

**FHWA-Indiana Environmental Document**  
**CATEGORICAL EXCLUSION / ENVIRONMENTAL ASSESSMENT FORM**  
**GENERAL PROJECT INFORMATION**

<b>Road No./County:</b>	US 27 / Wayne County
<b>Designation Number(s):</b>	1702941
<b>Project Description/Termini:</b>	US 27 over Norfolk Southern Railroad (NSRR) and Five Local Streets Bridge Project From 0.30 mile north of westbound US 40 to 0.48 mile north of westbound US 40

	<b>Categorical Exclusion, Level 2</b> – Required Signatories: INDOT DE and/or INDOT ESD
	<b>Categorical Exclusion, Level 3</b> – Required Signatories: INDOT ESD
<b>X</b>	<b>Categorical Exclusion, Level 4</b> – Required Signatories: INDOT ESD and FHWA
	<b>Environmental Assessment (EA)</b> – Required Signatories: INDOT ESD and FHWA
	<b>Additional Investigation (AI)</b> – The proposed action included a design change from the original approved environmental document. Required Signatories must include the appropriate environmental approval authority

**Approval**

_____	_____
INDOT DE Signature and Date	INDOT ESD Signature and Date
_____	
FHWA Signature and Date	

**Release for Public Involvement**

TD	10/1/2021	_____
INDOT DE Initials and Date		BDM / RFB INDOT ESD Initials and Date

**Certification of Public Involvement**

\_\_\_\_\_

INDOT Consultant Services Signature and Date

**INDOT DE/ESD Reviewer Signature and Date:** \_\_\_\_\_

**Name and Organization of CE/EA Preparer:** \_\_\_\_\_

Juliet Port, LPG / Parsons Transportation Group

# Indiana Department of Transportation

County Wayne

Route US 27

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## Part I – Public Involvement

Every Federal action requires some level of public involvement, providing for early and continuous opportunities throughout the project development process. **The level of public involvement should be commensurate with the proposed action.**

Does the project have a historic bridge processed under the Historic Bridges PA*?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
If No, then:		
Opportunity for a Public Hearing Required?	<input checked="" type="checkbox"/>	<input type="checkbox"/>

\*A public hearing is required for all historic bridges processed under the Historic Bridges Programmatic Agreement between INDOT, FHWA, SHPO, and the ACHP.

*Discuss what public involvement activities (legal notices, letters to affected property owners and residents (i.e. notice of entry), meetings, special purpose meetings, newspaper articles, etc.) have occurred for this project.*

Notice of Entry letters were mailed to potentially affected property owners near the project area on March 23, 2019 notifying them about the project and that individuals responsible for land surveying and field activities may be seen in the area. A sample copy of the Notice of Entry letter is included in Appendix G-1.

A draft *Public Involvement Plan* (PIP) was prepared by Parsons early in the project development process, which was concurred by the Indiana Department of Transportation (INDOT) on May 21, 2020. The purpose of the PIP was to establish goals and strategies for engaging with the public and key stakeholders in accordance with the current INDOT *Project Development Public Involvement Procedures Manual*. The PIP was updated in April 2021 to reflect changes in the project and INDOT policies, which was concurred by INDOT Environmental Services Division (ESD) on May 3, 2021. An updated PIP is provided in Appendix G-2 to G-11.

A public information meeting was held via webinar and in-person on August 27, 2020 at the Fourth Story Blues Club, adjacent to the project bridge at 923 North E Street in Richmond. An invitation letter was sent on August 12, 2020 to adjoining property owners and other stakeholders in accordance with the PIP (Appendix G-12 to G-13). The meeting was advertised on *Waynet.org* on August 13, 2020 (Appendix G-14 to G-15) and was featured in the *Richmond Palladium-Item* on August 17, 2020 (Appendix G-16). A total of 50 people attended the meeting either in-person or via webinar (Appendix G-17 to G-19). During the public meeting, project team members provided a handout (Appendix G-20 to G-21) and gave a presentation on the project's purpose and need, environmental analyses, and the recommended alternative (Appendix G-22 to G-29). Three public comments were received regarding bicycle accommodations, lighting, pedestrian access, and local subcontracting (Appendix G-33 to G-39).

A cultural resources (Section 106) consulting party (CP) meeting was also held via webinar and in-person on August 27, 2020 at the same location, prior to the public information meeting. A total of 26 people attended the meeting on-line or via webinar (Appendix G-32). During the meeting, questions were asked about maintenance of traffic (MOT), plans for the space beneath the bridge, design details including pedestrian access and aesthetics, the Section 106 process, impacts to nearby buildings, if there were potential issues from shock/vibrations during construction, the proposed bridge railing/fencing, and business access. The meeting summary and presentation are provided in Appendix D-109 to D-123. Additionally, one written comment was received regarding the project timing and MOT (Appendix G-40).

To meet the public involvement requirements of Section 106, a legal notice of the Federal Highway Administration's (FHWA's) finding of "No Adverse Effect" was published in the *Palladium-Item* on July 3, 2021 offering the public an opportunity to submit comment pursuant to 36 CFR 800.2(d), 800.3(e), and 800.6(a)(4). The public comment end-date was inadvertently omitted. Therefore, a second public notice was advertised in the *Palladium Item* on July 28, 2021. As advertised, the public comment period ended 30-days later on August 27, 2021. The texts of the public notices and affidavits of publication appear in Appendix D-171 to D-173 and D-179 to D-181. No comments were received.

The project will meet the minimum requirements described in the current INDOT *Project Development Public Involvement Procedures Manual*, which requires the project sponsor to offer the public an opportunity to submit comments and/or request a public hearing. Therefore, a legal notice will appear in a local publication contingent upon the release of this document for public involvement. This document will be revised after the public involvement requirements are fulfilled.

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## Public Controversy on Environmental Grounds

Discuss public controversy concerning community and/or natural resource impacts, including what is being done during the project to minimize impacts.

At this time, there is no substantial public controversy concerning impacts to the community or to natural resources.

## Part II - General Project Identification, Description, and Design Information

Sponsor of the Project: INDOT INDOT District: Greenfield

Local Name of the Facility: US 27 (locally known as "Chester Boulevard" north of the bridge and "North 9th Street" south of the bridge)

Funding Source (mark all that apply): Federal  State  Local  Other\*

\*If other is selected, please identify the funding source: \_\_\_\_\_

### PURPOSE AND NEED:

The need should describe the specific transportation problem or deficiency that the project will address. The purpose should describe the goal or objective of the project. The solution to the traffic problem should NOT be discussed in this section.

**Need:** The needs for this project stem from the deteriorated and substandard condition of the current structure, INDOT Structure No. 027-89-02136 B, and pedestrian facilities that do not meet current standards. The bridge was constructed in 1952 and has undergone rehabilitation twice since then, in 1978 and 1992. Per the INDOT Bridge Inspection Manual, bridge conditions are rated on a 0 to 9 numeric scale, with 0 representing "Failed Condition – out of service, bridge beyond corrective action" and 9 representing "Excellent Condition". According to the April 21, 2020 *Bridge Inspection Report*, although the superstructure has a rating of 6 out of 9 (satisfactory condition), the substructure was rated 4 out of 9 (poor condition) (Appendix I-1 to I-4). Concrete spalls are falling from the existing bridge substructure onto the roads and sidewalks below. Spalling, cracking, and delaminating concrete was noted throughout the structure. According to the April 2020, *Abbreviated Engineering Assessment*, the existing 17-span non-continuous bridge layout places unprotected piers in the middle of busy streets, creating sight distance issues, and there are areas of collision damage. The vertical clearances for local streets and the railroad are 13.0 feet and 22.0 feet, respectively, which are below the current standards of 14.5 feet and 23.0 feet. Additionally, the pedestrian facilities on the bridge are substandard, including a lack of fencing/handrails and Americans with Disabilities Act (ADA) requirements (Appendix I-4 to I-12).

**Purpose:** The purpose of the project is to provide a US 27 crossing over Norfolk Southern Railroad (NSRR) and local streets with an overall condition of at least 7 out of 9 (good condition), that meets current vertical clearance and sight distance standards, and to provide a pedestrian facility that meets current standards, including ADA.

### PROJECT DESCRIPTION (PREFERRED ALTERNATIVE):

County: Wayne Municipality: City of Richmond

Limits of Proposed Work: US 27, from 0.30 mile north of westbound US 40 (North D Street) to 0.48 mile north of westbound US 40 (100 feet north of North G Street)  
North F Street, from North 9<sup>th</sup> Street to Fort Wayne Avenue

Total Work Length: 0.18 Mile(s) Total Work Area: 1.35 Acre(s)

Is an Interstate Access Document (IAD)<sup>1</sup> required?  
If yes, when did the FHWA provide a Determination of Engineering and Operational Acceptability?

Yes <sup>1</sup>	No
	X
Date: _____	

<sup>1</sup>If an IAD is required; a copy of the approved CE/EA document must be submitted to the FHWA with a request for final approval of the IAD.

This is page 3 of 31 Project name: US 27 Bridge over NSRR and Local Streets Date: September 30, 2021

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*Describe location of project including township, range, city, county, roads, etc. Existing conditions should include current conditions, current deficiencies, roadway description, surrounding features, etc. Preferred alternative should include the scope of work, anticipated impacts, and how the project will meet the Purpose and Need. Logical termini and independent utility also need discussed.*

**Location:** INDOT, with funding from FHWA, is planning a bridge project on US 27 over NSRR and local streets in the City of Richmond, Wayne County, Indiana (Appendix B-1 and B-2). Specifically, the project is located in the United States Geological Survey (USGS) Topographic Richmond Quadrangle Map, in Section 32 of Township 14 North, Range 1 West (Appendix B-3).

**Existing Conditions:** This section of US 27 is classified as an Urban (Built Up) Arterial. In addition to crossing NSRR, the US 27 bridge crosses five local streets. Of those five, North E Street and Fort Wayne Avenue are both classified as Major Collectors; two are Local Roads, Elm Place and North F Street; and the fifth is an alley completely covered by the existing bridge that connects Elm Place and North E Street. This structure was originally built in 1952 to provide a grade-separated route for vehicle and pedestrian connectivity to downtown Richmond over a busy railyard with 10 sets of tracks. While the railroad's presence has diminished since the 1950s, there are still two sets of active tracks that pass under the US 27 bridge.

The project area is urban and surrounded by a mix of commercial, industrial, and residential properties. Portions of the project area are within the Richmond Railroad Station Historic District, and the Starr Historic District is adjacent to the east. This includes the John W. Grubbs Building, 831 North E Street (owned by Richmond Abilities), which abuts the project area at the southwest corner of North E Street and the alley beneath the bridge (Appendices B-6 and D-30 to D-31). The municipal North 10th Street Park abuts the northern project area, and construction of the Complete Street's Loop Project (Des. 1702679) (hereinafter referred to as the Loop Project) is planned along the south side of North E Street before this project is scheduled to begin.

As discussed in the Purpose and Need section, the existing bridge, INDOT Structure No. 027-89-02136 B, is deteriorating, and it has substandard vertical clearances and pedestrian facilities. This 17-span complex, non-continuous bridge is approximately 790.5 feet long and 61 feet wide (out-to-out coping). The span lengths and construction types vary; lengths range from approximately 36.4 to 93.2 feet (Appendix B-13). The existing skews range from zero to approximately 55.75 degrees. The structure carries two 12-foot wide travel lanes in each direction, with curb and gutter. The speed limit along this section of US 27 is 35 miles per hour (mph). The current minimum vertical clearance over local roads and the railroad are 13.0 feet and 22.0 feet, respectively. Open steel beam bridge railing is present along both sides of the structure.

The southern bridge abutment is south of Elm Street. US 27 has retaining walls along the southern approach and ties into the surrounding grade at the southern project terminus, at the intersection of US 27 and North D Street. The northern bridge abutment is north of North F Street, and the bridge approach is on an elevated grassy embankment that ties into the surrounding grade at the northern project terminus, approximately 100 feet north of North G Street.

There are 4.5-foot wide sidewalks on both sides of the bridge, which connect to concrete staircases that are present on all four quadrants. The southern staircases are local facilities and are outside the project area. The two northern staircases are located within the project area, along the grassy embankment at the northern bridge abutment. The western sidewalk extends further north beyond the project area where it connects to adequate crosswalks. However, the eastern sidewalk terminates at the irregular intersection of North H Street, US 27, and Chester Boulevard, where there are no crosswalk connections (Appendix B-8).

North F Street is a two-way local roadway that has 11-foot wide lanes and 4-foot wide sidewalks on each side under the bridge. The bridge also passes over Fort Wayne Avenue, North E Street, Elm Place, and the alley. These two-way streets have two lanes with curb and gutter, and sidewalks on each side. Currently there are bridge piers abutting and/or within these streets and sidewalks. Project area photographs are provided in Appendix B- 4 and B-5.

### **Preferred Alternative: Seven-Span Steel Plate Girder Replacement**

The preferred alternative will remove the existing US 27 bridge and replace it with a new bridge matching the existing in width and height. The new bridge will be a steel plate girder bridge with a reinforced concrete deck, on a zero to nine degree skew. The bridge will have seven spans, ranging in length from 65.8 feet to 193.5 feet; see the plan sheets for details (Appendix B-15 to B-18). The minimum vertical clearance over local roads and the railroad will be 14.5 feet and 23 feet, respectively. Annotated aerial imagery is provided in Appendix B-6 to B-8.

The new bridge will provide two 12-foot wide travel lanes in each direction. However, the typical section will differ from the existing by including 2.0- to 2.5-foot wide shoulders and a 6.5-foot wide sidewalk along the western side, and no sidewalk along the eastern side. Fencing will extend continuously on the west coping from Elm Place north for 450 feet, stopping just short of Fort Wayne Avenue. The purpose of the fencing is to address safety concerns for both the historic building at the southwest corner of North E Street and the alley, as well as for the railroad. Fencing will extend a minimum of ten feet tall vertically from the top of the sidewalk.



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The southern bridge abutment will be reused, which is in good condition according to the 2019 *Final Engineer's Report* (Appendix I-36 to I-37). The northern bridge abutment will be relocated approximately 50 feet south to the current location of North F Street, which will be closed permanently to vehicular traffic from North 9th Street to Fort Wayne Avenue. This will reduce the existing section of North F Street from two blocks, from North 8th Street to Fort Wayne Avenue, to one block, from North 8th Street to North 9th Street. Due to the presence of multiple subgrade utilities beneath North F Street, several relocations are needed including water, storm sewer, sanitary sewer, communication, and natural gas lines. The preferred alternative includes water and sanitary sewer utility work in-contract within the limits of construction (Appendix B-13).

The eastern sidewalk will be removed up to the irregular intersection of North H Street, US 27, and Chester Boulevard. The northeastern staircase will be removed, and sidewalk will be reconstructed to tie into the existing northwestern staircase. A new sidewalk will be constructed south of the new bridge abutment to continue to allow pedestrians east-west access from Fort Wayne Avenue to North F Street/North 9th Street. Annotated aerial photograph maps are provided as Appendix B-6 to B-8.

Regarding the alley beneath the bridge, the City of Richmond desires to close the alley to vehicular traffic, and the Loop Project is scheduled for construction before the US 27 bridge project. Therefore, the project team has coordinated with the City and the Loop Project regarding design and construction sequencing. The Loop Project will close the north end of the alley first, installing new sidewalk, a bike path, and curb and gutter to close access to North E Street. Temporary concrete barriers will be placed on the southern end of the alley to restrict traffic. The US 27 Project will then remove existing piers to two feet below grade, install bridge piers, install sidewalk and curb and gutter across the north side of Elm Place, and repair any damage to the sidewalk on the north end at North E Street (Appendix I-23 to I-24). No work is proposed for Fort Wayne Avenue, North E Street, or Elm Place, except the existing bridge piers will be removed, and any pavement, including sidewalk, will be repaired/reconstructed as needed.

The proposed project will occur almost entirely within existing, previously disturbed right-of-way (ROW). Approximately 0.094 acre of permanent ROW, and approximately 0.015 acre of temporary ROW, will be needed. During construction, the bridge will be closed and an official detour using I-70 and US 40 will be provided.

This project will not impact natural resources except for less than 0.10 acre of terrestrial habitat (maintained roadside), and it will have "No Adverse Effect" to cultural resources including the Historic Districts. Due to the need for temporary ROW for aerial space above a contributing building, *temporary occupancy* impacts are proposed for the Richmond Railway Station Historic District. There will be no impacts to the abutting North 10th Street Park, except for minor access changes, and impacts to the Loop Project have been minimized to the extent practicable. The project will permanently alter local traffic patterns by closing a portion of North F Street to vehicular traffic. This will reduce the existing section of North F Street from two blocks long (from North 8th Street to Fort Wayne Avenue) to one block long (from North 8th Street to North 9th Street).

The preferred alternative will meet the purpose and need of the project by providing a US 27 crossing over NSRR and local streets that will have an overall condition of at least 7 out of 9 (good condition), and meets vertical clearance and sight distance standards, and by providing a pedestrian facility that meets current standards, including ADA.

**Logical Termini/Independent Utility:** The US 27 bridge project termini, described above under Limits of Proposed Work, includes the bridge, its approaches, and areas beneath the bridge where work is proposed, including North F Street. These termini encompass areas that may be impacted by the project. The proposed bridge work will connect to the existing network of streets, and the proposed sidewalk work will improve connectivity by providing an ADA-compliant facility and maintaining pedestrian access beneath both sides of the bridge. Therefore, this project has rational endpoints and is of sufficient length to address any environmental impacts related to its design and construction. This project is a reasonable expenditure even if no additional transportation improvements in the area are made, and it should not restrict consideration of alternatives for other reasonably foreseeable transportation improvements. Therefore, this project meets FHWA criteria for independent utility and logical termini ([www.environment.fhwa.dot.gov/legislation/nepa/guidance\\_project\\_termini.aspx](http://www.environment.fhwa.dot.gov/legislation/nepa/guidance_project_termini.aspx)).

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### OTHER ALTERNATIVES CONSIDERED:

*Provide a header for each alternative. Describe all discarded alternatives, including the No Build Alternative. Explain why each discarded alternative was not selected. Make sure to state how each alternative meets or does not meet the Purpose and Need and why.*

Based on the *Abbreviated Engineering Assessment*, approved September 2020, several alternatives, including the No Build and the preferred alternative, Seven-Span Steel Plate Girder Replacement, were considered for this project (Appendix I-5 to I-12). The other alternatives considered for this project are summarized below.

#### Alternative 1: No Build

This alternative would leave the bridge and its pedestrian facilities in their current condition. This alternative would not impact North F Street, it would incur no costs, and it would not impact any historic or natural resources. However, the bridge would continue to deteriorate, accessibility issues for pedestrians would remain, and there would still be vertical clearance and sight distance issues for local streets and the railroad. Since this alternative does not meet the purpose and need of the project, it was dismissed from further consideration.

#### Alternative 2: Bridge Rehabilitation

This alternative would seek to rehabilitate the existing structure, including pedestrian facilities. This alternative would not impact North F Street. However, as discussed in the Purpose and Need section, the substructure is already in poor condition with advanced deterioration, and the existing layout creates sight distance and vertical clearance issues. Since this alternative would not meet the purpose and need of the project, it was dismissed from further consideration.

#### Alternative 3: Bridge Replacement with Sidewalks on Both Sides

This alternative would replace the current structure on the same alignment with a structure that could accommodate sidewalks on both sides of the bridge that would meet current standards. This alternative would meet the project's purpose and need. However, based on the capacity analysis, the number of travel lanes on the bridge should not be reduced. Therefore, widening the structure was considered; however, that would require relocation of the historic John W. Grubbs Building that abuts the southern portion of the bridge. Since this alternative would likely cause an adverse effect to, and Section 4(f) use of, the Richmond Railroad Station Historic District, it was dismissed from further consideration.

#### Alternative 4: Eight-Span Prestressed Composite Hybrid Bulb-Tee Bridge Replacement

This alternative would replace the current structure on the same alignment with an eight-span, composite, prestressed hybrid bulb-tee beam structure with a reinforced concrete deck. Supports would have varying skews, up to 55.75 degrees, and span lengths would range from 48 to 150 feet. This alternative would meet the project's purpose and need, and it would not impact North F Street. However, in order to meet vertical clearances, this alternative would require an approximate grade raise of three feet on the southern end of the bridge, which would require reconstruction of the North D Street intersection and could adversely affect the Historic Districts. Additionally, based on the life cycle cost analysis, this alternative's net present value (NPV), \$18,722,753, was estimated to be higher than the preferred alternative's NPV of \$16,009,026. This alternative was predicted to have greater costs and possible adverse effects to historic districts compared to the preferred alternative; therefore, it was dismissed from further consideration.

#### Alternative 5: Eight-Span W-Beam and Steel Plate Girder Bridge Replacement

This alternative would replace the current structure on the same alignment with an eight span, W-shape beam and hybrid steel plate girder structure with a reinforced concrete deck. Supports would have varying skews, up to 55.75 degrees, and span lengths would range from 48 to 150 feet. This alternative would meet the project's purpose and need, and it would not impact North F Street. However, the severe 55.75-degree skew on the north end of the bridge would create maintenance issues, and the skewed bents also present challenges in substructure design. Based on the life cycle cost analysis, this alternative's NPV, \$15,888,641, was estimated to be about the same as the preferred alternative's NPV, \$16,009,026. Since this alternative has complexity and maintenance issues compared to the preferred alternative, it was dismissed from further consideration.

#### The No Build Alternative is not feasible, prudent or practicable because (Mark all that apply):

- It would not correct existing capacity deficiencies;
- It would not correct existing safety hazards;
- It would not correct the existing roadway geometric deficiencies;
- It would not correct existing deteriorated conditions and maintenance problems; or
- It would result in serious impacts to the motoring public and general welfare of the economy.
- Other (Describe):

X
X

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### ROADWAY CHARACTER:

*If the proposed action includes multiple roadways, complete and duplicate for each roadway.*

Name of Roadway	<u>US 27</u>			
Functional Classification:	<u>Urban Arterial</u>			
Current ADT:	<u>14,077</u>	<u>VPD (2022)</u>	Design Year ADT:	<u>15,773</u> <u>VPD (2042)</u>
Design Hour Volume (DHV):	<u>1,486</u>	Truck Percentage (%)	<u>10</u>	
Designed Speed (mph):	<u>35</u>	Legal Speed (mph):	<u>35</u>	

	Existing		Proposed
Number of Lanes:	4		4
Type of Lanes:	Through		Through
Pavement Width:	48	ft.	52.5
Shoulder Width:	N/A	ft.	2 to 2.5
Median Width:	N/A	ft.	N/A
Sidewalk Width (west):	4.5	ft.	6.5
Sidewalk Width (east):	4.5	ft.	N/A

Setting:	<input checked="" type="checkbox"/> Urban	<input type="checkbox"/> Suburban	<input type="checkbox"/> Rural
Topography:	<input checked="" type="checkbox"/> Level	<input type="checkbox"/> Rolling	<input type="checkbox"/> Hilly

Name of Roadway	<u>North F Street (one block, from 9th Street to Fort Wayne Avenue)</u>			
Functional Classification:	<u>Local Road</u>			
Current ADT:	<u>N/A*</u>	<u>VPD (N/A)</u>	Design Year ADT:	<u>N/A*</u> <u>VPD (N/A)</u>
Design Hour Volume (DHV):	<u>N/A*</u>	Truck Percentage (%)	<u>N/A*</u>	
Designed Speed (mph):	<u>N/A*</u>	Legal Speed (mph):	<u>25</u>	

\*Not available for this local facility.

