

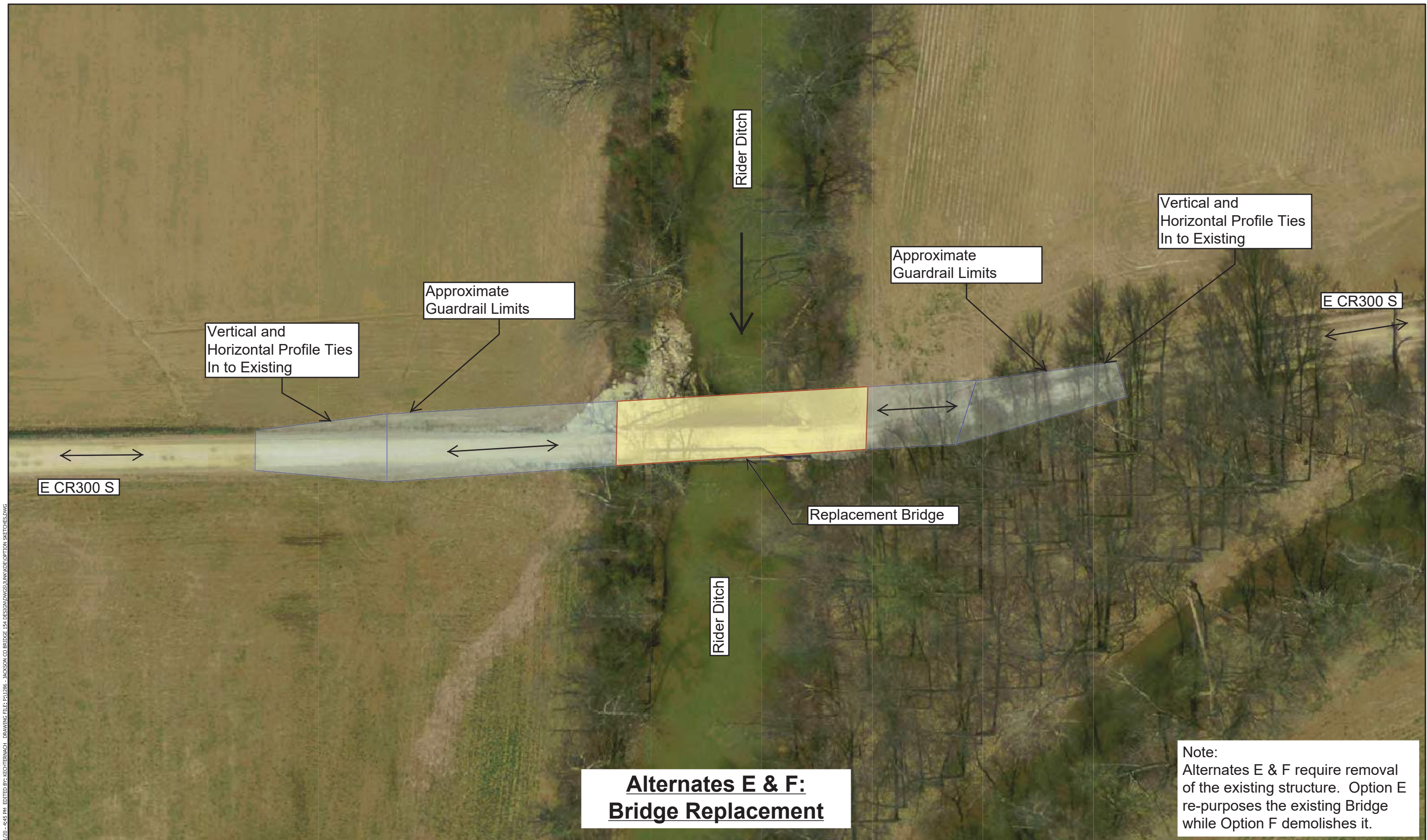
Note:
 The existing bridge would be rehabilitated to the extent necessary for non-vehicular traffic and in compliance with the SOIS.

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| ALTERNATE D OPTION LAYOUT | |

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**Alternates E & F:
Bridge Replacement**

Note:
 Alternates E & F require removal of the existing structure. Option E re-purposes the existing Bridge while Option F demolishes it.

| | | | |
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| Page 17 of 120 | RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER _____ DATE _____ | INDIANA DEPARTMENT OF TRANSPORTATION | HORIZONTAL SCALE BRIDGE FILE |
| | DESIGNED: _____ DRAWN: _____ CHECKED: _____ CHECKED: _____ | ALTERNATES E & F OPTION LAYOUT | VERTICAL SCALE NONE DESIGNATION |
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