	Existing		Proposed
Number of Lanes:	2		0 (permanent road closure, one block long)
Type of Lanes:	Through		None
Pavement Width:	28	ft.	0
Shoulder Width:	N/A	ft.	0
Median Width:	N/A	ft.	N/A
Sidewalk Width (north):	4	ft.	N/A
Sidewalk Width (south):	4	ft.	6.5

Setting:	<input checked="" type="checkbox"/> Urban	<input type="checkbox"/> Suburban	<input type="checkbox"/> Rural
Topography:	<input checked="" type="checkbox"/> Level	<input type="checkbox"/> Rolling	<input type="checkbox"/> Hilly

Although the bridge passes over Fort Wayne Avenue and North E Street, no work to these streets is proposed other than the removal of the adjacent bridge piers and related pavement reconstruction (including sidewalks). Regarding Elm Place and the alley beneath the bridge, no work is proposed except similar pier removal/patching work, and changes to vehicular and pedestrian access that is being coordinated with the City of Richmond and the Loop Project (Appendix I-23 to I-24); refer to the Project Description and Community Impacts sections for further discussion.

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### BRIDGES AND/OR SMALL STRUCTURE(S):

If the proposed action includes multiple structures, complete and duplicate for each bridge and/or small structure. Include both existing and proposed bridge(s) and/or small structure(s) in this section.

Structure/NBI Number(s): Existing: 027-89-02136 B / 007200      Sufficiency Rating: 38.3 (April 21, 2020 *Bridge Inspection Report, Appendix I-4*)  
 Proposed: 027-89-02814 / (TBD)      (Rating, Source of Information)

	Existing		Proposed	
Bridge/Structure Type:	Complex		Steel Plate Girder	
Number of Spans:	17		7	
Weight Restrictions:	N/A	ton	N/A	ton
Height Restrictions:	N/A	ft.	N/A	ft.
Curb to Curb Width:	48	ft.	52.5	ft.
Outside to Outside Width:	61	ft.	61	ft.
Shoulder Width:	N/A	ft.	2 to 2.5	ft.

Describe impacts and work involving bridge(s), culvert(s), pipe(s), and small structure(s). Provide details for small structure(s): structure number, type, size (length and dia.), location and impacts to water. Use a table if the number of small structures becomes large. If the table exceeds a complete page, put it in the appendix and summarize the information below with a citation to the table.

This project will replace the existing US 27 bridge, INDOT Structure No. 027-89-02136 B, National Bridge Inventory (NBI) 007200, with a new bridge, INDOT Structure No. 027-89-02814, which will match the existing in width and height. The existing bridge was determined not to be eligible for the National Register of Historic Places (NRHP) in the *Indiana Historic Bridges Inventory* (Appendix D-8).

The new bridge will be a seven-span steel plate girder bridge on a zero to nine degree skew. The spans will range in length from 65.8 feet to 193.5 feet; see the plan sheets for details (Appendix B-15 to B-18). The area is served by municipal subgrade sewers. No water resources are associated with this structure. No further bridge or structure work is proposed.

### MAINTENANCE OF TRAFFIC (MOT) DURING CONSTRUCTION:

Is a temporary bridge proposed?

Yes

No

Is a temporary roadway proposed?



Will the project involve the use of a detour or require a ramp closure? (describe below)



Provisions will be made for access by local traffic and so posted.



Provisions will be made for through-traffic dependent businesses.



Provisions will be made to accommodate any local special events or festivals.



Will the proposed MOT substantially change the environmental consequences of the action?



Is there substantial controversy associated with the proposed method for MOT?

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*Discuss closures and/or facilities (if any) that will be provided for maintenance of traffic. Any known impacts from these temporary measures should be quantified to the extent possible, particularly with respect to properties such as Section 4(f) resources and wetlands. Any local concerns about access and traffic flow should be detailed as well.*

The MOT for the project will require a full closure of the US 27 bridge. An official detour will be provided using US 40 and I-70, a distance of approximately 8.8 miles (Appendix B-11). The MOT will be implemented per *Indiana Design Manual* guidelines.

During construction, the contractor will be required to maintain access to North E Street, its sidewalk and bike lane (Loop Project), and surrounding businesses during the daytime. Likewise, Elm Place and Fort Wayne Avenue will remain open to through-traffic and pedestrians. Short-term nighttime closures will be restricted to activities needed to safely remove foundations, overhead hazards, and set new steel-beams. The contractor will be required to coordinate the MOT with local stakeholders including the City of Richmond, school corporations, and emergency services. These conditions are included in the Environmental Commitments section of this CE document.

The closures of US 27, Elm Place, North E Street, and Fort Wayne Avenue will pose a temporary inconvenience to traveling motorists (including school buses and emergency services); however, no significant delays are anticipated, and all inconveniences and delays will cease upon project completion.

As previously discussed, in coordination with the City of Richmond, both the alley beneath the bridge from Elm Place to North E Street, and North F Street from 9th Street to Fort Wayne Avenue will be permanently closed to vehicular traffic; see the Project Description and Community Impacts sections for further discussion.

On April 27, 2021, the Indiana Department of Natural Resources (IDNR), Division of Historic Preservation and Archaeology (DHPA), responded to the March 30, 2021 Section 106 Effects Finding letter with questions about the proposed MOT regarding options for alternate detours on the west side of town, other north-south railroad crossings, and if there would be any potential effects to historic properties or districts (Appendix D-158 to D-160).

ASC Group, Inc. (ASC) responded on behalf of INDOT on May 3, 2021, stating there are no state-owned routes on the west side of Richmond, and it is anticipated that any non-local traffic will use the official detour. Regarding the north-south streets that cross the railroad tracks, there are three local roads: 12th Street and 15th Street, which pass through the Starr Historic District; and 20th Street. The 20th Street route is grade-separated, and would allow access to E Street and thus back into the Starr Historic District, and then into the Richmond Railroad Station Historic District. Heavy truck traffic on any of these North-South Streets could have potential effects on the historic districts and/or individual properties within the districts. The official detour is intended ensure trucks avoid utilizing the local streets (Appendix D-177 to D-178).

On May 5, 2021, IDNR-DHPA responded that the response was satisfactory (Appendix D-177); see the Cultural Resources section for further discussion of Section 106. On January 15, 2020, early coordination letters (ECLs) were sent to local stakeholders including schools and emergency services (Appendix C-1 to C-5), see the Early Coordination Section for details. No further comments regarding the proposed MOT were received.

### ESTIMATED PROJECT COST AND SCHEDULE:

Engineering: \$ 1,500,000 (2020) Right-of-Way: \$ 300,000 (2021) Construction: \$ 11,000,000 (2023)  
Anticipated Start Date of Construction: Spring, 2023 (Appendix H-1)

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### RIGHT OF WAY:

Land Use Impacts	Amount (acres)	
	Permanent	Temporary
Residential	N/A	N/A
Commercial	0.094	0.015
Agricultural	N/A	N/A
Forest	N/A	N/A
Wetlands*	N/A	N/A
Other:		
Other:		
<b>TOTAL</b>	<b>0.094</b>	<b>0.015</b>

\*

*Describe both Permanent and Temporary right-of-way and describe their current use. Typical and Maximum right-of-way widths (existing and proposed) should also be discussed. Any advance acquisition, reacquisition or easements, either known or suspected, and their impacts on the environmental analysis should be discussed.*

The existing ROW primarily consists of the US 27 bridge and roadway approaches, including associated sidewalks, as well as the maintained grassy embankment and concrete staircases at the north abutment. Most of the land beneath the bridge is shared ROW, consisting of local streets and the alley (including pedestrian facilities), as well as the NSRR corridor. Additionally, there are parcels of INDOT-owned urban land north of the railroad, where piers are located. The existing ROW averages 31 to 40 feet wide from the US 27 centerline.

The project requires 0.094 acre of permanent ROW, and approximately 0.015 acre of temporary ROW. The proposed ROW is shown on the project plans (Appendix B-12 to B-15). The permanent ROW is needed from two commercial parcels and consists of built-up urban land. This includes an area of approximately 0.008 acre at the southwest corner of North F Street and Fort Wayne Avenue (Mosey Manufacturing) to accommodate the new abutment at North F Street. Approximately 0.086 acre of permanent ROW is also needed from the commercial parking lot beneath the bridge on the north side of North E Street (First Realty Group). Finally, approximately 0.015 acre of temporary ROW is needed to provide aerial working space above the historic John W. Grubbs building at the southwest corner of US 27 and North E Street (Abilities Richmond, Inc.). Further discussions about potential impacts to this structure is discussed in the Cultural Resources and Section 4(f) Resources/Section 6(f) Resources sections of this document.

If the scope of work or permanent or temporary ROW amounts change, INDOT ESD and the INDOT District Environmental Section will be contacted immediately.

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**Part III – Identification and Evaluation of Impacts of the Proposed Action**

**SECTION A - EARLY COORDINATION:**

List the date(s) coordination was sent and all resource agencies that were contacted as a part of the development of this Environmental Study. Also, include the date of their response or indicate that no response was received.

ECLs were sent on January 15, 2020 (Appendix C-1 to C-4).

<b>Agency</b>	<b>Dates Sent</b>	<b>Date Response Received</b>	<b>Appendix</b>
US Fish and Wildlife Service (USFWS)	January 15, 2020	January 23, 2020	C-6 to C-7
FHWA	January 15, 2020	No response received	N/A
US Housing and Urban Development	January 15, 2020	No response received	N/A
National Park Service	January 15, 2020	No response received	N/A
INDOT Office of Public Involvement	January 15, 2020	No response received	N/A
IDNR Division of Fish and Wildlife (DFW)	January 15, 2020	February 11, 2020	C-5
Indiana Department of Environmental Management (IDEM)*	January 15, 2020	January 15, 2020*	C-12 to C-19
Indiana Geological and Water Survey (IGWS)*	January 15, 2020	January 15, 2020*	C-9 to C-11
INDOT Greenfield District	January 15, 2020	No response received	N/A
City of Richmond Municipal Separate Storm Sewer Systems (MS4) Coordinator	January 15, 2020	No response received	N/A
City of Richmond, Parks & Recreation	January 15, 2020	No response received	N/A
Wayne County Highway Department	January 15, 2020	No response received	N/A
Wayne County Surveyor	January 15, 2020	No response received	N/A
City of Richmond, Infrastructure & Development	January 15, 2020	No response received	N/A
Mayor of Richmond	January 15, 2020	No response received	N/A
Richmond City Council	January 15, 2020	No response received	N/A
Wayne County Trustees	January 15, 2020	No response received	N/A
Richmond Community Schools	January 15, 2020	January 15, 2020	C-8
Seton Catholic Schools	January 15, 2020	No response received	N/A
Richmond Fire Department	January 15, 2020	No response received	N/A
Richmond Police Department	January 15, 2020	No response received	N/A
Wayne County Emergency Management	January 15, 2020	No response received	N/A
Rose View Transit – City of Richmond	January 15, 2020	No response received	N/A
Norfolk Southern Corporation	January 15, 2020	No response received	N/A
State Senator Raatz	January 15, 2020	No response received	N/A
State Representative Barrett	January 15, 2020	No response received	N/A

\* Electronic coordination

All applicable recommendations are included in the Environmental Commitments section of this CE document.

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### SECTION B – ECOLOGICAL RESOURCES:

**Streams, Rivers, Watercourses & Other Jurisdictional Features**

- Federal Wild and Scenic Rivers
- State Natural, Scenic or Recreational Rivers
- Nationwide Rivers Inventory (NRI) listed
- Outstanding Rivers List for Indiana
- Navigable Waterways

Presence


Impacts

Yes                  No


Total stream(s) in project area: N/A Linear feet      Total impacted stream(s): N/A Linear feet

Stream Name	Classification	Total Size in Project Area (linear feet)	Impacted linear feet	Comments (i.e. location, flow direction, likely Water of the US, appendix reference)
N/A	N/A	N/A	N/A	N/A
N/A	N/A	N/A	N/A	N/A

*Describe all streams, rivers, watercourses and other jurisdictional features adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if the streams or rivers are listed on any federal or state lists for Indiana. Include if features are subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.*

Based on the desktop review, the aerial map of the project area, and the RFI report (Appendix E-1 to E-13), there are seven streams, rivers, watercourses, or other jurisdictional features within the 0.5 mile search radius. That number was confirmed by the site visit on March 12, 2020 by Parsons. No streams, rivers, watercourses, or other jurisdictional features are present within or adjacent to the project area. Therefore, no impacts are expected.

IDNR-DFW responded to early coordination on February 11, 2020 with standard recommendations to protect water resources, such as erosion control and revegetating disturbed areas (Appendix C-5). All applicable recommendations are included in the Environmental Commitments section of this CE document.

**Open Water Feature(s)**

- Reservoirs
- Lakes
- Farm Ponds
- Retention/Detention Basin
- Storm Water Management Facilities
- Other: \_\_\_\_\_

Presence


Impacts

Yes                  No


*Describe all open water feature(s) identified adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if features are subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.*

Based on the desktop review, the aerial map of the project area, and the RFI report (Appendix E-1 to E-13), there are no open water features within the 0.5 mile search radius. That number was confirmed by the site visit on March 12, 2020 by Parsons. Storm water management facilities are subgrade and do not include open water features such as detention ponds. There are no open water features present within or adjacent to the project area. Therefore, no impacts are expected. Responses to early coordination did not contain recommendations applicable to open water features.



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<u>Presence</u>	<u>Impacts</u>	
<input type="checkbox"/>	<u>Yes</u>	<u>No</u>
	<input type="checkbox"/>	<input type="checkbox"/>

**Wetlands**

Total wetland area: N/A Acre(s) Total wetland area impacted: N/A Acre(s)

(If a determination has not been made for non-isolated/isolated wetlands, fill in the total wetland area impacted above.)

Wetland No.	Classification	Total Size (Acres)	Impacted Acres	Comments (i.e. location, likely Water of the US, appendix reference)
N/A	N/A	N/A	N/A	N/A
N/A	N/A	N/A	N/A	N/A

**Wetlands (Mark all that apply)**

- Wetland Determination
- Wetland Delineation
- USACE Isolated Waters Determination

Documentation


ESD Approval Dates


**Improvements that will not result in any wetland impacts are not practicable because such avoidance would result in** (Mark all that apply and explain):

- Substantial adverse impacts to adjacent homes, business or other improved properties;
- Substantially increased project costs;
- Unique engineering, traffic, maintenance, or safety problems;
- Substantial adverse social, economic, or environmental impacts, or
- The project not meeting the identified needs.


*Describe all wetlands identified adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if features are subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.*

Based on the desktop review (Appendix F-1), the aerial map of the project area, and the RFI report (Appendix E-1 to E-13), there are four wetlands within the 0.5 mile search radius. That number was confirmed by the site visit on March 12, 2020 by Parsons. No wetlands are present within or adjacent to the project area. Therefore, no impacts are expected. Responses to early coordination did not contain recommendations applicable to wetlands.

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	<u>Presence</u>	<u>Impacts</u>	
		Yes	No
<b>Terrestrial Habitat</b>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Total terrestrial habitat in project area: 0.10 Acre(s) Total tree clearing: 0.00 Acre(s)  
 Describe types of terrestrial habitat (i.e. forested, grassland, farmland, lawn, etc.) adjacent or within the project area. Include whether or not impacts will occur to habitat identified. Include total terrestrial habitat impacted and total tree clearing that will occur. Discuss measure to avoid, minimize, and mitigate if impacts will occur.

Based on a desktop review, a site visit on March 12, 2020 by Parsons, and the aerial map of the project area (Appendix B-3 to B-11), habitats within the project area consist of maintained grassy roadside. Dominant species include common species of grasses (*Festuca arundinacea* and *Setaria pumila*), clover (*Trifolium repens*), and broadleaf plantain (*Plantago major*). Most of the project area is paved.

A total of approximately 0.10 acre of terrestrial habitat will be disturbed by this project, primarily at the northern bridge abutment. Avoiding impacts to terrestrial habitat is not feasible because it is present within existing ROW, and the bridge and sidewalks need to be replaced. There are no trees within the project area; therefore, no tree trimming or clearing will occur. Mitigation is not anticipated.

Responses to early coordination did not contain applicable recommendations regarding terrestrial habitat, except standard recommendations to revegetate disturbed areas.

**Protected Species**

**Federally Listed Bats**

Information for Planning and Consultation (IPaC) determination key completed  
 Section 7 informal consultation completed (IPaC cannot be completed)  
 Section 7 formal consultation Biological Assessment (BA) required

	Yes	No
	<input checked="" type="checkbox"/>	<input type="checkbox"/>
	<input type="checkbox"/>	<input type="checkbox"/>
	<input type="checkbox"/>	<input type="checkbox"/>

Determination Received for Listed Bats from USFWS: NE  NLAA  LAA

**Other Species not included in IPaC**

Additional federal species found in project area (based on IPaC species list)  
 State species (not bird) found in project area (based upon consultation with IDNR)

	Yes	No
	<input type="checkbox"/>	<input checked="" type="checkbox"/>
	<input type="checkbox"/>	<input checked="" type="checkbox"/>

**Migratory Birds**

Known usage or presence of birds (i.e. nests)  
 State bird species based upon coordination with IDNR

	Yes	No
	<input type="checkbox"/>	<input checked="" type="checkbox"/>
	<input type="checkbox"/>	<input checked="" type="checkbox"/>

\*Bird nests were observed but they are not associated with migratory birds; see discussion below.

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*Discuss IDNR coordination and species identified. Describe USFWS Section 7 consultation and determination received for Indiana bat and northern long-eared bat impacts. Discuss if other federally listed species were identified. If so, include consultation that has occurred and the determination that was received. Discuss if migratory birds have been observed and any impacts.*

Based on a desktop review and the RFI report (Appendix E-1 to E-13) approved by INDOT on June 21, 2019, the IDNR Wayne County Endangered, Threatened and Rare (ETR) Species List has been checked. According to the IDNR-DFW early coordination response letter dated February 11, 2020 (Appendix C-5), the Natural Heritage Program's Database has been checked, and no plant or animal species listed as state or federally threatened, endangered, or rare have been reported to occur in the project vicinity.

Project information was submitted through the USFWS's Information for Planning and Consultation (IPaC) portal, and an official species list was generated (Appendix C-20 to C-25). The project is within range of the federally endangered Indiana bat (*Myotis sodalis*) and the federally threatened northern long-eared bat (NLEB) (*Myotis septentrionalis*). No additional species were generated in the IPaC species list other than the Indiana bat and NLEB.

The project qualifies for the Range-wide Programmatic Informal Consultation for the Indiana bat and NLEB, dated May 2016 (revised February 2018), between FHWA, the Federal Railroad Administration (FRA), the Federal Transit Administration (FTA), and USFWS.

Bridge inspections occurred on March 12, 2020, April 21, 2020, and May 8, 2021, and no evidence for the presence of bats was reported (Appendix C-23 to C-42). An effect determination key was completed on March 18, 2021, and based on the responses provided, the project was found to "Not Likely to Adversely Affect" the Indiana bat and/or the NLEB (Appendix C-30 to C-36). INDOT reviewed and verified the effect finding on March 18, 2020, and requested USFWS's review of the finding. No response was received from USFWS within the 14-day review period; therefore, it was concluded they concur with the finding. The Avoidance and Minimization Measures (AMMs) for this project are General AMM 1 and Lighting AMM 1. These AMMs are included as firm commitments in the Environmental Commitments section of this document.

INDOT Structure No. 027-089-02814 B was reported to have bird nests in the April 21, 2020 INDOT *Bridge Inspection Report* (Appendix C-39 to C-40). At the direction of INDOT, environmental staff from Parsons re-inspected the bridge on May 8, 2021 and identified common pigeons (*Columba livia*) nesting on the structure (Appendix C-41 to C-42). The inspection was provided to INDOT ESD on May 11, 2021. Per the USFWS Federal Register *List of Bird Species To Which the Migratory Bird Treaty Act Does Not Apply* (<https://www.govinfo.gov/content/pkg/FR-2020-04-16/pdf/2020-06782.pdf>), these birds are not protected by the Migratory Bird Treaty Act (MBTA). Therefore, no impacts to migratory birds are expected.

This precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act, as amended. If new information on endangered species at the site becomes available, or if project plans are changed, USFWS will be contacted for consultation.

### Geological and Mineral Resources

- Project located within the Potential Karst Features Area of Indiana
- Karst features identified within or adjacent to the project area
- Oil/gas or exploration/abandoned wells identified in the project area

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Date Karst Study/Report reviewed by INDOT EWPO (if applicable): \_\_\_\_\_

*Discuss if project is located in Potential Karst Features Area of Indiana and if any karst features have been identified in the project area (from RFI). Discuss response received from IGWS coordination. Discuss if any mines, oil/gas, or exploration/abandoned wells were identified and if impacts will occur. Describe if any impacts will occur to any karst features. Include discussion of karst study/report was completed and results. (Karst investigation must comply with the current Karst MOU and coordinated and reviewed by INDOT EWPO)*

Based on a desktop review, the project is located outside the designated karst region of Indiana as outlined in the October 13, 1993 Karst Memorandum of Understanding (MOU). According to the topographic map of the project area (Appendix B-2) and the RFI report (Appendix E), there are no karst features identified within or adjacent to the project area. In the early coordination response dated January 15, 2020, the IGWS did not indicate that karst features exist in the project area (Appendix C-9 to C-11). The IGWS reported a moderate liquefaction potential, low potential for bedrock resources, low potential for sand and gravel resources, and a floodway. Response from IGWS has been communicated with the designer on March 17, 2020. No impacts are expected.

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### SECTION C – OTHER RESOURCES

**Drinking Water Resources**

- Wellhead Protection Area(s)
- Source Water Protection Area(s)
- Water Well(s)
- Urbanized Area Boundary
- Public Water System(s)

**Presence**

X
X

**Impacts**

Yes	No
	X
	X

Is the project located in the St. Joseph Sole Source Aquifer (SSA):

If Yes, is the FHWA/EPA SSA MOU Applicable?

If Yes, is a Groundwater Assessment Required?

Yes	No

*Check the appropriate boxes and discuss each topic below. Provide details about impacts and summarize resource-specific coordination responses and any mitigation commitments. Reference responses in the Appendix.*

The project is located in Wayne County, which is not located within the area of the St. Joseph Sole Source Aquifer, the only legally designated sole source aquifer in the state of Indiana. Therefore, the FHWA/Environmental Protection Agency (EPA) Sole Source Aquifer MOU is not applicable to this project, a detailed groundwater assessment is not needed, and no impacts are expected.

IDEM's Wellhead Proximity Determinator website (<http://www.in.gov/idem/cleanwater/pages/wellhead/>) was accessed on May 14, 2021 by Parsons. This project is not located within a Wellhead Protection Area or Source Water Area.

The IDNR Water Well Record Database website (<https://www.in.gov/dnr/water/3595.htm>) was accessed on May 14, 2021 by Parsons. No wells are located near this project. Therefore, no impacts are expected.

Based on the RFI report approved by INDOT on June 21, 2019, this project is located in an Urban Area Boundary (Appendix E-1 to E-13). An ECL was sent to the local MS4 Coordinator on January 15, 2021 (Appendix C-1 to C-4). The MS4 Coordinator did not respond within the 30-day timeframe. Utility coordination is ongoing due to the need for water and sewer relocations at North F Street. A copy of the Utility Coordination Log is provided in Appendix I-35. The project includes storm water system improvements that will comply with 327 IAC 15-13 (Rule 13). Therefore, no impacts are expected.

Based on a desktop review, the March 12, 2020 site visit by Parsons, the aerial map of the project area (Appendix B-2), and utility coordination, this project is located where there is a public water system. The public water system will not be affected because utility coordination is occurring and there will be no disruption to service (Appendix I-35). The City of Richmond did not respond to early coordination (Appendix C-1 to C-4).

**Floodplains**

- Project located within a regulated floodplain
- Longitudinal encroachment
- Transverse encroachment
- Homes located in floodplain within 1000' up/downstream from project

**Presence**


**Impacts**

Yes	No

If applicable, indicate the Floodplain Level?

Level 1     Level 2     Level 3     Level 4     Level 5

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Use the IDNR Floodway Information Portal to help determine potential impacts. Include floodplain map in appendix. Discuss impacts according to the classification system. If encroachment on a flood plain will occur, coordinate with the Local Flood Plain Administrator during design to insure consistency with the local flood plain planning.

The Indiana Department of Natural Resources Indiana Floodway Information Portal website (<http://dnrmaps.dnr.in.gov/appsphp/fdms/>) was accessed on March 8, 2021 by Parsons. This project is not located in a regulatory floodplain as determined from approved IDNR floodplain maps (Appendix F-2). Therefore, it does not fall within the guidelines for the implementation of 23 CFR 650, 23 CFR 771, and 44 CFR. No impacts are expected.

Farmland	<u>Presence</u>	<u>Impacts</u>	
		Yes	No
Agricultural Lands	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Prime Farmland (per NRCS)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Total Points (from Section VII of CPA-106/AD-1006*) _____			
*If 160 or greater, see CE Manual for guidance.			

Discuss existing farmland resources in the project area, impacts that will occur to farmland, and mitigation and minimization measures considered.

Based on a desktop review, a site visit on March 12, 2020 by Parsons, and the aerial map of the project area (Appendix B-2), there is no land that meets the definition of farmland under the Farmland Protection Policy Act (FPPA) within or adjacent to the project area. The requirements of the FPPA do not apply to this project; therefore, no impacts are expected. An ECL was sent on January 15, 2021 to NRCS (Appendix C-1 to C-4). No response was received.

### SECTION D – CULTURAL RESOURCES

<b>Minor Projects PA</b>	<u>Category(ies) and Type(s)</u> <input type="text"/>	<u>INDOT Approval Date(s)</u> <input type="text"/>	<u>N/A</u> <input checked="" type="checkbox"/>
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**Full 106 Effect Finding**

No Historic Properties Affected  No Adverse Effect  Adverse Effect

**Eligible and/or Listed Resources Present**

NRHP Building/Site/District(s)  Archaeology  NRHP Bridge(s)

Documentation Prepared (mark all that apply)	<u>ESD Approval Date(s)</u>	<u>SHPO Approval Date(s)</u>
APE, Eligibility and Effect Determination	<input checked="" type="checkbox"/>	<input type="checkbox"/>
800.11 Documentation	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Historic Properties Report or Short Report	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Archaeological Records Check and Assessment	<input type="checkbox"/>	<input type="checkbox"/>
Archaeological Phase Ia Survey Report	<input type="checkbox"/>	<input type="checkbox"/>
Archaeological Phase Ic Survey Report	<input type="checkbox"/>	<input type="checkbox"/>
Other: Phase Ib Archaeological Field Reconnaissance Survey and Mechanical Deep Testing Report	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Other: Cemetery Development Plan (CDP)	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Memorandum of Agreement (MOA)

**MOA Signature Dates** (List all signatories)

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If the project falls under the MPPA, describe the category(ies) that the project falls under and any approval dates. If the project requires full Section 106, use the headings provided. The completion of the Section 106 process requires that a Legal Notice be published in local newspapers. Please indicate the publication date, name of the paper(s) and the comment period deadline. Include any further Section 106 work which must be completed at a later date, such as mitigation from a MOA or avoidance commitments.

**Area of Potential Effect (APE):** Pursuant to 36 CFR 800.16(d), the Area of Potential Effect (APE) for aboveground resources extends along US 27 from H Street on the north to B Street on the south, and is bounded by 8th Street on the west and 11th and 10th streets on the east (Appendix D-5). The APE for archaeology was the project footprint.

**Coordination with Consulting Parties (CPs):** Early coordination was initiated on January 9 and 10, 2020 with a letter inviting organizations and individuals to become CPs (Appendix D-52 to D-62). The Indiana State Historic Preservation Officer (SHPO) from IDNR-DHPA is a designated CP. The following is a list of the organizations and individuals formally invited to become a CP (those organizations who accepted the invitation to become a CP are in bold):

- Richmond Mayor
- **Richmond City Council**
- Wayne Township Trustee
- Richmond Street Department
- Richmond Public Works and Engineering
- **Richmond Community Development**
- Wayne County Highway Department
- Wayne County Planning and Zoning Office
- Economic Development Corporation of Wayne County
- Richmond Historic Preservation Commission
- Indiana Landmarks, Eastern Regional Office
- Indiana National Road Association
- Preserve Richmond, Inc.
- Wayne County Historian
- Wayne County Genealogy Society
- Wayne County Historical Museum
- **Wayne County Area Chamber of Commerce**
- **Richmond Columbian Properties**
- **City of Richmond, Department of Infrastructure and Development**
- Center City Development Corporation
- **Historic Richmond Depot District Association**
- Eastern Shawnee Tribe of Oklahoma
- **Miami Tribe of Oklahoma**
- Peoria Tribe of Indians of Oklahoma
- Pokagon Band of Potawatomi Indians
- Delaware Tribe of Indians, Oklahoma
- Individual community members (landowners, etc., see Appendix D-18 to D-19)

The Miami Tribe of Oklahoma responded on January 21, 2020 accepting the invitation to serve as a CP and offering no objections (Appendix D-82). Richmond City Council 5<sup>th</sup> District responded on January 9, 2020 with questions about project timing and maintenance of traffic (Appendix D-63). Richmond Community Development responded on January 10, 2020 with additional contact information; the CP list was updated accordingly (Appendix D-66 to D-68). Members of the Historic Richmond Depot District Association responded separately on January 10, 2020 requesting to be a CP and providing additional contact information; the CP list was updated accordingly (Appendix D-69 to D-72 and D-75). Richmond Columbian Properties responded on January 11, 2020 requesting to be a CP (Appendix D-73 to D-74). The City of Richmond Department of Infrastructure and Development responded on January 14, 2020 requesting to be a CP (Appendix D-80). The Wayne County Chamber of Commerce responded on January 15, 2021 inquiring about the project schedule and MOT (Appendix D-135). On February 5, 2020, the Historic Richmond Depot District Association requested information on the proposed dates of construction for the project (Appendix D-86 to D-87).

SHPO responded to early coordination on January 28, 2020 stating they were not aware of additional CPs beyond those whom were already invited, and requesting detailed contact information (Appendix D-83 to D-84). A copy of this response including the requested contact information was forwarded to the CPs on January 29, 2020 (Appendix D-85 to D-86).

As discussed in the Public Involvement section, a CP meeting was held on August 27, 2020 (Appendix D-105 to D-123). During the meeting, questions were asked about MOT, plans for the space beneath the bridge, design details including pedestrian access and aesthetics, the Section 106 process, impacts to nearby buildings, if there were potential issues from shock/vibrations during

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construction, the proposed bridge railing/fencing, and business access. The meeting summary and presentation are provided in Appendix D-109 to D-123. SHPO responded to the CP Meeting Summary on September 21, 2020 with no additional comments (Appendix D-124 to D-126). No other CP responses were received.

**Archaeology:** Pursuant to 36 CFR § 800.4(b), staff from ASC conducted a *Phase Ib Archaeological Reconnaissance Survey and Mechanical Deep Testing Report*, which identified one new site (Appendix D-167 to D-169). The site was recommended not eligible for inclusion on the NRHP. No further work was recommended.

The results of the archaeology report were distributed to the CPs on January 14, 2021 (Appendix D-129 to D-131). The Wayne County Chamber of Commerce responded on January 15, 2021 inquiring about the project schedule and MOT (Appendix D-135). SHPO responded on February 15, 2021 and agreed that the site did not appear to be eligible for the NRHP, and that no further work is necessary (Appendix D-137 to D-138). No other CP responses were received.

Due to the presence of a historical cemetery that was formerly adjacent to the north of the project area, a *Cemetery Development Plan* (CDP) was developed in accordance with IC 14-21-1 (Appendix D-185 to D-187). In coordination with INDOT-CRO, the CDP includes proposed work by Vectren to move a natural gas line (even though this work is outside the project contract). This CDP was submitted to the SHPO on April 6, 2021 for review and concurrence. On May 6, 2021, SHPO responded that the plan is acceptable as submitted, with conditions outlined in the letter regarding unexpected discoveries, avoidance of the cemetery except as detailed, and any changes or modifications to the CDP be submitted to SHPO in writing for review and concurrence (D-162 to D-163). A copy of the CDP will be included with the contract documents and provided to Vectren. Applicable commitments are included in the Environmental Commitments section of this CE document.

**Historic Properties:** ASC prepared the *Historical Properties Report* (HPR) on April 27, 2020 (Appendix D-165 to D-166). The following three resources within the APE are listed in, or are eligible for listing in, the NRHP (see Appendix D-26 to D-50 for an aerial map and photographs):

Richmond Railroad Station Historic District: This district crosses the project area from the NSRR to Elm Place. It was listed in the NRHP in 1978 under Criterion A as a major late nineteenth and early twentieth century commercial district that developed in part due to its proximity to the railroad station, and under Criterion C for its architecturally significant group of commercial and industrial buildings from the period of significance (1853–1915).

Starr Historic District: This district is located nearly adjacent to the southeast of the project area. It was listed in the NRHP in 1974 under Criterion A as the location of homes of many of Richmond's leading nineteenth century industrialists and merchants, and under Criterion C as an intact collection of nineteenth century residential buildings from the period of significance of 1825–1875. The buildings in the district include examples of Greek Revival, Italianate, Second Empire, and Queen Anne.

Quaker Meeting House/Tenth Street Meeting House: This property is located northeast of the project area at the southeast corner of Chester Boulevard and G Street. It was constructed circa 1878 and is an example of "Quaker Italianate," a simplified version of the Italianate style adopted by the Society of Friends for their meeting houses in the second half of the nineteenth century. It was recommended eligible for the NRHP under Criterion A under the theme of religion, and under Criterion C as an example of the Italianate style as simplified by the Quakers in the design of meeting houses in the late-nineteenth century. It was also recommended eligible under Criteria Consideration (a) for its historical and architectural significance.

The HPR was distributed to CPs on April 28, 2020. SHPO responded on May 11, 2020 and agreed with the HPR's conclusions and recommendations (Appendix D-102 to D-104). The Historic Richmond Depot District Association responded by email on April 29, 2020 asking at what stage in the process would CPs be informed of recommendations of any alterations to the historic properties discussed in the HPR (Appendix D-100). No other CP responses were received.

**Documentation Findings:** INDOT, acting on behalf of FHWA, issued the following findings on June 29, 2021 (Appendix D-5 to D-22):

Richmond Railroad Station Historic District: "No Adverse Effect"

The US 27 bridge project will not cause physical destruction or damage to any contributing resource of the Richmond Railroad Station Historic District. The project will not require the destruction of any buildings. The project will not physically alter or remove, nor cause neglect to, any contributing resource of the Richmond Railroad Station Historic District, and it will not require the removal or alteration of any physical features that contribute to the historic district's significance. Additionally, ownership of contributing resources of the Richmond Railroad Station Historic District will not change as a result of this project.

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The existing US 27 bridge is an intrusion in the Richmond Railroad Station Historic District. The proposed replacement bridge will not differ in height or width from the existing bridge and will have fewer piers, thus somewhat reducing the visual impact of the bridge both within the historic district and beyond in the district's setting. The INDOT project team and stakeholders are in the process of discussing potential aesthetic treatments for some of the bridge's appurtenances, such as period replica light poles, the type of pedestrian fence to protect 831 North E Street, the design of fencing to be added over the railroad ROW, and treatments for the bridge railings, which should help to further reduce the visual impact of the bridge within the historic district. No additional comments have been received regarding the potential aesthetic treatments. The project is not adding capacity to the bridge and so will not introduce new atmospheric or audible elements into the historic district.

Care will be taken to avoid damage to the contributing building John W. Grubbs Building at 831 N. E Street that is immediately adjacent to the bridge. The piers for the southern portion of the bridge will have spread footings to avoid the need to drive piles in the historic district, thus reducing the risk of damage to buildings from vibration. Special provisions regarding vibrations are included as a firm commitment in the Environmental Commitments section.

### Starr Historic District: "No Adverse Effect"

The replacement of the US 27 bridge will not cause physical destruction or damage to any contributing resource of the Starr Historic District. The existing bridge is located approximately 110 feet from the boundary of the Starr Historic District at its closest point. The project will not physically impact, remove, or cause neglect to any properties in the Starr Historic District. The project will not cause a change in use of any resources within the historic district. Additionally, there will be no change in ownership of any contributing resource.

The existing US 27 bridge is not a physical feature in the setting of the Starr Historic District that contributes to its historic significance. The proposed new bridge's location will be on the same alignment as the existing bridge and, therefore, the new bridge will not impact physical features that contribute to the historic district's significance. Furthermore, it will have fewer piers, thus somewhat reducing the visual impact of the bridge within the historic district's setting. The project will not introduce new atmospheric or audible elements into the historic district or its setting.

### Quaker Meeting House/10th Street Meeting House: "No Adverse Effect"

The undertaking will not physically affect the Quaker Meeting House. There will be special provisions covering vibrations near buildings, so no damage as a result of vibrations is anticipated. The project will not physically impact, remove, or cause neglect to the Quaker Meeting House. The undertaking will not introduce an intrusive new visual element that will diminish the integrity of setting of the Quaker Meeting House, and it will not introduce new atmospheric or audible elements to the resource's setting. Additionally, there will be no change in ownership of the Quaker Meeting House.

Pursuant to 800.5(c), the SHPO concurred with these findings on July 19, 2021 (Appendix D-174 to D-176).

**Public Involvement:** A public notice of the "No Adverse Effect" finding was advertised in the *Palladium Item* on July 3, 2021. The public comment end-date was inadvertently omitted. Therefore, a second public notice was advertised in the *Palladium Item* on July 28, 2021. As advertised, the public comment period ended 30-days later on August 27, 2021. The texts of the public notices and affidavits of publication appear in Appendix D-171 to D-173 and D-179 to D-181. No comments were received.

The Section 106 process has been completed, and the responsibilities of the FHWA under Section 106 have been fulfilled.



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### SECTION E – SECTION 4(f) RESOURCES/ SECTION 6(f) RESOURCES

	<u>Presence</u>	<u>Use</u>	
		Yes	No
<b>Parks and Other Recreational Land</b>			
Publicly owned park	<b>X</b>		<b>X</b>
Publicly owned recreation area			
Other (school, state/national forest, bikeway, etc.)			
<b>Wildlife and Waterfowl Refuges</b>			
National Wildlife Refuge			
National Natural Landmark			
State Wildlife Area			
State Nature Preserve			
<b>Historic Properties</b>			
Site eligible and/or listed on the NRHP	<b>X</b>		<b>X</b>

#### Evaluations Prepared

Programmatic Section 4(f)	
“De minimis” Impact	
Individual Section 4(f)	
Any exception included in 23 CFR 774.13	<b>X</b>

*Discuss Programmatic Section 4(f) and “de minimis” Section 4(f) impacts in the discussion below. Individual Section 4(f) documentation must be included in the appendix and summarized below. Discuss proposed alternatives that satisfy the requirements of Section 4(f). FHWA has identified various exceptions to the requirement for Section 4(f) approval. Refer to 23 CFR § 774.13 - Exceptions.*

Section 4(f) of the U.S. Department of Transportation Act of 1966 prohibits the use of certain public and historic lands for federally funded transportation facilities unless there is no feasible and prudent alternative. The law applies to significant publicly owned parks, recreation areas, wildlife / waterfowl refuges, and NRHP eligible or listed historic properties regardless of ownership. Lands subject to this law are considered Section 4(f) resources.

Based on a desktop review, the aerial map of the project area (Appendix B-2), Section 106 Documentation (Appendix D), and the RFI report (Appendix E), there are eighteen potential Section 4(f) resources located within the 0.5 mile search radius. According to additional research (Appendix I-13 to I-32), Section 106 Documentation (Appendix D), and the March 12, 2020 site visit by Parsons, there are four Section 4(f) resources located within or adjacent to the project area: the Richmond Railroad Historic District, the Starr Historic District, the Quaker Meeting House/10th Street Meeting House, and North 10th Street Park. These resources are discussed further below. A Section 4(f) Analyses memorandum was prepared by Parsons on June 9, 2021 and concurred by INDOT District Environmental and ESD on July 26, 2021 (Appendix I-13 to I-32).

Two potential resources were evaluated and determined not to qualify as Section 4(f) resources: the Loop Project and the recommended Chester Boulevard Multi-Use Trail. The Loop Project is a multi-use trail planned for construction along the south side of North E Street beneath the US 27 bridge. It is owned by the City of Richmond. Per the FHWA *Section 4(f) Policy Paper* (<https://www.environment.fhwa.dot.gov/legislation/section4f/4fpolicy.pdf>), “If the publicly owned facility is primarily used for transportation and is an integral part of the local transportation system, the requirements of Section 4(f) would not apply since it is not a recreational area.” The *Richmond Parks and Recreation Master Plan* (Park Master Plan), adopted on October 22, 2020, states the Loop Project “was designed to create a balanced transportation system” (Appendix I-16 to I-21). Therefore, the requirements of Section 4(f) do not apply. Nonetheless, the proposed impacts to this facility have been reduced and minimized, see the Community Impacts section for further discussion.

The recommended Chester Boulevard Multi-use Trail is a potential trail segment identified in the 2015 *City of Richmond Bicycle and Pedestrian Master Plan* (Bike/Ped Master Plan) along US 27, locally known as Chester Boulevard, through the project area (from Sim Hodgin Parkway to North E Street). Based on coordination with the City of Richmond, this project has not been programmed. Per the FHWA *Section 4(f) Policy Paper*, “Section 4(f) applies when the land is one of the enumerated types of publicly owned lands and the public agency that owns the property has formally designated and determined it to be significant for park, recreation area, or

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wildlife and waterfowl refuge purposes." Since the recommended trail is not programmed, the requirements of Section 4(f) do not apply (Appendix I-13 to I-34). See the Community Impacts section for further discussion.

### Cultural Resources

As discussed in the Cultural Resources section, there are three resources within the APE that are listed, or eligible for listing, in the NRHP: the Starr Historic District, the Quaker Meeting House/10th Street Meeting House, and the Richmond Railroad Station Historic District. According to the FHWA *Section 4(f) Policy Paper*, Section 4(f) applies to "all historic sites that are listed, or eligible for inclusion, in the...[NRHP] at the local, state, or national level of significance regardless of whether or not the historic site is publicly owned or open to the public." Therefore, Section 4(f) applies to all three resources. See the Cultural Resources section and Appendix D for a discussion of each resource's characteristics.

According to 23 CFR 774.17, a Section 4(f) "Use" is defined as

Except as set forth in §§ 774.11 and 774.13, a "use" of Section 4(f) property occurs: (1) When land is permanently incorporated into a transportation facility; (2) When there is a *temporary occupancy* of land that is adverse in terms of the statute's preservation purpose as determined by the criteria in § 774.13(d); or (3) When there is a *constructive use* of a Section 4(f) property as determined by the criteria in § 774.15.

Starr Historic District and Quaker Meeting House/10th Street Meeting House: Based on the 800.11 documentation (Appendix D-5 to D-7), the project will not convert these resources to a transportation use:

- Starr Historic District
- Quaker Meeting House/10th Street Meeting House

No permanent or temporary ROW is needed; therefore, this project will not convert property from the Starr Historic District or Quaker Meeting House to a transportation use. Furthermore, INDOT, acting on FHWA's behalf, has determined the appropriate Section 106 finding is "No Adverse Effect". Therefore, in accordance with the Safe, Accountable, Flexible, Efficient Transportation Equity Act: a Legacy for Users (SAFETEA-LU), and the June 8, 2020 MOU regarding Section 4(f) and historic properties between FHWA-IN Division, SHPO, and INDOT (Appendix Q of the current *INDOT CE Manual*), no Section 4(f) evaluation was required (Appendix D-5 to D-7). SHPO concurred with this finding on June 29, 2021 (Appendix D-174 to D-175).

Richmond Railroad Station Historic District: This project will occupy land from the Richmond Railroad Station Historic District, a Section 4(f) historic land. Specifically, approximately 0.086 acre of permanent ROW is needed from the commercial parking lot beneath the bridge on the north side of North E Street, and approximately 0.015 acre of temporary ROW is needed to provide aerial working space above the historic John W. Grubbs building at the southwest corner of US 27 and North E Street (Appendix B-12). The proposed permanent ROW is from a non-contributing area (parking lot), and the proposed temporary ROW was evaluated as a *temporary occupancy*. Details are described further below.

INDOT, acting on FHWA's behalf, has determined the appropriate Section 106 finding is "No Adverse Effect" (Appendix D-5 to D-7). FHWA believes that the *temporary occupancy* of the Richmond Railroad Historic District will not constitute a Section 4(f) use because all of the conditions listed in 23 CFR 774.13(d) are satisfied:

1. Duration must be temporary, i.e., less than the time needed for construction of the project, and here should be no change in ownership of the land;
2. Scope of the work must be minor, i.e., both the nature and the magnitude of the changes to the Section 4(f) property are minimal;
3. There are no anticipated permanent adverse physical impacts, nor will there be interference with the protected activities, features, or attributes of the property, on either a temporary or permanent basis;
4. The land being used must be fully restored, i.e., the property must be returned to a condition which is at least as good as that which existed prior to the project; and
5. There must be documented agreement of the official(s) with jurisdiction over the Section 4(f) resource regarding the above conditions.

The fulfillment of conditions 1–4 are detailed in Section 4 of the 800.11 documentation, "Describe the Undertaking's Effects on Historic Properties." (Appendix D-13 to D-14), which states:

"Temporary right-of-way will be required from 831 N. E Street within the historic district to allow for aerial working room above the building. The temporary right-of-way will not constitute a Section 4(f) use: the duration is temporary, the construction activity within the temporary right-of-way will not involve the building, there will no interference with activity within the building, and the condition of the building before and after construction will not change.

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The undertaking will also require a small amount of permanent right-of-way on either side of the bridge between N. E Street and the NSRR tracks. The parcel is a parking lot that does not contribute to the historic district. The nomination for the Richmond Railroad Station Historic District only specifies noncontributing buildings and not non-contributing areas such as parking lots. Because this area is not specified in the nomination, INDOT requested concurrence that this area does not contribute to the historic district in the effects letter dated March 30, 2021. SHPO's response letter dated April 27, 2021, stated, "Regarding the Richmond Railroad Station Historic District (NR-0781), we agree with the effects letter that the parcel to be acquired north of E Street under the current bridge does not contribute to the historic district." Therefore, this right-of-way acquisition does not constitute a use under Section 4(f)."

With regard to condition 5, on July 19, 2021 the SHPO concurred with this finding, stating "Accordingly, we concur with INDOT's June 29, 2021, Section 106 finding of "No Adverse Effect" for this federal undertaking. We also concur that the impacts to the Richmond Railroad Station Historic District constitutes a *temporary occupancy* for the purposes of Section 4(f)" (Appendix D-174 to D-175). Applicable conditions are included in the Environmental Commitments section.

In conclusion, the proposed permanent ROW from the Richmond Railroad Station Historic District does not constitute a Section 4(f) use because it is from a non-contributing property (parking lot). Additionally, since all of the conditions listed in 23 CFR 774.13(d) are satisfied, the proposed temporary ROW of the John W. Grubbs building is a *temporary occupancy* of the Richmond Railroad Station Historic District. Therefore, no Section 4(f) use is expected.

### North 10th Street Park

The North 10th Street Park is adjacent to the northern portion of the project area (Appendix B-7 and Appendix I-22). Per the FHWA *Section 4(f) Policy Paper*, "Section 4(f) applies when the land is one of the enumerated types of publicly owned lands and the public agency that owns the property has formally designated and determined it to be significant for park, recreation area, or wildlife and waterfowl refuge purposes." This property is identified as a "mini park" in the above-referenced Park Master Plan (Appendix I-18 to I-19). Based on its public ownership and identification as a park, North 10th Street Park is a Section 4(f) resource. An ECL was sent to City of Richmond Parks and Recreation on January 15, 2020 (Appendix C-1 to C-4) and no response was received.

Access to the park will be slightly impacted by this project, as shown on the Annotated Aerial maps (Appendix B-7 to B-8):

- The block of North F Street between North 9th Street and Fort Wayne Avenue will be permanently closed to motorists.
- New sidewalk will be constructed south of the new bridge abutment to connect Fort Wayne Avenue to North F Street and North 9th Street, providing pedestrians continued east-west access through the area.
- The existing pedestrian facilities on US 27 will be upgraded by the removal of the eastern sidewalk and staircase, and replacement of the western sidewalk with a 6.5-foot wide sidewalk that meets current standards, including ADA.
- The western pedestrian staircase will remain.

These changes in access will not adversely affect North 10th Street Park. There is a grid of local streets that will continue to provide motorists with access to the park, and pedestrian access will be improved by upgrading facilities to current standards. All work will occur within existing INDOT ROW. The only park feature beyond open lawn is a stone monument, located approximately 70 feet east of the project area, which will not be disturbed.

The project will not use this resource by taking permanent ROW and will not indirectly use the resource in such a way that the protected activities, features, or attributes that qualify a resource for protection under Section 4(f) are substantially impaired. Therefore, no Section 4(f) use is expected.

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**Section 6(f) Involvement**

Presence

Use

**Section 6(f) Property**

Yes

No

*Discuss Section 6(f) resources present or not present. Discuss if any conversion would occur as a result of this project. If conversion will occur, discuss the conversion approval.*

The US Land and Water Conservation Fund Act of 1965 established the Land and Water Conservation Fund (LWCF), which was created to preserve, develop, and assure accessibility to outdoor recreation resources. Section 6(f) of this Act prohibits conversion of lands purchased with LWCF monies to a non-recreation use.

A review of Section 6(f) properties on the INDOT ESD website revealed a total of three properties in Wayne County (Appendix I-34). None of these properties are located within or adjacent to the project area. Therefore, there will be no impacts to 6(f) resources.

### SECTION F – Air Quality

**STIP/TIP and Conformity Status of the Project**

Is the project in the most current STIP/TIP?

Yes

No

Is the project located in an MPO Area?



Is the project in an air quality non-attainment or maintenance area?



If Yes, then:

Is the project in the most current MPO TIP?



Is the project exempt from conformity?



If No, then:

Is the project in the Transportation Plan (TP)?



Is a hot spot analysis required (CO/PM)?



Location in STIP:

Initial (Appendix H-1)

Name of MPO (if applicable):

\_\_\_\_\_

Location in TIP (if applicable):

\_\_\_\_\_

Level of MSAT Analysis required?

Level 1a  Level 1b  Level 2  Level 3  Level 4  Level 5

*Describe if the project is listed in the STIP and if it is in a TIP. Describe the attainment status of the county(ies) where the project is located. Indicate whether the project is exempt from a conformity determination. If the project is not exempt, include information about the TP and TIP. Describe if a hot spot analysis is required and the MSAT Level.*

This project is included in the Fiscal Year (FY) 2020-2024 Statewide Transportation Improvement Program (STIP) (Appendix H-1).

This project is located in Wayne County, which is currently in attainment for all criteria pollutants according to IDEM ([www.in.gov/idem/airquality/2339.htm](http://www.in.gov/idem/airquality/2339.htm)). Therefore, the conformity procedures of 40 CFR Part 93 do not apply.

This project is of a type qualifying as a categorical exclusion (Group 1) under 23 CFR 771.117(c), or exempt under the Clean Air Act conformity rule under 40 CFR 93.126, and as such, a Mobile Source Air Toxics analysis is not required.

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### SECTION G - NOISE

**Noise**

**Yes      No**

Is a noise analysis required in accordance with FHWA regulations and INDOT's traffic noise policy?

Date Noise Analysis was approved/technically sufficient by INDOT ESD: \_\_\_\_\_

*Describe if the project is a Type I or Type III project. If it is a Type I project, describe the studies completed to date and if noise impacts were identified. If noise impacts were identified, describe if abatement is feasible and reasonable and include a statement of likelihood.*

This project is a Type III project. In accordance with 23 CFR 772 and the current INDOT *Traffic Noise Analysis Procedure*, this action does not require a formal noise analysis.

### SECTION H – COMMUNITY IMPACTS

**Regional, Community & Neighborhood Factors**

**Yes      No**

Will the proposed action comply with the local/regional development patterns for the area?

<b>X</b>	<b>X</b>
----------	----------

Will the proposed action result in substantial impacts to community cohesion?

	<b>X</b>
--	----------

Will the proposed action result in substantial impacts to local tax base or property values?

	<b>X</b>
--	----------

Will construction activities impact community events (festivals, fairs, etc.)?

	<b>X</b>
--	----------

Does the community have an approved transition plan?

<b>X</b>	
----------	--

If No, are steps being made to advance the community's transition plan?

--	--

Does the project comply with the transition plan? (explain in the discussion below)

<b>X</b>	
----------	--

*P[Discuss how the project complies with the area's local/regional development patterns; whether the project will impact community cohesion; and impact community events. Discuss how the project conforms with the ADA Transition Plan.*

The US 27 Bridge Project is consistent with local and regional land use and transportation plans, including the previously-referenced 2005 Bike/Ped Master Plan, the 2006 *Richmond Comprehensive Plan* (<https://www.richmondindiana.gov/resources/comprehensive-plan>), and the 2019 *Richmond Rising: Community Action Plan* (<https://www.richmondindiana.gov/resources/richmond-rising-plan>). Coordination with the City of Richmond has been ongoing during preliminary planning and selection of the preferred alternative, including the closure of North F Street to vehicular traffic, the Loop Project and the alley beneath the bridge, and the recommended Chester Boulevard Multi-Use Trail, discussed further below.

Regarding community events, no comments concerning potential impacts to fair, festivals, etc., have been received. The contractor will be required to coordinate with the City of Richmond and local stakeholders regarding the MOT, which is included in the Environmental Commitments section. Therefore, impacts to community events are not expected.

The City of Richmond has an *ADA Transition Plan* from 2017 (<https://www.richmondindiana.gov/docs/ada-transition-plan-richmond>). This project will improve provide ADA-complaint pedestrian facilities. Therefore, this project will comply with the community's transition plan.

**Closure of North F Street**

The preferred alternative will permanently alter local traffic patterns by closing North F Street to vehicular traffic (Appendix B-7 and B-15). This will reduce the existing section of North F Street from two blocks long (from North 8th Street to Fort Wayne Avenue) to one block long (from North 8th Street to North 9th Street). Sidewalk will be constructed along the new bridge abutment, which will continue to allow pedestrians east-west access through the area, from Fort Wayne Avenue to North F Street / North 9th Street. Traffic counts are not available for this local street segment. Adjoining property owners and nearby residents along North 9th Street were invited to the public information meeting (Appendix G-12 to G-13). No public comments about the proposed street closure were received.

The change in access is minor because there is a grid of local streets that will continue to provide access for motorists north and west of the bridge, and pedestrian access will be improved by facilities that meet current standards.

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### **Loop Project (Des. 1702679) and Alley Beneath Bridge**

The Loop Project is planned for construction in 2022 along the south side of North E Street beneath the US 27 bridge (Appendix B-6). The US 27 Bridge Project is scheduled for construction the following year in 2023. Furthermore, the City of Richmond proposes to close off the alleyway beneath the bridge from North E Street to Elm Place to vehicular traffic. Currently, this alley has bridge piers in the middle of the roadway, and it is open to vehicular traffic (Photo 3, Appendix B-4). The City may utilize the alley beneath the bridge as a public space in the future (e.g., farmers market location or similar). Therefore, the project team for both projects and the City of Richmond have been collaborating on both projects' designs and schedules.

The Loop Project segment within the project area will consist of a 6-foot wide reconstructed concrete sidewalk and an 8-foot wide decorative concrete bike lane off-set from parking and through-lanes by curb and gutter (Appendix I-24). There will be no change in ROW. The Loop Project will install temporary barrier along Elm Place at the southern end of the alley to prohibit vehicles from entering the area. The US 27 Bridge Project will remove the existing pier columns that abut the proposed Loop Project's sidewalk, install new piers in new locations that will not impact the Loop Project, and construct new sidewalk along Elm Place at the southern end of the alley under the US 27 bridge.

During construction, the contractor will be required to maintain access to North E Street, its sidewalk and bike lane (Loop Project), and surrounding businesses during the daytime. Short-term nighttime closures will be restricted to activities needed to safely remove foundations, overhead hazards, and set new steel-beams. This is included in the Environmental Commitments section.

### **Recommended Chester Boulevard Multi-Use Trail**

Richmond's Bike/Ped Master Plan recommends a potential multi-use trail segment along US 27, locally known as Chester Boulevard, through the project area (from Sim Hodgkin Parkway to North E Street) (Appendix I-25 to I-27). Note, the southern terminus would likely need to extend to the end of the structure's southern approach and tie-in at North D Street, under both existing and proposed conditions. One member of the public asked about this trail segment during the public information meeting, requesting access for bicyclists along US 27 (Appendix G-34 to G-36). Based on coordination with the City of Richmond, this trail is not yet in the design phase and is anticipated in the next three to five years (Appendix I-30 to I-32). There were no other public comments or responses to early coordination about this recommended trail.

Construction of the US 27 Bridge project will not alter the City of Richmond's ability to develop a future multi-use trail along this section of US 27 compared to current conditions. Furthermore, as previously discussed, this project will enhance pedestrian connectivity by providing a new 6.5-foot wide sidewalk that meets current standards, including ADA. Additionally, Richmond's City Code does not appear to prohibit the use of sidewalks by bicyclists (<https://www.richmondindiana.gov/resources/city-code>). Therefore, no impacts are expected.

## **Public Facilities and Services**

*Discuss what public facilities and services are present in the project area and impacts (such as MOT) that will occur to them. Include how the impacts have been minimized and what coordination has occurred. Some examples of public facilities and services include health facilities, educational facilities, public and private utilities, emergency services, religious institutions, airports, transportation or public pedestrian and bicycle facilities.*

Based on a desktop review, the aerial map of the project area (Appendix B-2), and the RFI report (Appendix E), there are ten religious facilities, one hospital, and five schools located within 0.5 mile of the project. None of these facilities are adjacent to the project area. Those numbers were confirmed by the site visit on March 12, 2020 by Parsons. There will be no change in access to these properties, and access to all properties will be maintained during construction. Therefore, no impacts are expected

There are multiple subgrade utilities beneath North F Street where the new bridge abutment is proposed. Therefore, this project will relocate subgrade water, storm sewer, sanitary sewer, communication, and natural gas lines in that area. The project team, in conjunction with the INDOT Utilities and Rail Office, have ongoing utility engineering and coordination (Appendix I-35). There will be no disruption in services. Therefore, no impacts are expected.

Impacts to pedestrian and bicycle facilities were previously discussed in the Project Description and Regional, Community, and Neighborhood Factors sections of this CE document.

The proposed project should not impact property values or the local tax base. There should be no impacts to community cohesion, and the proposed construction should not impact community events. Therefore, no significant economic or community impacts are expected to develop as a result of the project.

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**Environmental Justice (EJ)** (Presidential EO 12898)

During the development of the project were EJ issues identified?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Does the project require an EJ analysis?

If YES, then:

Are any EJ populations located within the project area?

<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>

Will the project result in adversely high and disproportionate impacts to EJ populations?

*Indicate if EJ issues were identified during project development. If an EJ analysis was not required, discuss why. If an EJ analysis was required, describe how the EJ population was identified. Include if the project has a disproportionately high and adverse effect on EJ populations and explain your reasoning. If yes, describe actions to avoid, minimize and mitigate these effects.*

Under FHWA Order 6640.23A, FHWA and the project sponsor, as a recipient of funding from FHWA, are responsible to ensure that their programs, policies, and activities do not have a disproportionately high and adverse effect on minority or low-income populations. This project will have no relocations and will require less than 0.5 acre of additional permanent ROW; therefore, an EJ analysis is not required per the current INDOT *Categorical Exclusion Manual*.

**Relocation of People, Businesses or Farms**

Will the proposed action result in the relocation of people, businesses or farms?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Is a BIS or CSRS required?

Number of relocations: Residences: 0 Businesses: 0 Farms: 0 Other: 0

*Discuss any relocations that will occur due to the project. If a BIS or CSRS is required, discuss the results in the discussion below.*

No relocations of people, businesses, or farms will take place as a result of this project.

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## SECTION I – HAZARDOUS MATERIALS & REGULATED SUBSTANCES

**Hazardous Materials & Regulated Substances** (Mark all that apply)

- Red Flag Investigation (RFI)
- Phase I Environmental Site Assessment (Phase I ESA)
- Phase II Environmental Site Assessment (Phase II ESA)
- Design/Specifications for Remediation required?

Documentation

X
X

Date RFI concurrence by INDOT SAM (if applicable): June 21, 2019 (Appendix E-7)

*Include a summary of the potential hazardous material concerns found during review. Discuss in depth sites found within, directly adjacent to, or ones that could impact the project area. Refer to current INDOT SAM guidance. If additional documentation (special provisions, pay quantities, etc.) will be needed, include in discussion. Include applicable commitments.*

Based on a review of GIS and available public records, an RFI was approved by INDOT Site Assessment & Management on June 21, 2019 (Appendix E-1 to E-13). Eight Resource Conservation and Recovery Act (RCRA) generator sites are located within 0.5 mile of the project area. Eighteen underground storage tank (UST) sites are located within 0.5 mile of the project area. Eleven leaking UST (LUST) sites are located within 0.5 mile of the project area. One waste transfer station (TS) is located within 0.5 mile of the project area. Six brownfield sites are located within 0.5 mile of the project area. Nine institutional controls (IC) sites are located within 0.5 mile of the project area. Additionally, there are eight National Pollutant Discharge Elimination System (NPDES) facilities and one NPDES pipe location within 0.5 mile of the project area. The RFI identified three hazmat sites that could affect the project area: Rumpke of Indiana, Mechanics Laundry, and Pennsylvania Railroad Depot.

Rumpke of Indiana at 275 North Fort Wayne Avenue, Agency Interest (AID) 13819, is a TS that was a railroad switchyard and maintenance property. Current operations are limited to “truck parking and light engine servicing inside an enclosed building”. However, this property has been in continuous use since 1853. It was a rail switchyard and the headquarters of the Pennsylvania Railroad. Historically, the rail yard was located within the project area (i.e., within current INDOT ROW). The facility has a 12,000-gallon diesel fuel UST that was installed in 1988 and remained in use as recently as 2015. A former on-site manager speculated that motor oil was sprayed along the former tracks as herbicide. Subsurface investigations reported the presence of “industrial byproducts including slag, coal, coal fines, cinders and ash...”. Its status as a TS facility remains active. The existing environmental restrictive covenant (ERC) and State Cleanup Activities were limited and only related to two surface releases of petroleum products. This facility is also associated with the Pennsylvania Railroad Depot site, AID 56240, discussed below. A Phase II Environmental Site Assessment (PhII ESA) was recommended.

Mechanics Laundry at 1002 North E Street, AID 57281, is a dry cleaner located 0.08 mile east of the project area. The site had a registered UST for kerosene. A release of chlorinated solvents that extends off-site is under investigation. The extent of contamination is not fully defined, and the groundwater flow is west-northwest towards the project bridge. A PhII ESA was recommended.

The Pennsylvania Railroad Depot at 930 North E Street, AID 56240, is mapped 0.02 mile east of the project area. This property was investigated due to adjacent industrial, railroad, and dry cleaning land uses. The 1937 Sanborn map shows a filling station with gasoline tanks within existing ROW beneath the project bridge. Chlorinated solvents and polyaromatic hydrocarbons (PAHs) were encountered in soil and groundwater samples collected in 1999. Samples were also analyzed for polychlorinated biphenyls (PCBs). The results for PCBs were below detection limits. A PhII ESA was recommended.

In May and August 2020, Terracon and Parsons performed a PhII ESA consisting of geophysical and subsurface investigations with soil and groundwater sampling and analyses. The work was conducted in accordance with the “Memorandum for a Phase II Environmental Site Assessment Scope of Work”, approved by INDOT Site Assessment & Management on April 29, 2020. The results were summarized in a PhII ESA report, approved by INDOT Site Assessment & Management on March 15, 2021 (Appendix E-14 to E-47), which included the following conclusions and recommendations:

The results of the geophysical investigation identified “high target areas” that are likely fill mixed with construction debris. No evidence of an in-place UST system was noted. These results are further supported by the sandy/loamy fill materials encountered in soil borings advanced at the project area, which contained typical construction debris fragments that were mostly brick and concrete. A *de minimis* amount (<0.002 cubic foot) of transite-like suspect asbestos containing material was encountered in boring TB-8 at around 4 feet below ground surface (ft-bgs).



## Indiana Department of Transportation

County Wayne

Route US 27

Des. No. 1702941

In general, soil samples submitted for laboratory analyses contained low levels of adsorbed chemicals of concern (CoCs) that are indicative of past uses of the site and surrounding area which included rail, service stations, a junk yard, and a dry cleaner. None of the CoCs in soil were detected at concentrations that exceed migration to groundwater screening levels (MTG SLs), except for chromium VI (aka hexavalent chromium). The laboratory results for chromium VI ranged from <0.201 to 0.481 milligrams per kilogram (mg/kg), which exceed the MTG SL of 0.14 mg/kg.

- A copy of this PhII ESA report, including laboratory analyses, should be provided to the contractor.
- Soil generated from construction of Pier 3 to Pier 8, as well as the retaining wall, should not be handled as "clean fill". This soil should be disposed of at a municipal waste landfill as solid waste.
- The contractor should be prepared to deal with the potential for asbestos containing materials to be present in fill wastes generated during construction of the new piers, especially Pier 6, in accordance with INDOT *Standard Specifications*.
- Soil analytical results were below hazardous waste levels (RCRA), as well as IDEM construction worker screening levels. The contractor will ensure that appropriate personal protective equipment (PPE) is used and that all work is completed in compliance with Occupational Safety and Health Administration (OSHA) regulations.

Similarly, groundwater samples contained relatively low levels of CoCs that are indicative of the historical urban use of the project area. This included a dissolved concentration of lead of 41.7 micrograms per liter (ug/L) in field-filtered sample B-7-GW (near Pier 4), which is above the SL of 15 ug/L, but below the toxic characteristic leachate procedure (TCLP) limit of 5,000 ug/L. Additionally, unfiltered samples contained total metals above SLs. Therefore, even though the groundwater is non-hazardous, proper handling, removal and disposal is needed.

The recommendations from the PhII ESA are included in the Environmental Commitments section of this CE document.

**Indiana Department of Transportation**

County Wayne

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**Part IV – Permits and Commitments**

<b>PERMITS CHECKLIST</b>
--------------------------

**Permits** (mark all that apply)

**Likely Required**

**Army Corps of Engineers (404/Section10 Permit)**

- Nationwide Permit (NWP)
- Regional General Permit (RGP)
- Individual Permit (IP)
- Other

**IN Department of Environmental Management (401/Rule 5)**

- Nationwide Permit (NWP)
- Regional General Permit (RGP)
- Individual Permit (IP)
- Isolated Wetlands
- Rule 5
- Other

**IN Department of Natural Resources**

- Construction in a Floodway
- Navigable Waterway Permit
- Other

**Mitigation Required**

**US Coast Guard Section 9 Bridge Permit**

**Others (Please discuss in the discussion below)**

*List the permits likely required for the project and summarize why the permits are needed, including permits designated as "Other."*

More than one acre of land will be disturbed; therefore, an IDEM Rule 5 permit is anticipated. IDEM's electronic coordination discussed this permit requirement (Appendix C-12 to C-19).

Applicable recommendations provided by resource agencies are included in the Environmental Commitments section of this document. If permits are found to be necessary, the conditions of the permit will be requirements of the project and will supersede these recommendations.

It is the responsibility of the project sponsor to identify and obtain all required permits.

## Indiana Department of Transportation

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### ENVIRONMENTAL COMMITMENTS

List all commitments and include the name of agency/organization requesting/requiring the commitment(s). Listed commitments should be numbered.

**Firm:**

- 1) If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately. (INDOT ESD and INDOT District)
- 2) It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access. (INDOT ESD)
- 3) General AMM 1: Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs. (USFWS)
- 4) Lighting AMM 1: Direct temporary lighting away from suitable habitat during the active season. (USFWS)
- 5) During construction, the contractor will be required to maintain access to North E Street, its sidewalk and bike lane (Loop Project), and surrounding businesses during the daytime. Likewise, Elm Place and Fort Wayne Avenue will remain open to through-traffic and pedestrians. Short-term nighttime closures will be restricted to activities needed to safely remove foundations, overhead hazards, and set new steel-beams. (INDOT ESD).
- 6) The contractor will coordinate the MOT with local stakeholders including the City of Richmond, school corporations, and emergency services. (INDOT ESD)
- 7) A copy of the PhII ESA report, including laboratory analyses, will be provided to the contractor. Soil generated from construction of Pier 3 to Pier 8, as well as the retaining wall, shall not be handled as "clean fill". This soil will be disposed of at a municipal waste landfill as solid waste. The contractor should be prepared to deal with the potential for asbestos containing materials to be present in fill wastes generated during construction of the new piers, especially Pier 6, in accordance with INDOT Standard Specifications. Soil analytical results were below hazardous waste levels (RCRA), as well as IDEM construction worker screening levels. The contractor will ensure that appropriate personal protective equipment (PPE) is used and that all work is completed in compliance with Occupational Safety and Health Administration (OSHA) regulations. (INDOT ESD)
- 8) Groundwater samples contained relatively low levels of contaminants of concern (CoCs) that are indicative of the historical urban use of the project area. This included a dissolved concentration of lead of 41.7 micrograms per liter (ug/L) in field-filtered sample B-7-GW (near Pier 4), which is above the screening level of 15 ug/L, but below the toxic characteristic leachate procedure (TCLP) limit of 5,000 ug/L. Additionally, unfiltered samples contained total metals above SLs. Therefore, even though the groundwater is non-hazardous, proper handling, removal and disposal is required. (INDOT ESD)
- 9) The abutting John W. Grubbs Building at 831 North E Street will be protected from damage during construction, and a protective fence will be installed as part of the preferred alternative. Contract documents will include special provisions to limit vibrations and related concerns, which will also be discussed at any pre-bid and pre-construction meetings. (INDOT ESD and INDOT-CRO)
- 10) Regarding proposed temporary ROW in the Richmond Railroad Historic District (John W. Grubbs Building, 831 North E Street): the duration of occupancy must be temporary, i.e., less than the time needed for construction of the project, and there should be no change in ownership of the land. (INDOT-CRO)
- 11) Regarding proposed temporary ROW in the Richmond Railroad Historic District (John W. Grubbs Building, 831 North E Street): the land being used must be fully restored, i.e., the property must be returned to a condition which is at least as good as that which existed prior to the project. (INDOT-CRO)
- 12) A copy of the approved Cemetery Development Plan (CDP) will be provided as part of the contract documents, and a copy will be provided to Vectren. This plan is not transferable. (INDOT-CRO / IDNR-DHPA)
- 13) Except for the elements of the proposed project detailed in the CDP, the cemetery must be avoided by all ground-disturbing project-related activities (e.g., demolition, construction, grading, dredging, and/or filling, tree clearance, vehicle or equipment staging, materials stockpiling, temporary land use, etc.), and provisions of relevant state statutes regarding cemeteries (including IC 14-21-1 and IC 23- 14) must be adhered to. (INDOT-CRO / IDNR-DHPA)
- 14) Any proposed changes or modifications to the CDP shall be submitted to the IDNR-DHPA in writing for review and comment prior to implementation. (INDOT-CRO / IDNR-DHPA)

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# Appendix A

## INDOT Supporting Documentation

## Categorical Exclusion Level Thresholds

	PCE	Level 1	Level 2	Level 3	Level 4 <sup>1</sup>
<b>Section 106</b>	Falls within guidelines of Minor Projects PA	“No Historic Properties Affected”	“No Adverse Effect”	-	“Adverse Effect” Or Historic Bridge involvement <sup>2</sup>
<b>Stream Impacts<sup>3</sup></b>	No construction in waterways or water bodies	< 300 linear feet of stream impacts	≥ 300 linear feet of stream impacts	-	USACE Individual 404 Permit <sup>4</sup>
<b>Wetland Impacts<sup>3</sup></b>	No adverse impacts to wetlands	< 0.1 acre	-	< 1.0 acre	≥ 1.0 acre
<b>Right-of-way<sup>5</sup></b>	Property acquisition for preservation only or none	< 0.5 acre	≥ 0.5 acre	-	-
<b>Relocations</b>	None	-	-	< 5	≥ 5
<b>Threatened/Endangered Species (Species Specific Programmatic for Indiana bat &amp; northern long eared bat)*</b>	“No Effect”, “Not likely to Adversely Affect” (With select AMMs <sup>6</sup> )	“Not likely to Adversely Affect” (With any AMMs or commitments)	-	“Likely to Adversely Affect”	Project does not fall under Species Specific Programmatic <sup>7</sup>
<b>Threatened/Endangered Species (Any other species)*</b>	Falls within guidelines of USFWS 2013 Interim Policy or “No Effect”	“Not likely to Adversely Affect”	-	-	“Likely to Adversely Affect”
<b>Environmental Justice</b>	No disproportionately high and adverse impacts	-	-	-	Potential <sup>8</sup>
<b>Sole Source Aquifer</b>	No Detailed Groundwater Assessment	-	-	-	Detailed Groundwater Assessment
<b>Floodplain</b>	No Substantial Impacts	-	-	-	Substantial Impacts
<b>Section 4(f) Impacts</b>	None	-	-	-	Any <sup>9</sup>
<b>Section 6(f) Impacts</b>	None	-	-	-	Any
<b>Permanent Traffic Alteration</b>	None	-	-	-	Any
<b>Noise Analysis Required</b>	No	-	-	-	Yes
<b>Air Quality Analysis Required</b>	No	-	-	-	Yes <sup>10</sup>
<b>Approval Level</b>	Concurrence by DE or ESD	DE or ESD	DE or ESD	DE and/or ESD	DE and/or ESD; and FHWA
<ul style="list-style-type: none"> <li>• District Env. (DE)</li> <li>• Env. Serv. Div. (ESD)</li> <li>• FHWA</li> </ul>					

<sup>1</sup> Coordinate with INDOT Environmental Services Division. INDOT will then coordinate with the appropriate FHWA Environmental Specialist.

<sup>2</sup> Any involvement with a bridge processed under the Historic Bridge Programmatic Agreement.

<sup>3</sup> Total permanent impacts to streams (linear feet) and wetlands (acres).

<sup>4</sup> US Army Corps of Engineers Individual 404 Permit

<sup>5</sup> Total permanent and temporary right-of-way. This does not include reacquisition of existing apparent right-of-way.

<sup>6</sup> Avoidance and Mitigation Measures (AMMs) determined by the IPAC determination key to be required that are not tree AMMs, bridge AMMs, or structure AMMs.

<sup>7</sup> Projects that do not fall under a Species Specific Programmatic and results in a “Likely to Adversely Affect”. Other findings can be processed as a lower level CE.

<sup>8</sup> Potential for causing a disproportionately high and adverse impact.

<sup>9</sup> Section 4(f) use resulting in an Individual, Programmatic, or *de minimis* evaluation. The only exception is a *de minimis* evaluation for historic properties (Effective January 2, 2020). If a historic property *de minimis* and no other use, mark the *None* column.

<sup>10</sup> Hot Spot Analysis and/or MSAT Quantitative Emission Analysis.

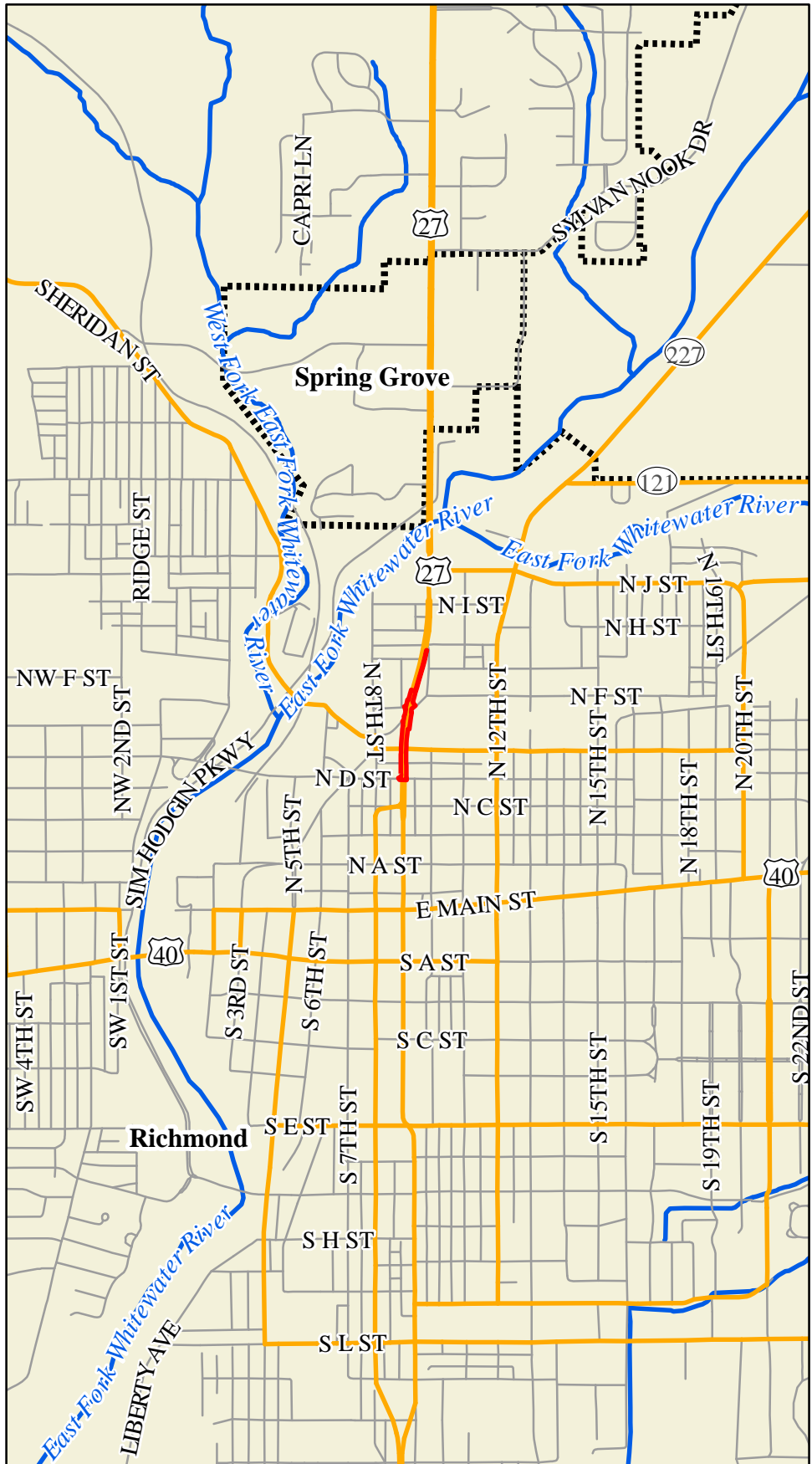
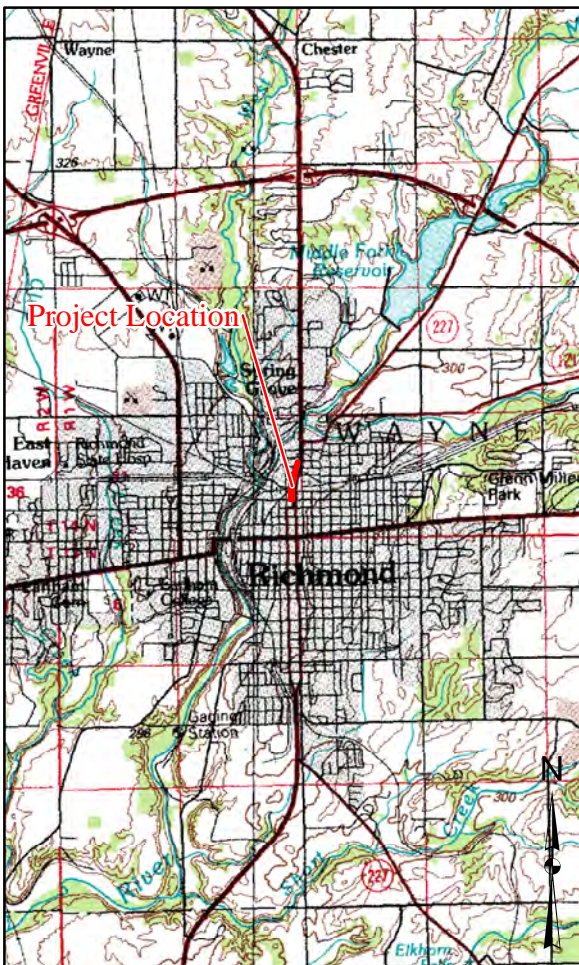
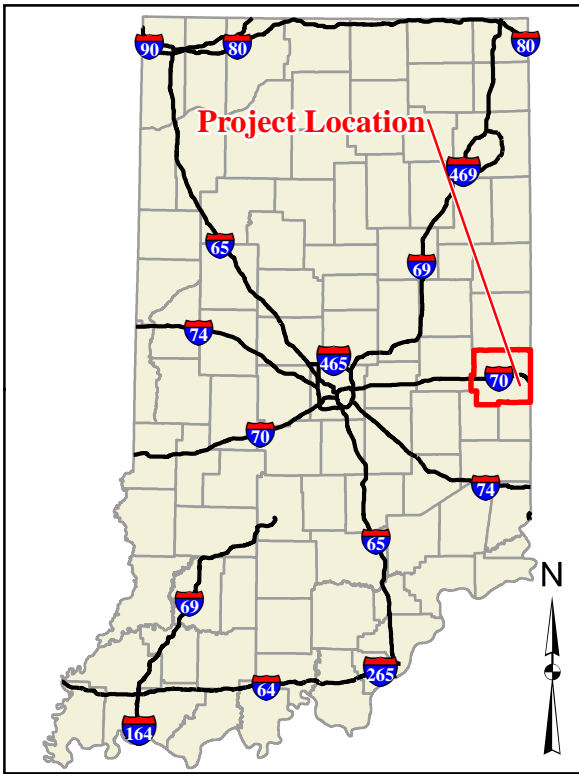
\* Includes the threatened/endangered species critical habitat

Note: Substantial public or agency controversy may require a higher-level NEPA document.

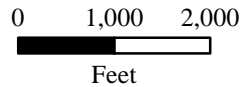
# Appendix B

## Graphics





- Study Area
- ~ Interstate
- ~ Streams and Rivers
- ~ State Routes
- Incorporated Areas
- ~ Local Roads



Sources:  
 Non Orthophotography Data -  
 Obtained from the State of Indiana  
 Geographical Information Office Library  
 Orthophotography -  
 Obtained from Indiana Map  
 Framework Data ([www.indianamap.org](http://www.indianamap.org))

**US 27 Bridge over NSRR and  
 Local Streets  
 Wayne County, Indiana  
 Project Location**

Des. 1702941

Date: 12/1/2020

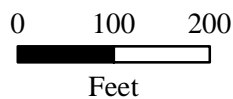


**PARSONS**





 Project Area



Sources:  
 Non Orthophotography Data -  
 Obtained from the State of Indiana Geographical  
 Information Office Library  
 Orthophotography -  
 Obtained from Indiana Map  
 Framework Data ([www.indianamap.org](http://www.indianamap.org))

**US 27 Bridge over NSRR and  
 Local Streets  
 Wayne County, Indiana  
 2017 Aerial Imagery**

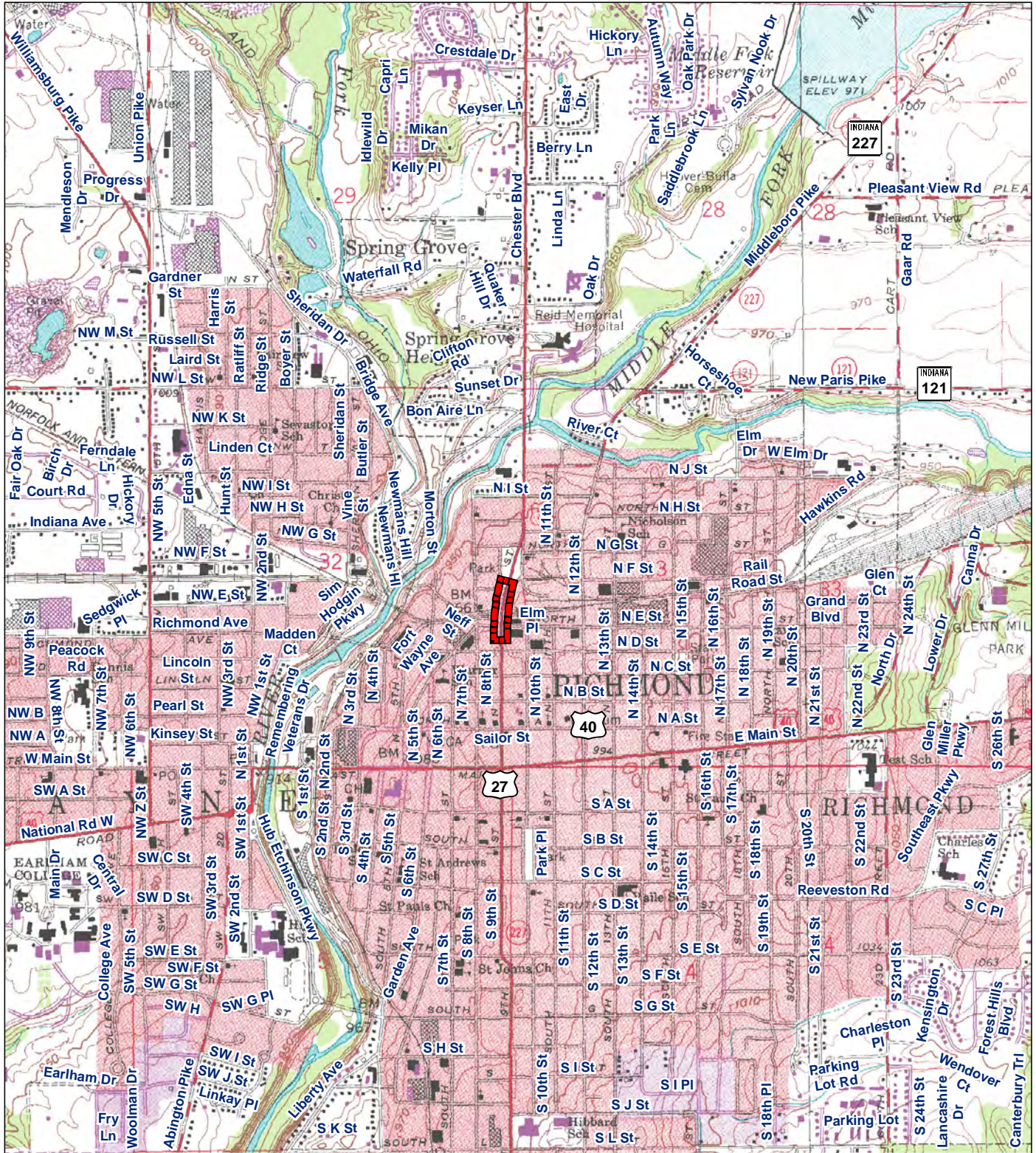
Des. 1702941

Date: 12/1/2020



**PARSONS**





Project boundary



**US 27 Bridge  
over NSRR and Local Streets  
Wayne County, IN  
Des. 1702941  
USGS Topographical Map**

Base: USGS New Paris and Richmond, Indiana,  
7.5' series quadrangles





Photo 1—View of the south end of the US 27 Bridge, Structure No. 027-89-02136 B, facing northwest along N. D Street (09/02/2018).



Photo 2—View of the US 27 Bridge over N. E Street and NSRR, facing northwest (09/02/2018).



Photo 3—View beneath the US 27 Bridge at the alley that connects Elm Place and N. E Street, facing south (09/02/2018).



Photo 4—View of the US 27 Bridge over the NSRR, facing northeast (09/02/2018).





Photo 5— View of the north abutment and a staircase for the US 27 Bridge facing northeast (09/02/2018).



Photo 6—View of the US 27 Bridge over formerly leased property and N. F Street facing southeast. This segment of N. F Street is proposed for closure as part of this project (09/02/2018).

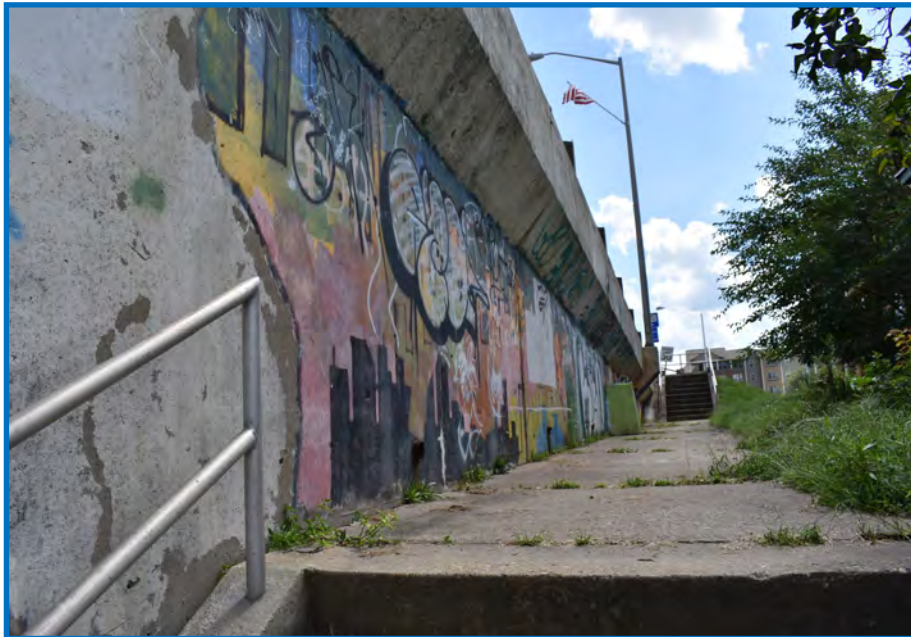
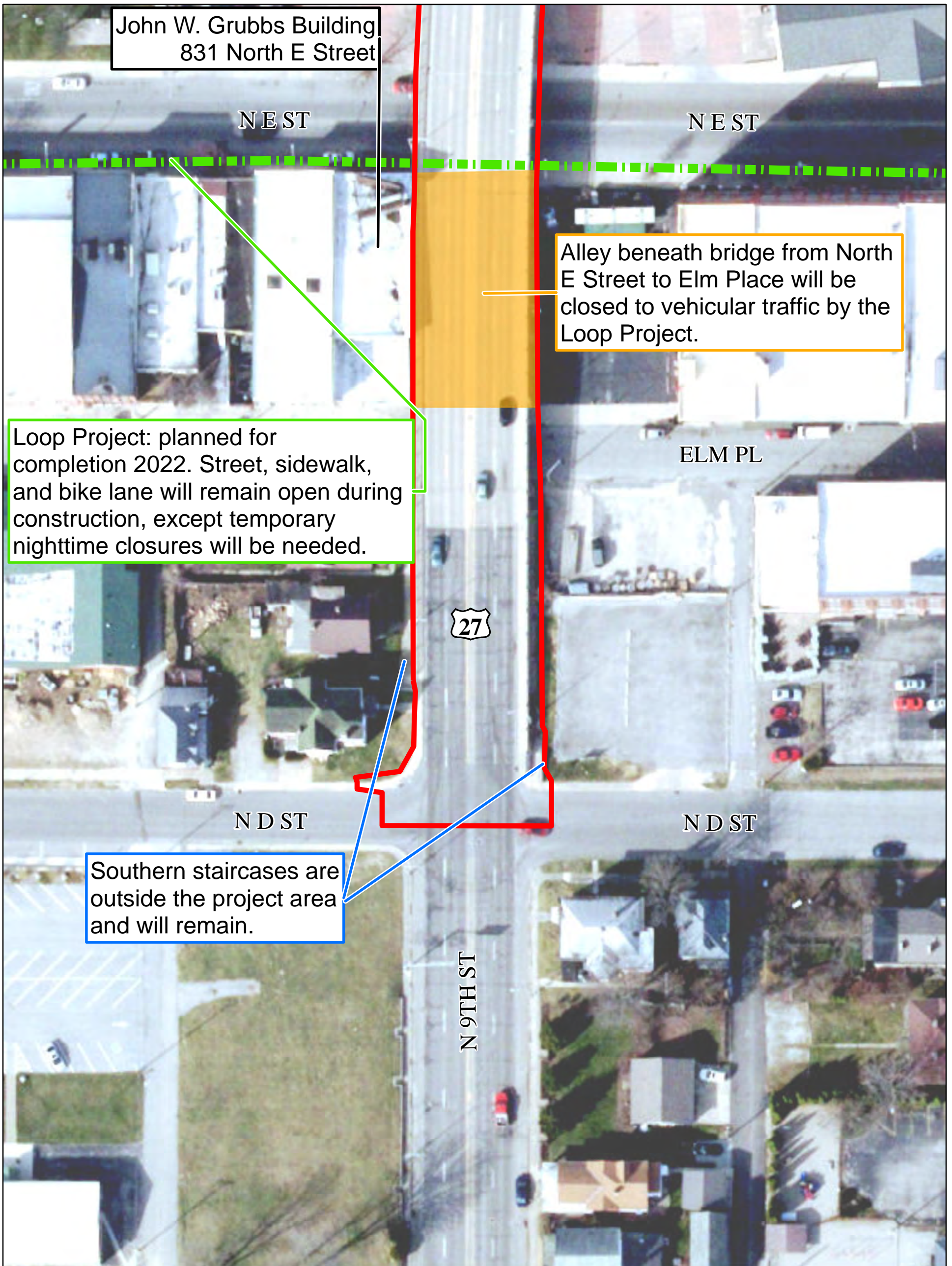


Photo 7 —View along southwestern side of bridge and existing pedestrian facilities facing south (09/02/2018).



Photo 8 —View along southeastern side of bridge and existing pedestrian facilities facing south (09/02/2018).





John W. Grubbs Building  
831 North E Street

NE ST

NE ST

Alley beneath bridge from North E Street to Elm Place will be closed to vehicular traffic by the Loop Project.

Loop Project: planned for completion 2022. Street, sidewalk, and bike lane will remain open during construction, except temporary nighttime closures will be needed.

ELM PL

27

ND ST

ND ST


Southern staircases are outside the project area and will remain.

N 9TH ST

-  Project Area
-  Alley Vehicular Closure
-  Street Vehicular Closure
-  New Sidewalk
-  Loop Project

Des. 1702941  
Date: 5/3/21

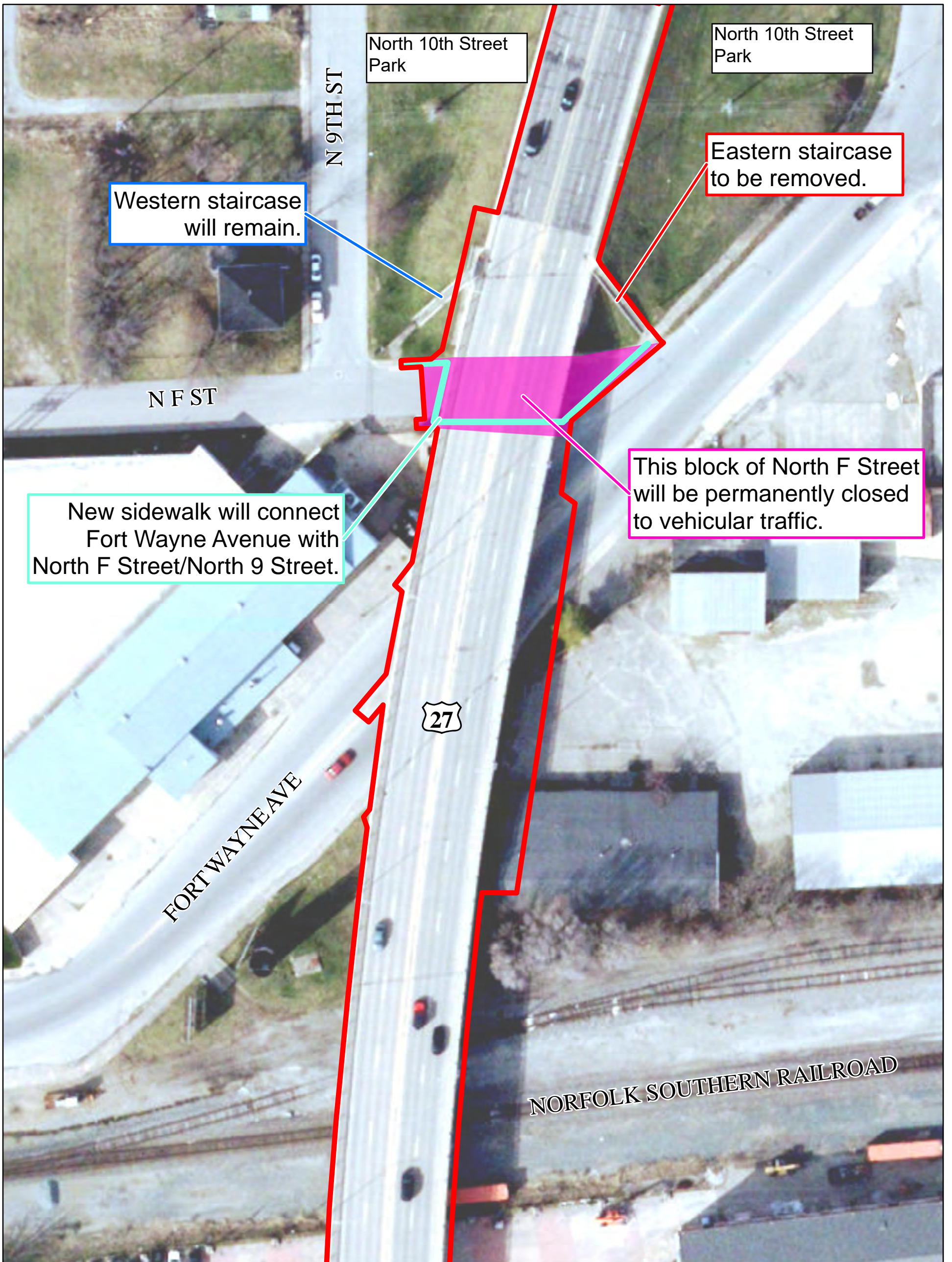
**US 27 Bridge over NSRR  
and Local Streets  
Wayne County, Indiana  
Annotated Aerial Imagery**








  
0 25 50 Feet  
1 Inch = 50 Feet

Sources:  
**Non Orthophotography Data** -  
Obtained from the State of  
Indiana Geographical  
Information Office Library  
**Orthophotography** -  
Obtained from Indiana  
Map Framework Data  
([www.indianamap.org](http://www.indianamap.org))

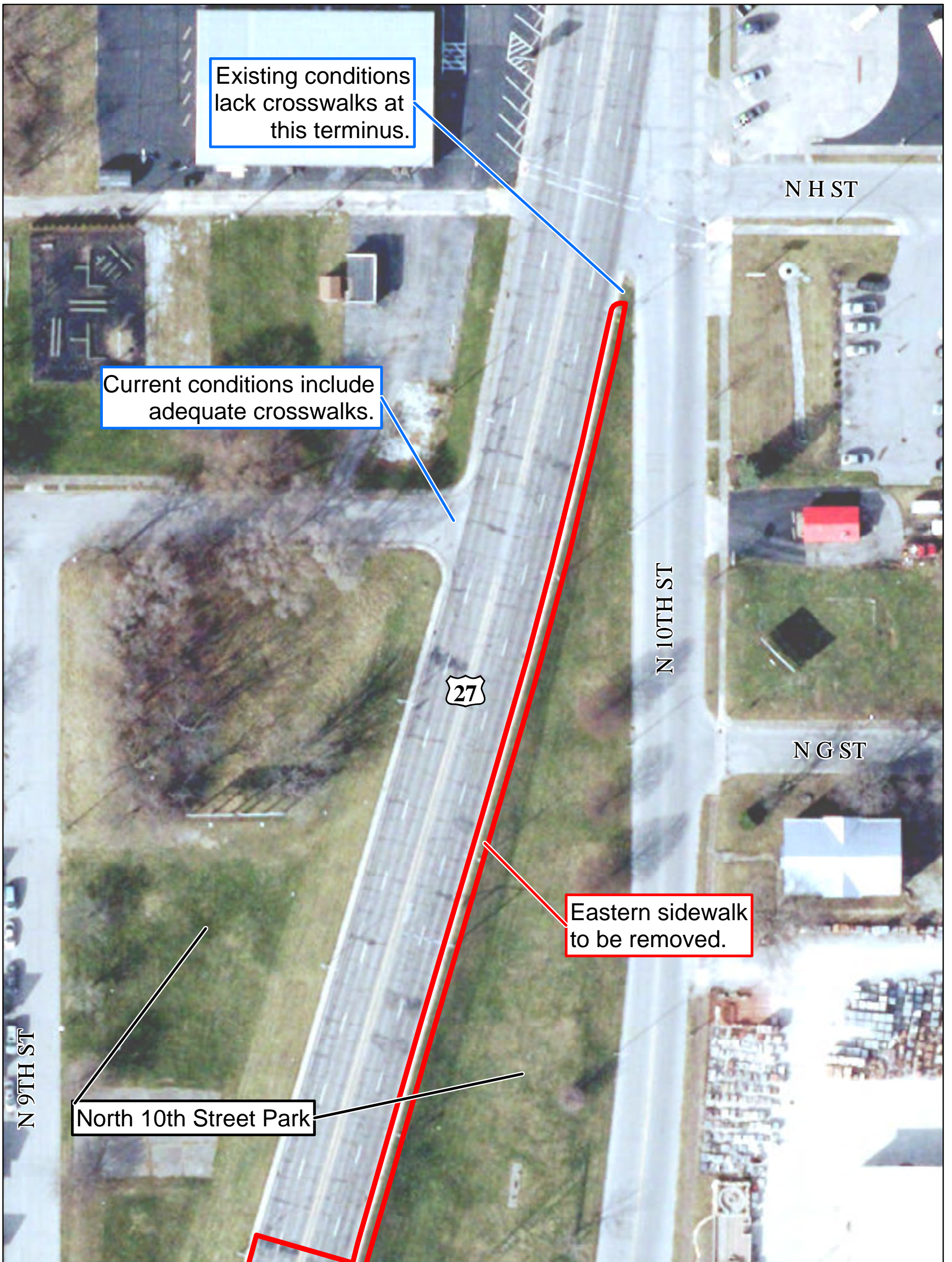












<ul style="list-style-type: none"> <li> Project Area</li> <li> Alley Vehicular Closure</li> <li> Street Vehicular Closure</li> <li> New Sidewalk</li> <li> Loop Project</li> </ul>	<p>Des. 1702941 Date: 5/3/21</p>	<p><b>US 27 Bridge over NSRR and Local Streets</b> <b>Wayne County, Indiana</b> <b>Annotated Aerial Imagery</b></p>
	<p> 0 25 50 Feet 1 Inch = 50 Feet</p>	<p>Sources: <b>Non Orthophotography Data</b> - Obtained from the State of Indiana Geographical Information Office Library <b>Orthophotography</b> - Obtained from Indiana Map Framework Data (<a href="http://www.indianamap.org">www.indianamap.org</a>)</p> 





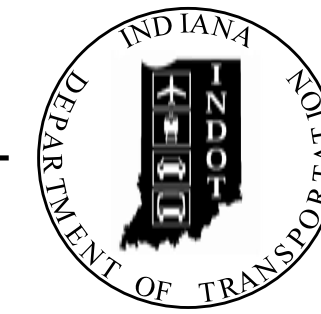
<ul style="list-style-type: none"> <li> Project Area</li> <li> Alley Vehicular Closure</li> <li> Street Vehicular Closure</li> <li> New Sidewalk</li> <li> Loop Project</li> </ul>	<p>Des. 1702941 Date: 5/3/21</p>	<p><b>US 27 Bridge over NSRR and Local Streets</b> <b>Wayne County, Indiana</b> <b>Annotated Aerial Imagery</b></p>
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PROJECT 1702941	DESIGNATION 1702941
CONTRACT B-41510	BRIDGE FILE 027-89-02814

STRUCTURE INFORMATION				
STRUCTURE	TYPE	SPAN AND SKEW	OVER	STATION
027-89-02814	CONTINUOUS COMPOSITE CURVED STEEL PLATE GIRDER BRIDGE	7 Spans: 65'-10", 75'-0", 89'-0", 69'-0", 112'-0", 122'-6" & 193'-6" Skew: Varies 0° Min. To 9° Max.	NORFOLK SOUTHERN RAILROAD AND 5 STREETS	18+74.87 "PR-C-3"

# INDIANA DEPARTMENT OF TRANSPORTATION



## BRIDGE PLANS

FOR SPANS OVER 20 FEET

ROUTE: US 27 AT: RP 22+0.33

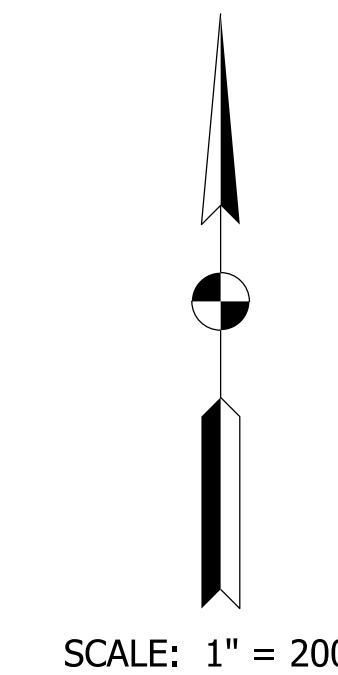
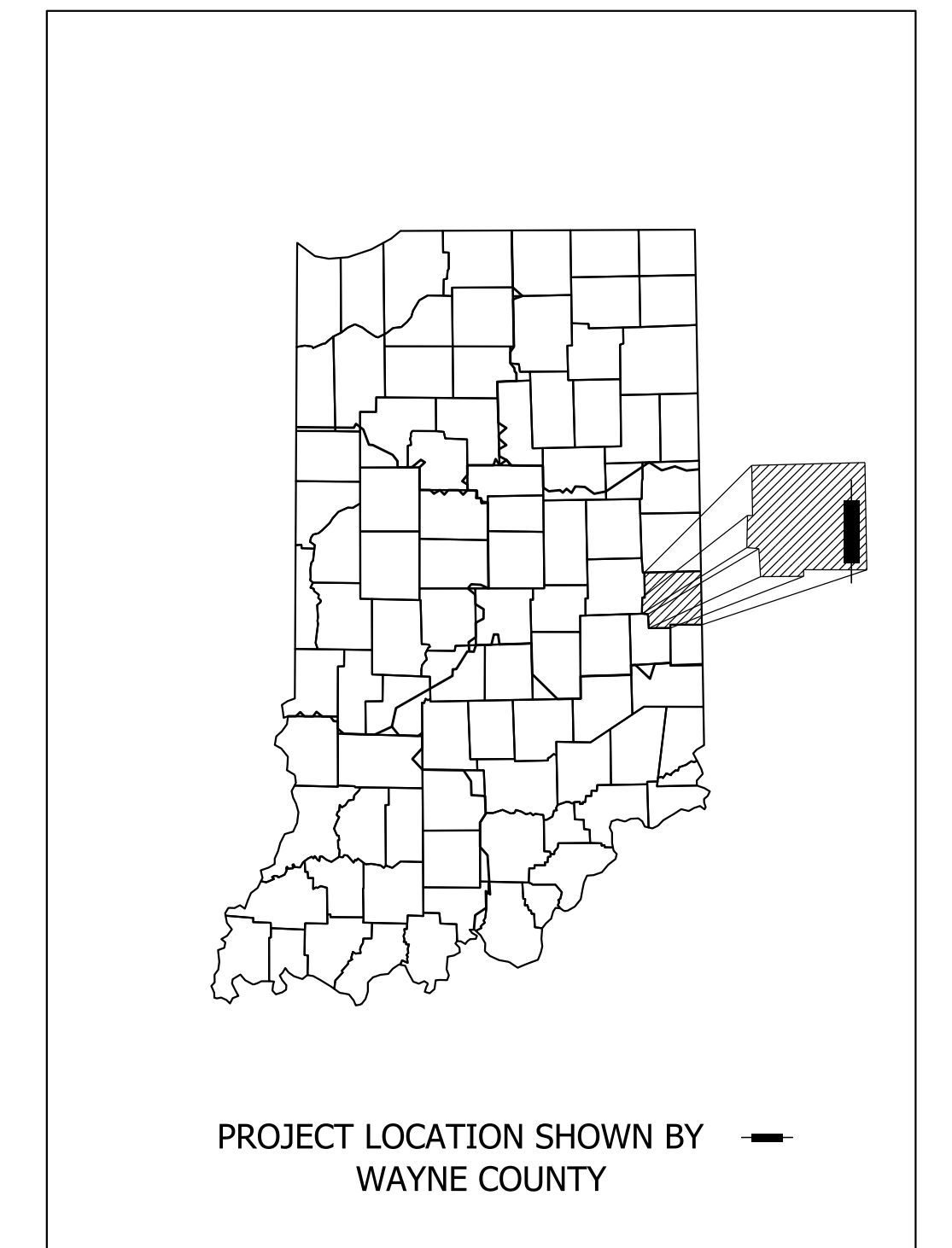
PROJECT NO. 1702941 P.E.  
1702941 R/W  
1702941 CONST.

Bridge Replacement on US 27 over Norfolk Southern Railroad & 5 Streets  
Located 0.3 Miles North of WB US 40  
Section 32, T-14-N, R-1-W, Wayne Township, Wayne County, Indiana

Excerpts

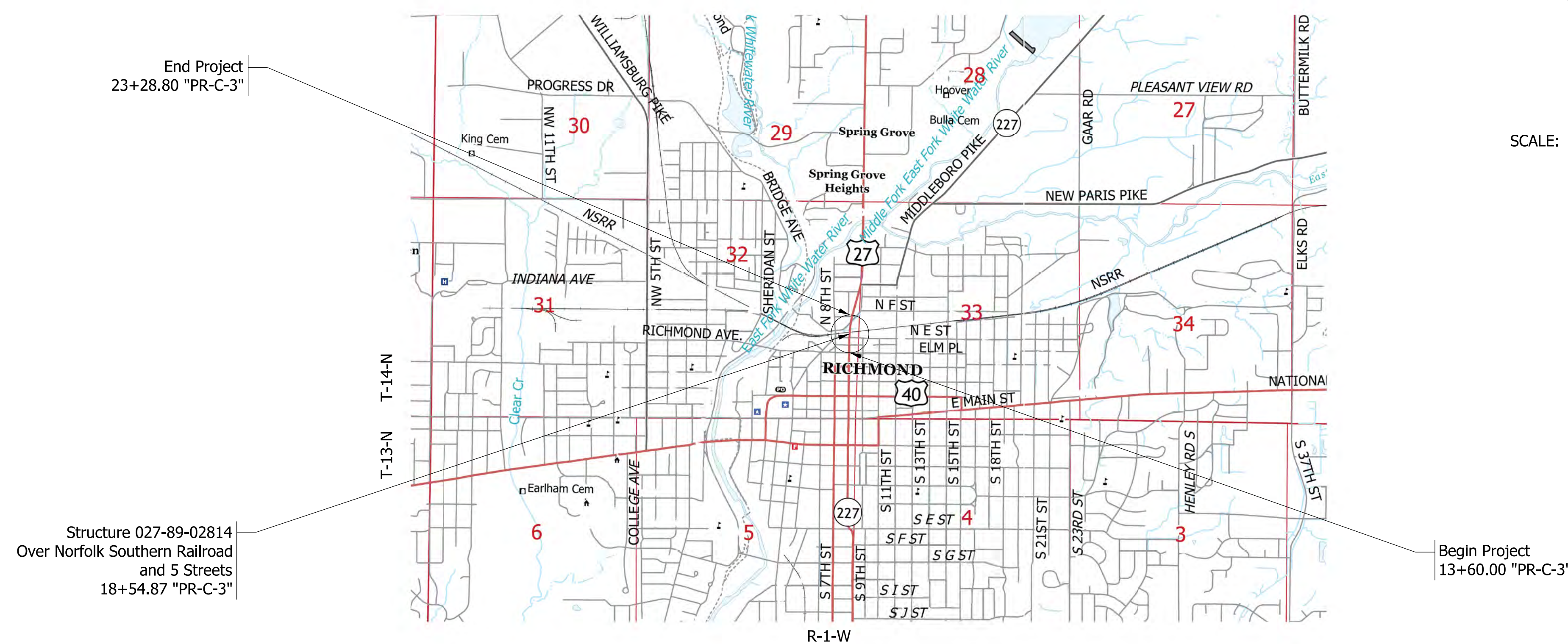
TRAFFIC DATA		
A.A.D.T. (2022)		14,077 V.P.D.
A.A.D.T. (2042)		15,773 V.P.D.
D.H.V (2042)		1,486 V.P.H.
DIRECTIONAL DISTRIBUTION		50.49%
TRUCKS		10 % A.A.D.T. 5 % D.H.V.

DESIGN DATA	
DESIGN SPEED	35 M.P.H.
PROJECT DESIGN CRITERIA	4R RECONSTRUCTION
FUNCTIONAL CLASSIFICATION	URBAN (BUILT UP) ARTERIAL
RURAL/URBAN	URBAN
TERRAIN	LEVEL
ACCESS CONTROL	FULL



LATITUDE: 39° 50' 15" N LONGITUDE: 84° 15' 26" W

BRIDGE LENGTH:	0.14 MI.
ROADWAY LENGTH:	0.04 MI.
TOTAL LENGTH:	0.18 MI.
MAX. GRADE:	4.5 %



INDIANA DEPARTMENT OF TRANSPORTATION  
STANDARD SPECIFICATIONS DATED 2020  
TO BE USED WITH THESE PLANS.

**PARSONS**

101 W. Ohio St., Suite 2121  
Indianapolis, IN 46204  
Bus (317) 616-1000  
Fax (317) 616-1033

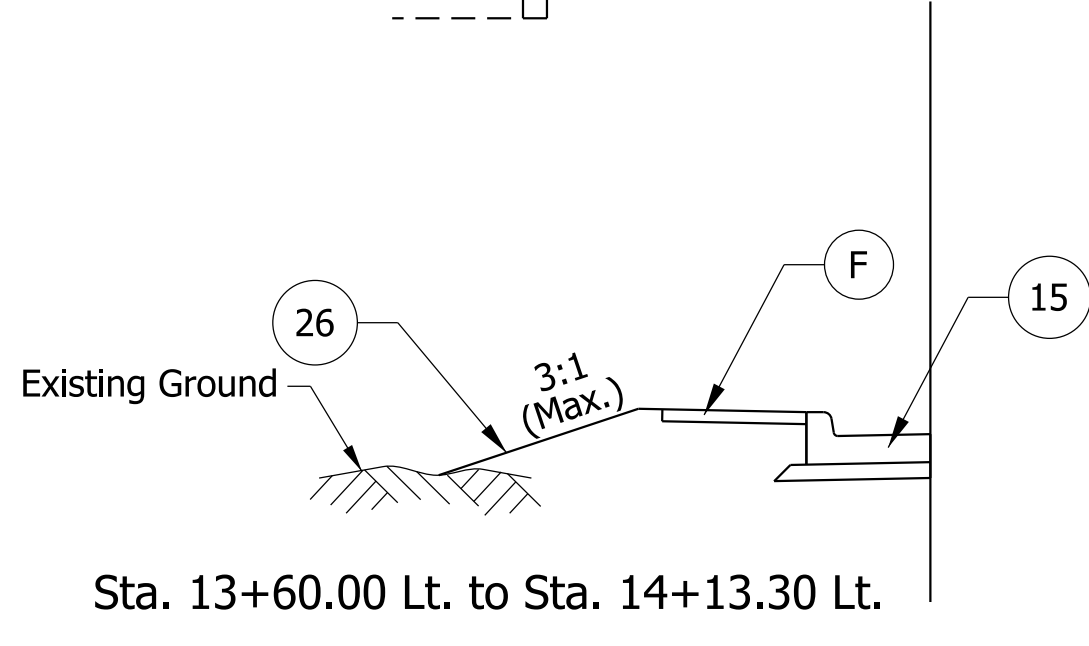
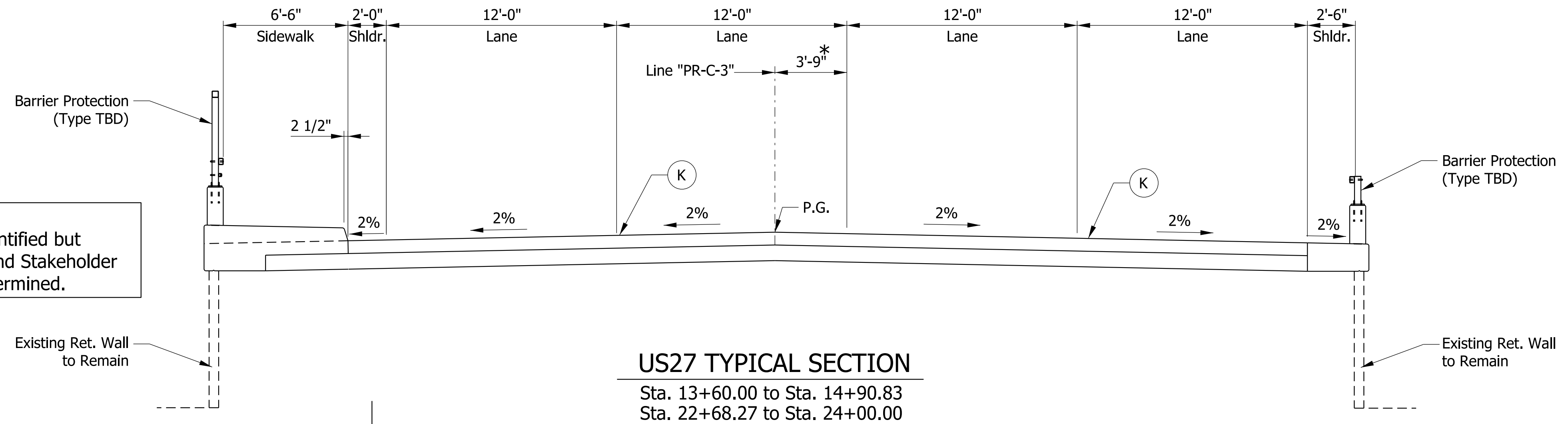
IP\_PWP:dms32629\US 27\_BR\_Title.dgn  
09-JUL-2020

PLANS PREPARED BY:	PARSONS	317-616-1000 PHONE NUMBER
CERTIFIED BY:		DATE
APPROVED FOR LETTING:	INDIANA DEPARTMENT OF TRANSPORTATION	DATE

BRIDGE FILE	
027-89-02814	
DESIGNATION	
1702941	
SURVEY BOOK	SHEETS
ELECTRONIC	1 of 20
CONTRACT	PROJECT
B-41510	1702941

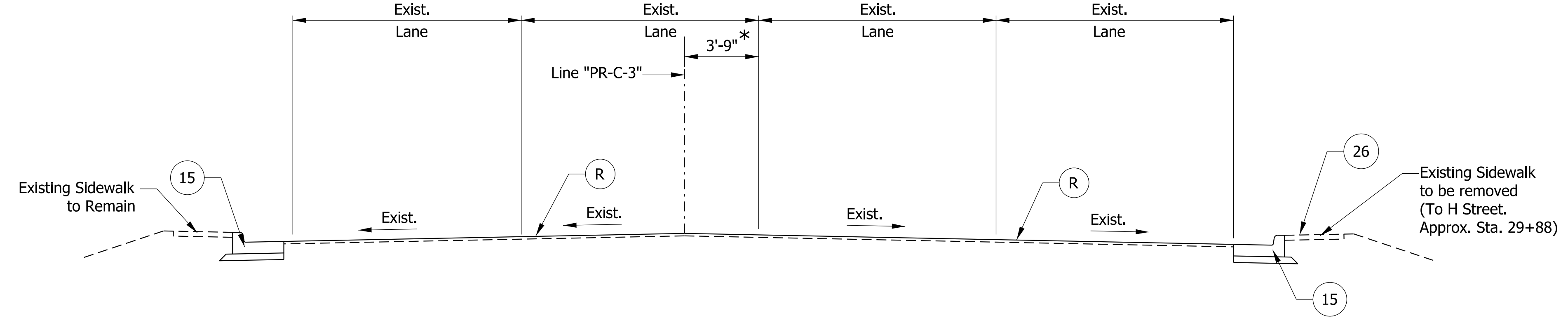


Note to Reviewer:  
Barrier Protection has been identified but will require INDOT reference and Stakeholder & Public Input. Type to be Determined.



\* Width Varies from 0' to 3'-9"  
from Sta. 13+60.00 to Sta. 14+60.00  
Striped Median Varies from 1'-6" (Existing) to 0'  
from Station 13+60.00 to Sta. 14+60.00

Width Varies from 3'-9" to 0'  
from Sta. 23+28.80 to Sta. 24+60.00  
Striped Median varies from 0' to 3'-6" (Existing)  
from Sta. 23+28.80 to St. 24+60.00



Note to Reviewer:  
Coordination has Occurred with the INDOT TAC Committee about Existing Pedestrian Facilities. Some Facilities have been Determined to be Removed and not Replaced. Please Refer to Engineers Report for Coordination.

- (14) Integral Concrete Curb
- (15) Concrete Curb and Gutter
- (26) Nursery Sodding
- (F) Sidewalk, Concrete
- (K) Pavement, TBD
- (K1) Full Depth Patching, TBD
- (R) Resurface, TBD
- (S) Sawcut
- (X) Curb Ramp

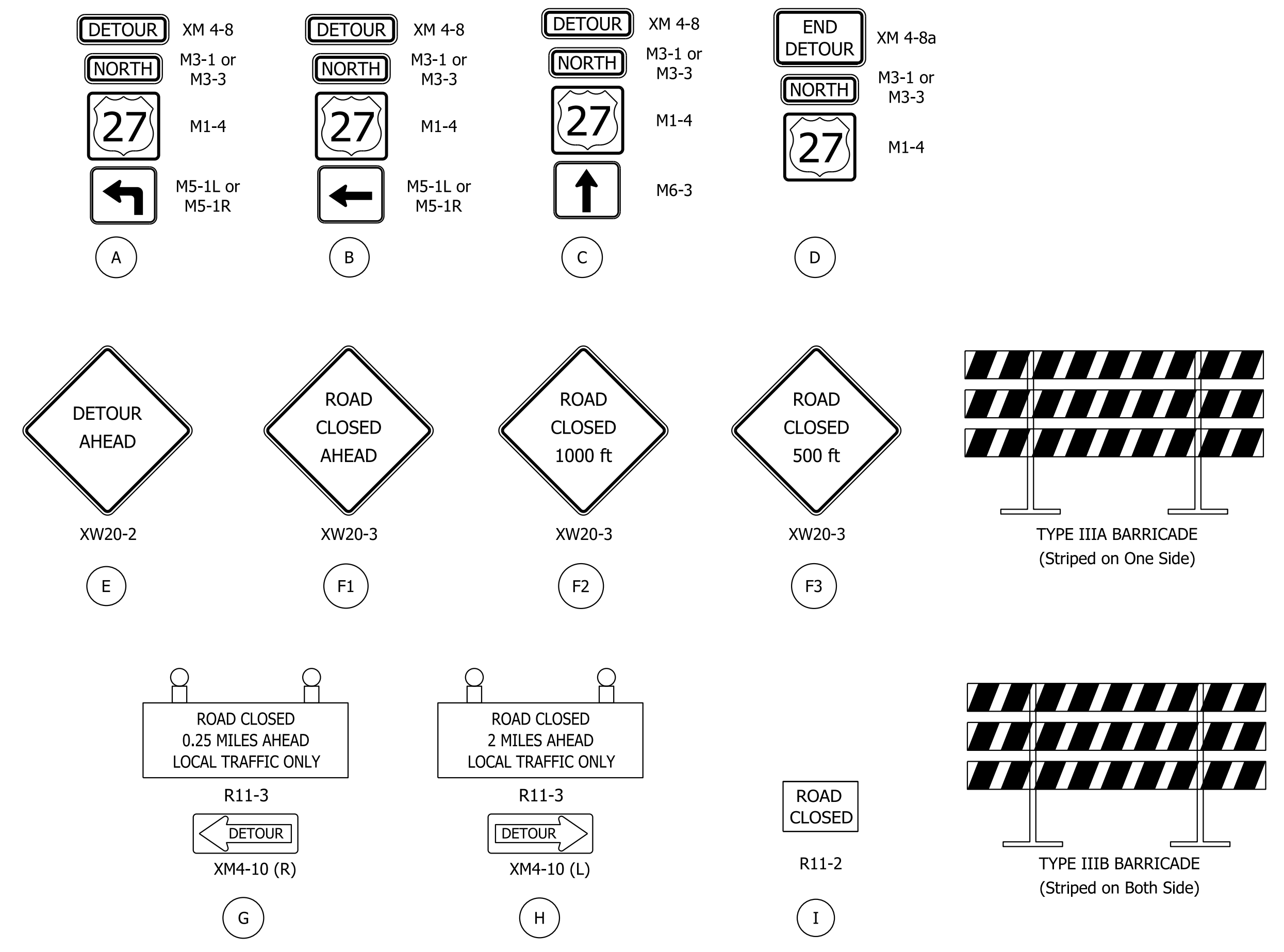
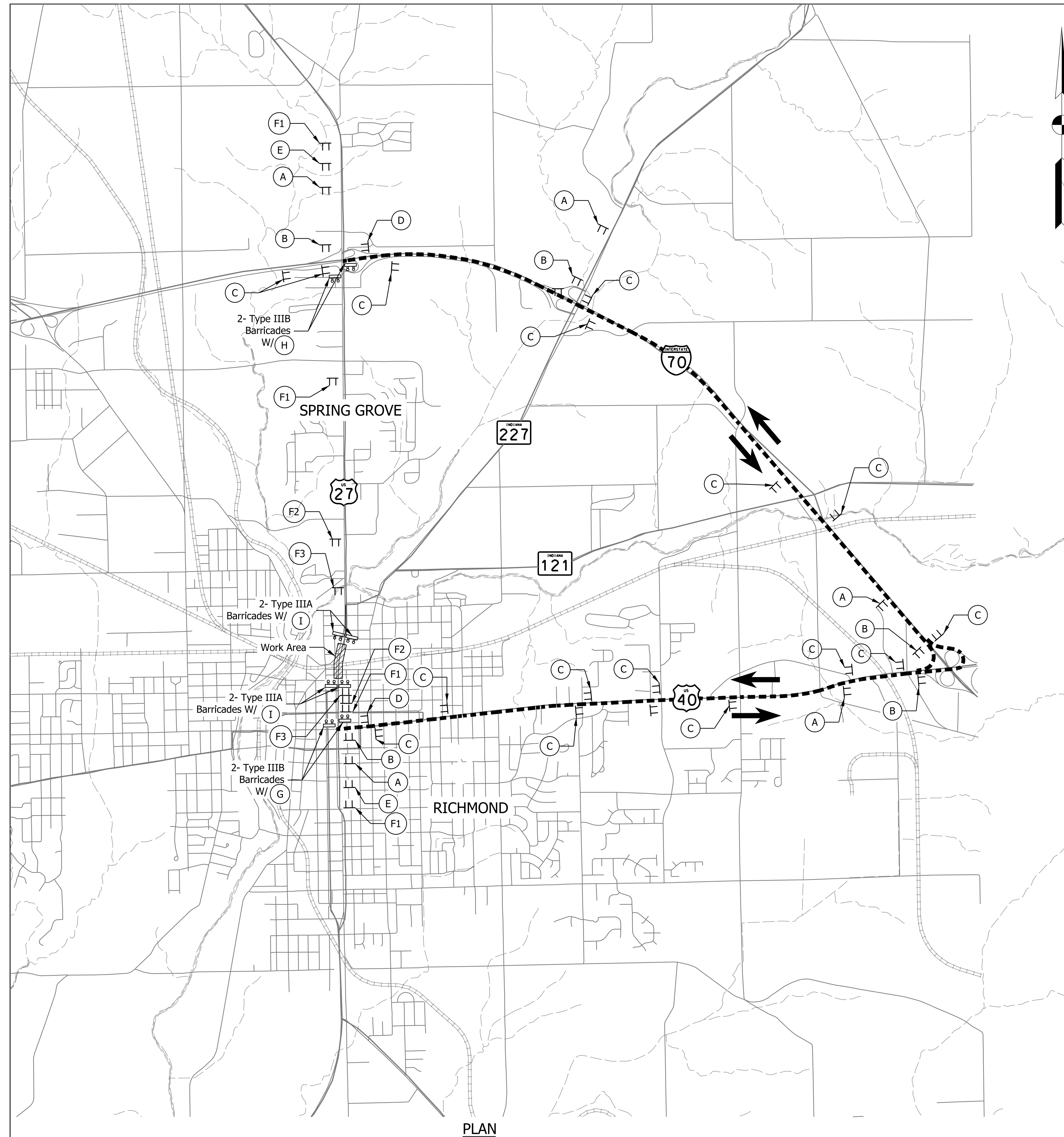
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: SSP	DRAWN: DH	
CHECKED: JTL	CHECKED: JTL	

**INDIANA  
DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS**

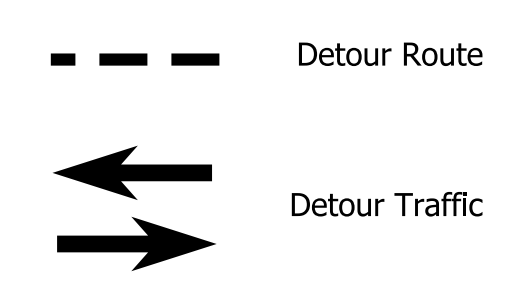
HORIZONTAL SCALE	BRIDGE FILE
1/8" = 1'	027-89-02814
VERTICAL SCALE	DESIGNATION
	1702941
SURVEY BOOK	SHEETS
	3 of 20
CONTRACT	PROJECT
B-41510	1702941

IP\_PWP:dms32637\US 27\_Sht\_Typical\_01.dgn  
07-JUL-2020



QUANTITY SUMMARY TABLE	
ITEM	QUANTITY
Construction Sign Type A	10 Each
Road Closure Sign Assembly	8 Each
Detour Route Marker Assembly	24 Each
Barricade Type IIIA	96 LFT.
Barricade Type IIIB	96 LFT.

**LEGEND**

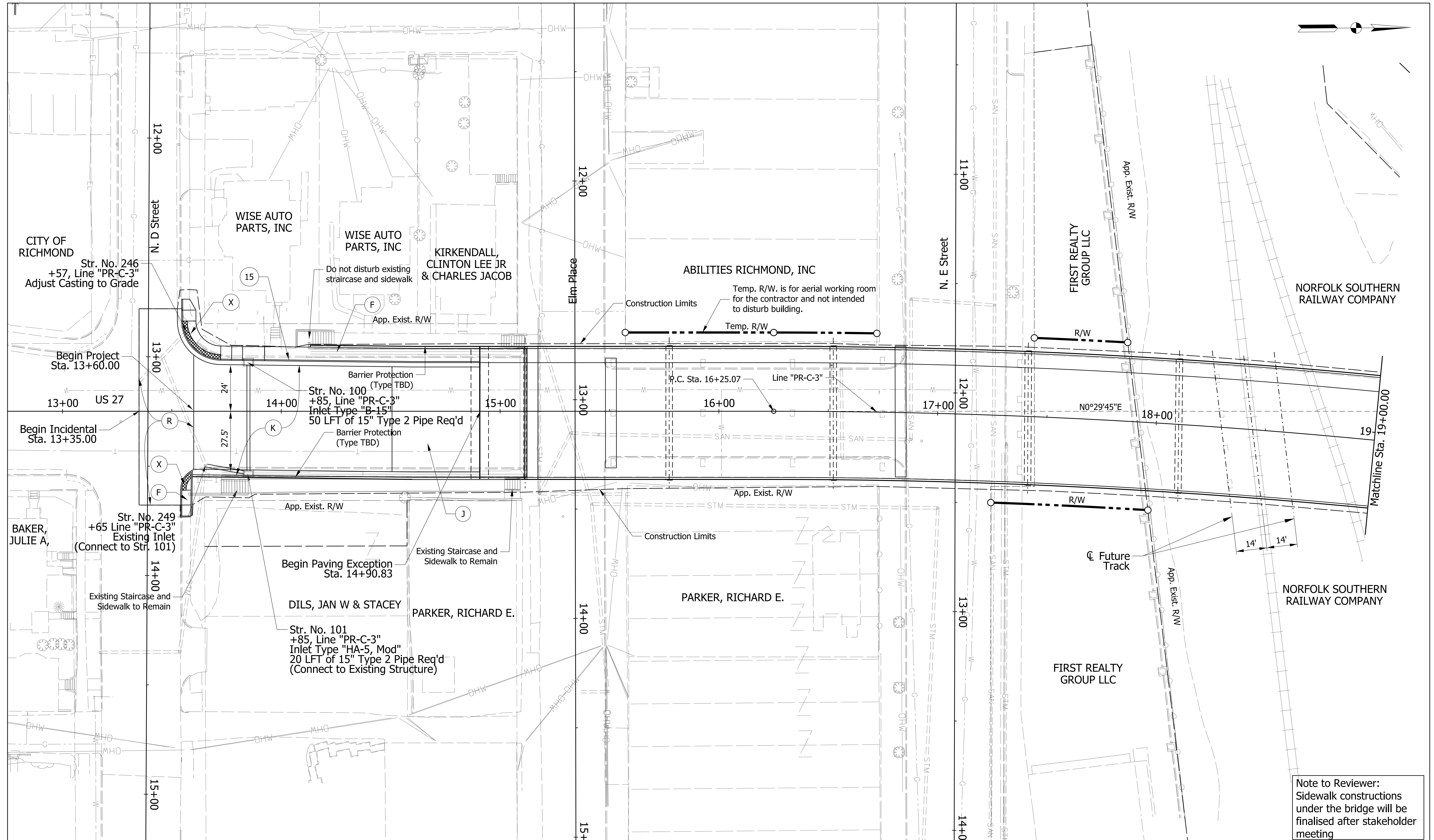


NOTE:  
 Confirming detour route marker assemblies (C) shall be placed every 0.5 miles along U.S. 40 and I-70  
 Detour signing to be in accordance with INDOT Std. Dwg. E801-TCDDT-01.

RECOMMENDED FOR APPROVAL _____ DESIGNED: SSP CHECKED: JTL	DESIGN ENGINEER _____	DATE _____	INDIANA DEPARTMENT OF TRANSPORTATION  DETOUR ROUTE	HORIZONTAL SCALE 1"=2000'	BRIDGE FILE 027-89-02814
	DRAWN: JEW			VERTICAL SCALE N/A	DESIGNATION 1702941
	CHECKED: JTL			SURVEY BOOK ELECTRONIC	SHEETS 5 of 20
				CONTRACT B-41510	PROJECT 1702941

IP\_PWP:dms32637\US 27\_SHT\_Detour.dgn  
09-JUL-2020





Note to Reviewer:  
Sidewalk constructions under the bridge will be finalised after stakeholder meeting

14	Integral Concrete Curb	K	Pavement, TBD	X	Curb Ramp
15	Concrete Curb and Gutter	K1	Full Depth Patching, TBD	J	JRPC for PCCP Terminal Joint
26	Nursery Sodding	R	Resurface, TBD		
F	Sidewalk, Concrete	S	Sawcut		

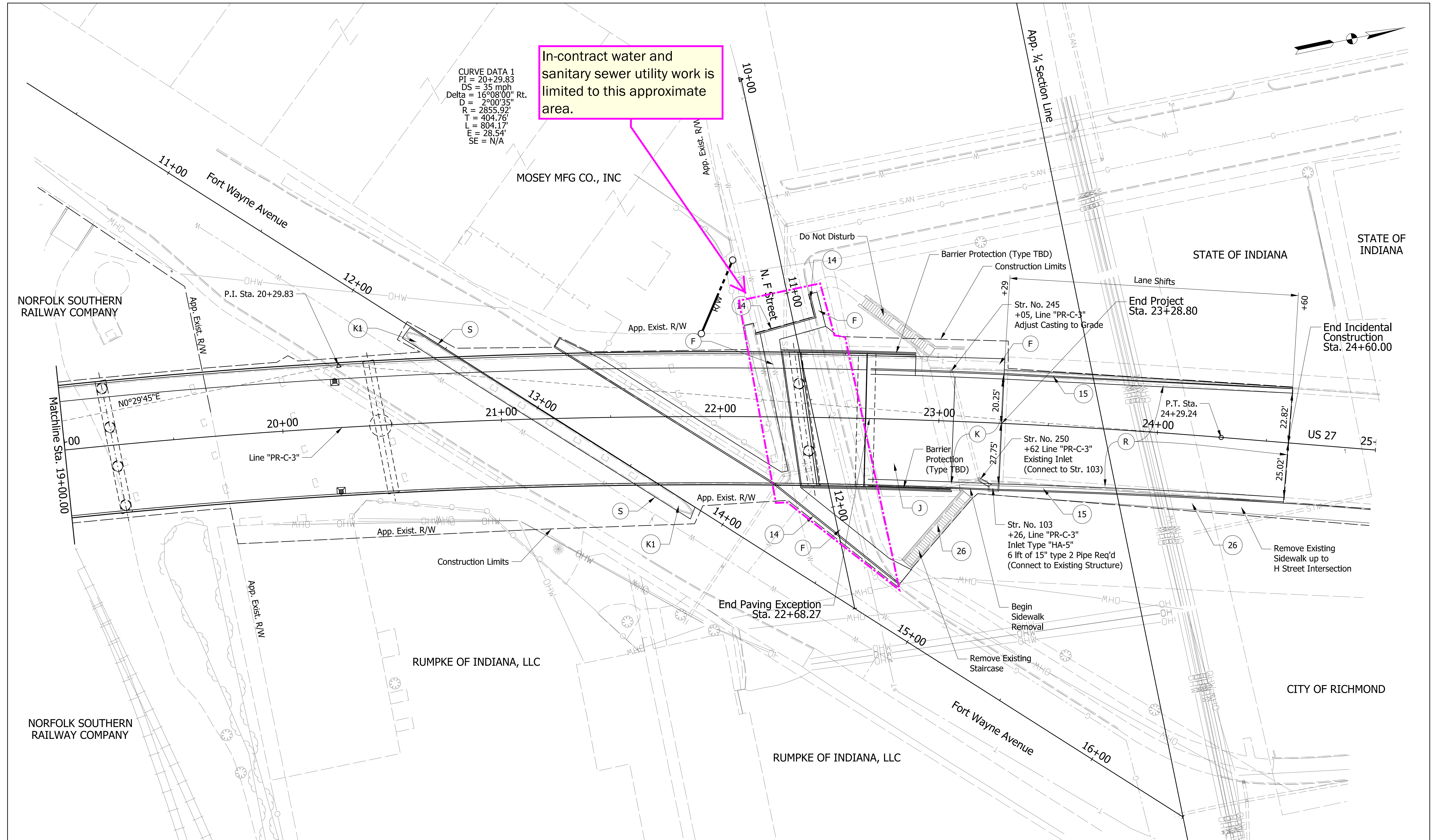
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: SSP	DRAWN: DH	
CHECKED: JTL	CHECKED: JTL	

INDIANA DEPARTMENT OF TRANSPORTATION	
PLAN SHEET	
STA. 13+00.00 TO STA. 19+00.00	

HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	027-89-02814
VERTICAL SCALE	DESIGNATION
N/A	1702941
SURVEY BOOK	SHEETS
	7 of 30
CONTRACT	PROJECT
B-41510	1702941

IP\_PWP:dms32637\US 27\_Sht\_Plan\_01.dgn  
14-DEC-2020





In-contract water and sanitary sewer utility work is limited to this approximate area.

CURVE DATA 1  
 PI = 20+29.83  
 DS = 35 mph  
 Delta = 16°08'00" Rt.  
 D = 2°00'35"  
 R = 2855.92'  
 T = 404.76'  
 L = 804.17'  
 E = 28.54'  
 SE = N/A

14	Integral Concrete Curb	K	Pavement, TBD	X	Curb Ramp
15	Concrete Curb and Gutter	K1	Full Depth Patching, TBD	J	JRCP for PCCP Terminal Joint
26	Nursery Sodding	R	Resurface, TBD		
F	Sidewalk, Concrete	S	Sawcut		

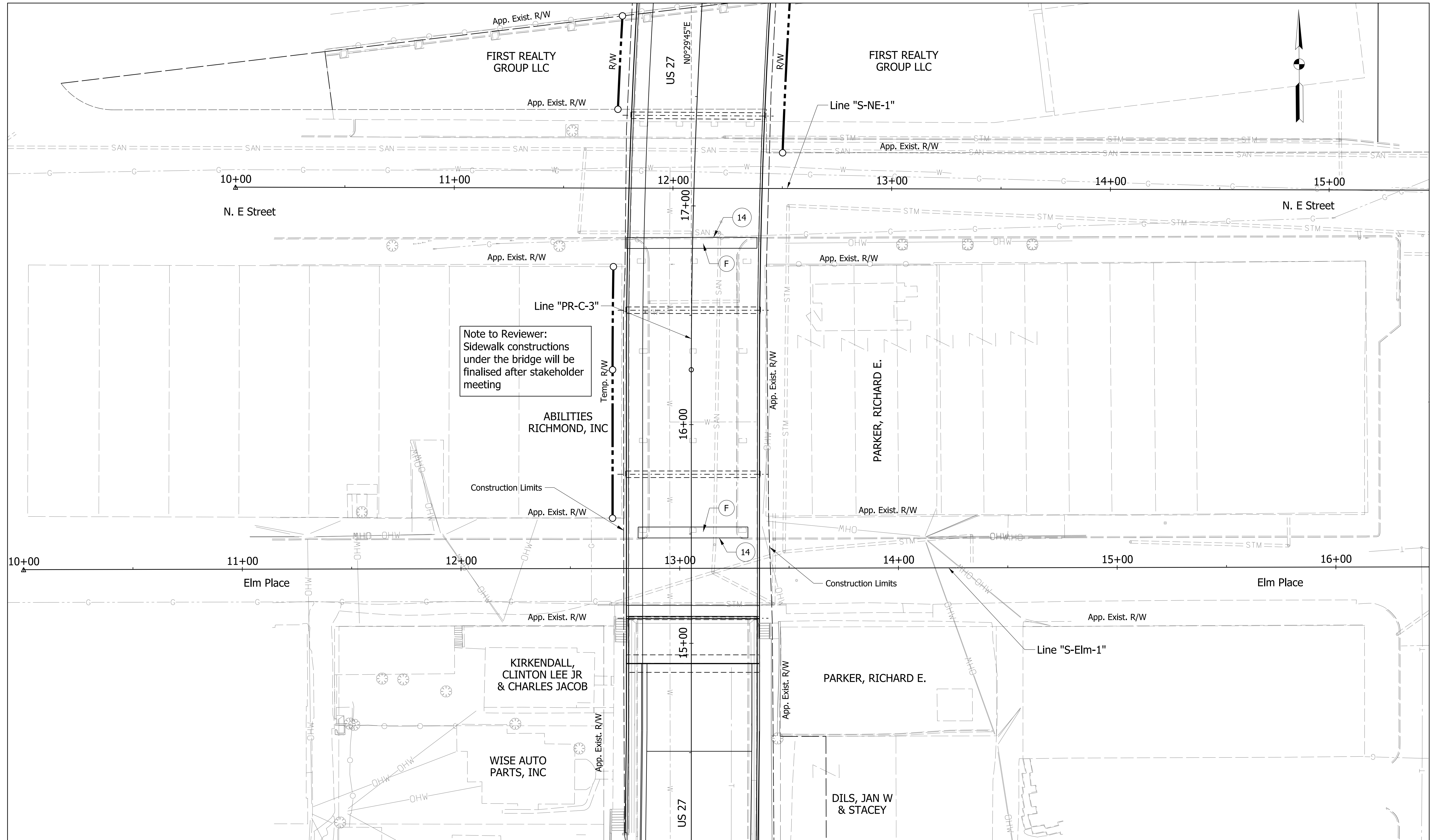
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: SSP	DRAWN: DH	
CHECKED: JTL	CHECKED: JTL	

INDIANA DEPARTMENT OF TRANSPORTATION  
 PLAN SHEET  
 STA. 19+00.00 TO STA. 25+00.00

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE 027-89-02814
VERTICAL SCALE N/A	DESIGNATION 1702941
SURVEY BOOK	SHEETS 9 of 30
CONTRACT B-41510	PROJECT 1702941

IP\_PWP:dms32637\US 27\_Sht\_Plan\_02.dgn  
 14-DEC-2020





Note to Reviewer:  
Sidewalk constructions  
under the bridge will be  
finalised after stakeholder  
meeting

14	Integral Concrete Curb	K	Pavement, TBD	X	Curb Ramp
15	Concrete Curb and Gutter	K1	Full Depth Patching, TBD	J	JRPC for PCCP Terminal Joint
26	Nursery Sodding	R	Resurface, TBD		
F	Sidewalk, Concrete	S	Sawcut		

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: SSP	DRAWN: DH	
CHECKED: JTL	CHECKED: JTL	

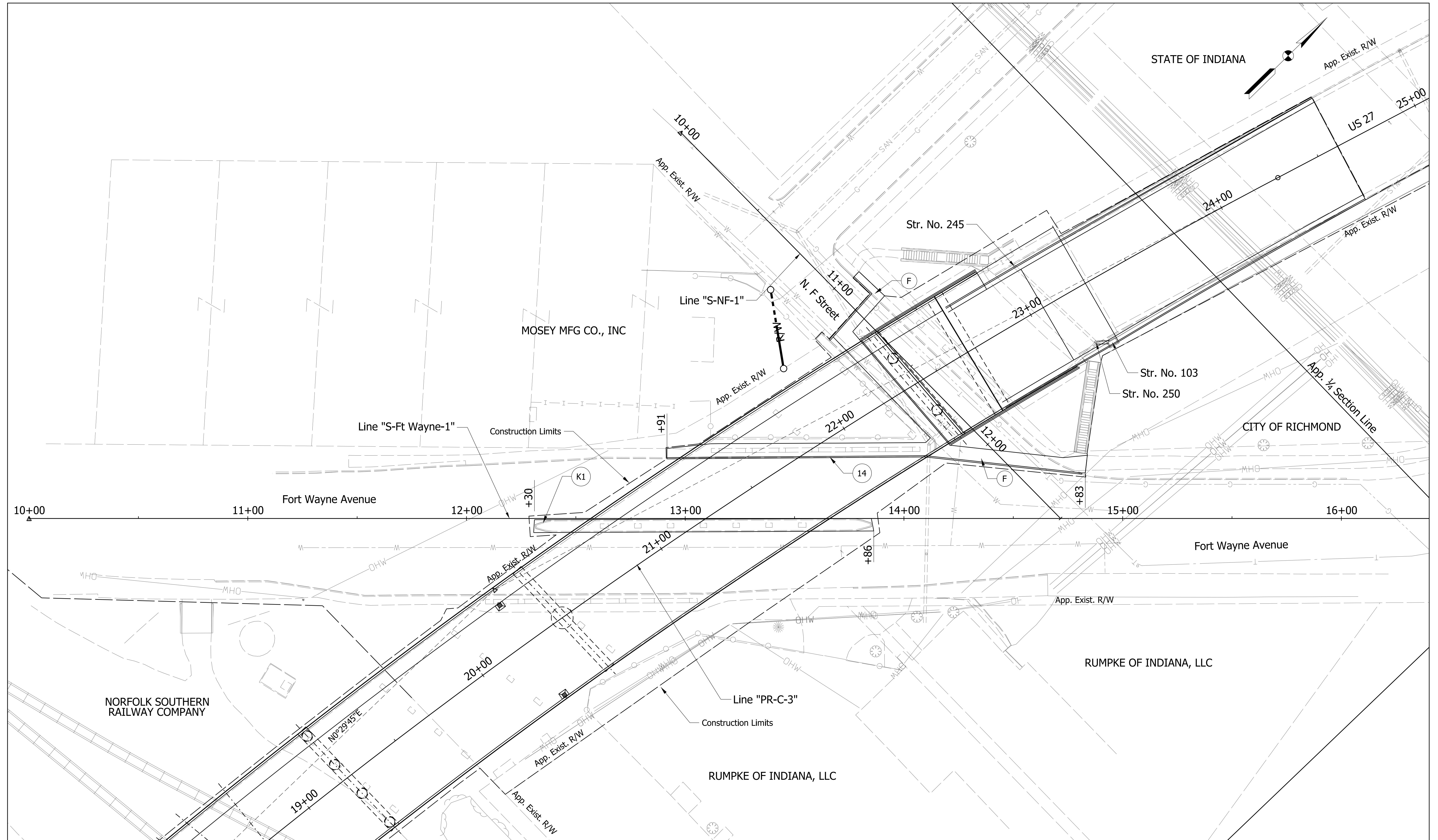
INDIANA  
DEPARTMENT OF TRANSPORTATION

LOCAL STREET PLAN SHEET

HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	027-89-02814
VERTICAL SCALE	DESIGNATION
N/A	1702941
SURVEY BOOK	SHEETS
	11 of 30
CONTRACT	PROJECT
B-41510	1702941

IP\_PWP:dms32637\US 27\_Sht\_Plan\_03.dgn  
14-DEC-2020





14	Integral Concrete Curb	K	Pavement, TBD	X	Curb Ramp
15	Concrete Curb and Gutter	K1	Full Depth Patching, TBD	J	JRPC for PCCP Terminal Joint
26	Nursery Sodding	R	Resurface, TBD		
F	Sidewalk, Concrete	S	Sawcut		

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: SSP	DRAWN: DH	
CHECKED: JTL	CHECKED: JTL	

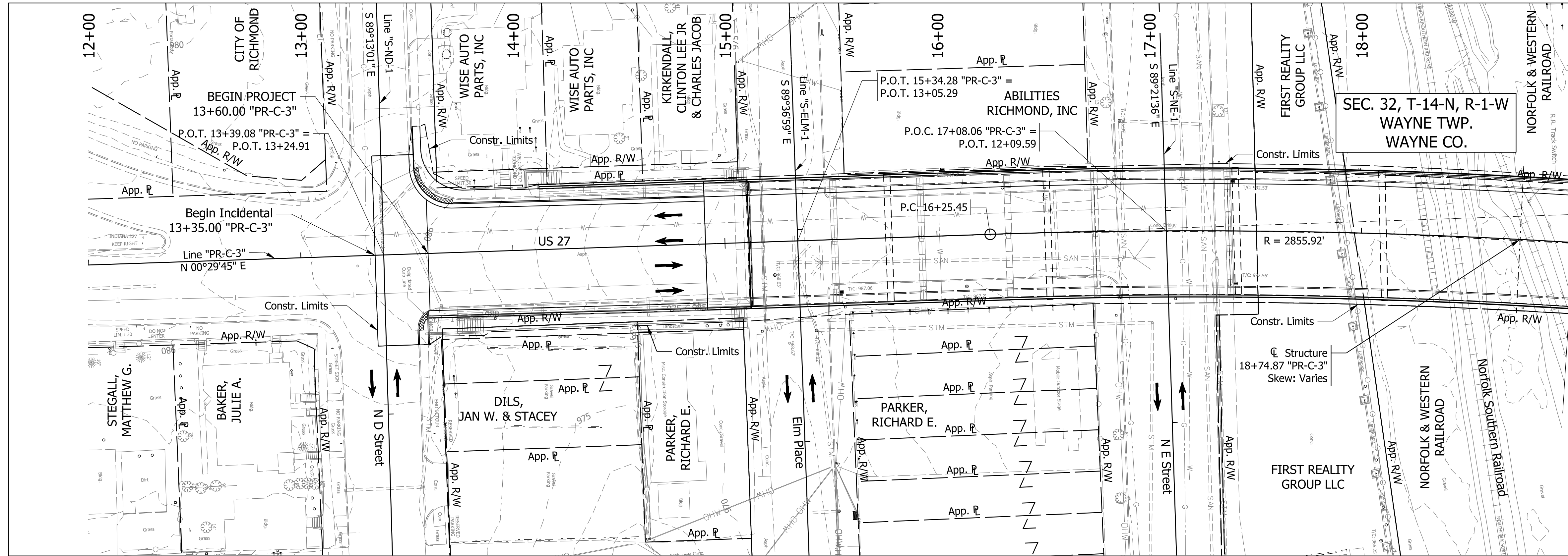
INDIANA  
DEPARTMENT OF TRANSPORTATION

LOCAL STREET PLAN SHEET

HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	027-89-02814
VERTICAL SCALE	DESIGNATION
N/A	1702941
SURVEY BOOK	SHEETS
	12 of 30
CONTRACT	PROJECT
B-41510	1702941

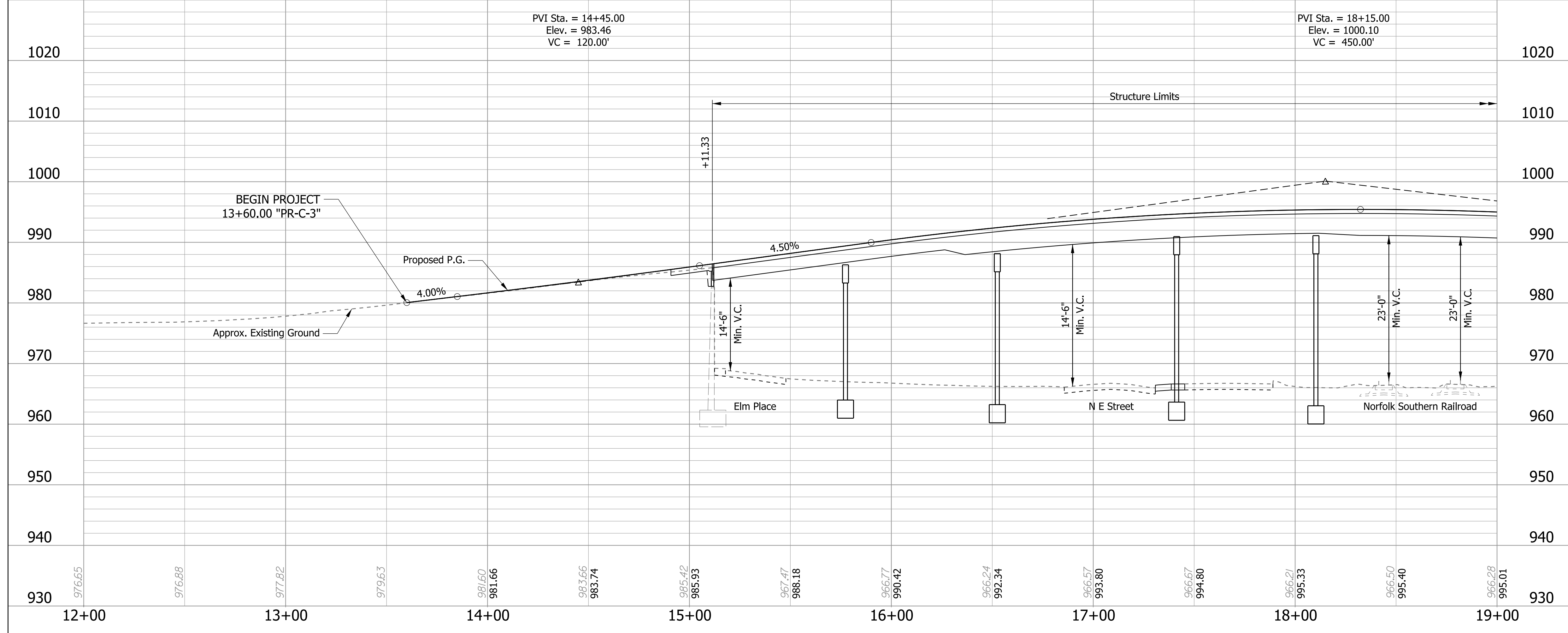
IP\_PWP:dms32637\US 27\_Sht\_Plan\_04.dgn  
14-DEC-2020





**NOTE TO REVIEWER:**  
 Proposed Condition for area beneath bridge between Elm Place and N. E Street to be determined through coordination with City of Richmond at a future submittal.

**EXISTING STRUCTURE**  
 The existing steel beam and prestressed box beam bridge was built in 1952 with 17 spans: 39'-6", 3 @ 41'-0", 61'-5", 93'-1 1/4", 93'-1 1/4", 3 @ 41'-0", 2 @ 60'-10 1/2", 36'-4 1/2", and 2 @ 41'-0". The existing bridge was widened in 1992 with 2 @ 24'-0" min. clear roadway reinforced concrete decks. Existing structure to be removed except Abutment 1 retaining wall to remain.



**EARTHWORK TABULATION**

Fill + 20%	= Cys
Common Excavation	= Cys
Usable Waterway Excavation (70%)	= Cys
Surplus Foundation Excavation (70%)	= Cys
Borrow	= Cys
Total Waterway Excavation	= Cys
Excavation Unclassified	= Cys
Benching (Estimated)	= Cys

No direct payment for Benching. Benching will not be paid for as Common Excavation.

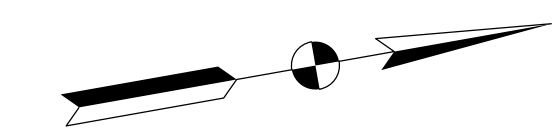
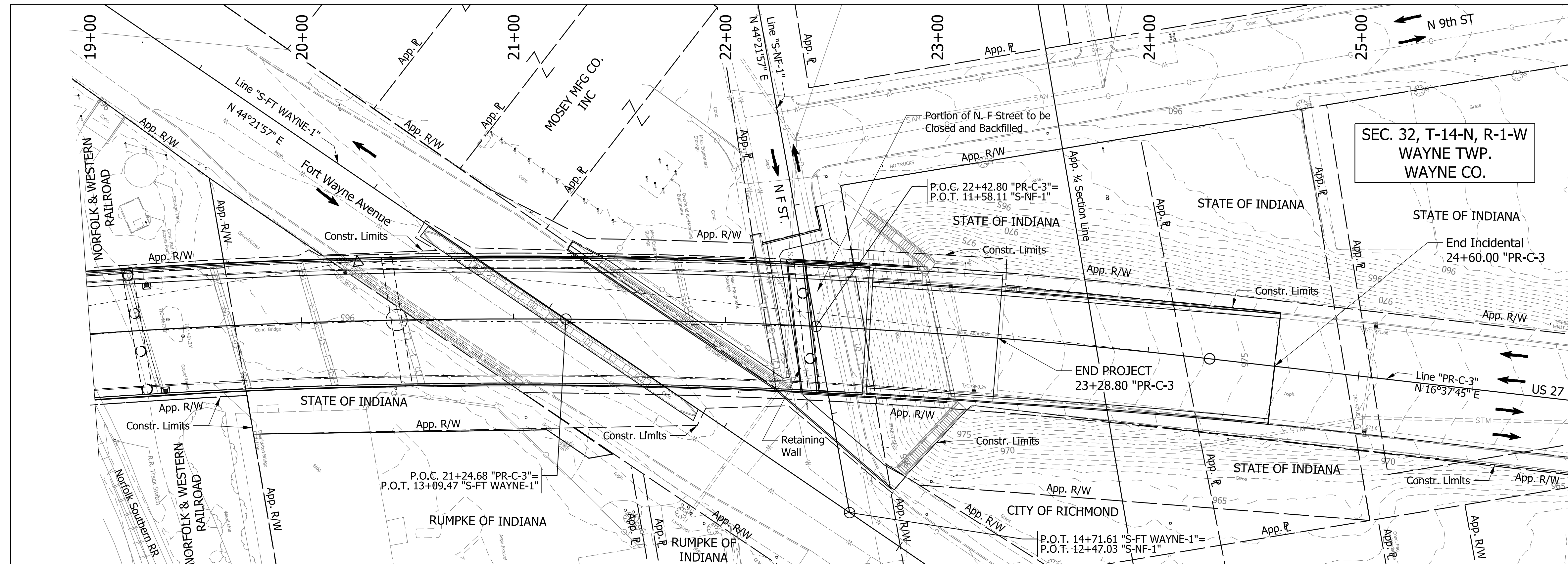
**NOTES:**  
 For R/W and Additional information see Roadway Plan & Profile sheets.  
 For Utility Contacts see Index Sheet No.2.

**CONTINUOUS COMPOSITE CURVED STEEL PLATE GIRDER BRIDGE**  
 7 SPANS: 65'-10", 75'-0", 89'-0", 69'-0", 112'-0", 122'-6" & 193'-6"  
 52'-6" CLEAR ROADWAY;  
 SKEW: VARIES 0° MIN. TO 9° MAX.  
 US 27 OVER ELM PLACE, ALLEY, N E STREET, NORFOLK SOUTHERN RAILROAD, FORT WAYNE AVE, & N F STREET WAYNE COUNTY

NOTE TO REVIEWER: Foundations to be designed upon receipt of geotechnical report and are shown here for reference only.	RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER _____ DATE _____	INDIANA DEPARTMENT OF TRANSPORTATION  LAYOUT	HORIZONTAL SCALE 1"=30'-0"	BRIDGE FILE 027-89-02814
	DESIGNED: CEP DRAWN: JEW CHECKED: RAM CHECKED: RAM		VERTICAL SCALE 1"=10'-0"	DESIGNATION 1702941
	SURVEY BOOK ELECTRONIC CONTRACT B-41510		SHEETS 12 of 20 PROJECT 1702941	

IP\_PWP:dms32629\US 27\_BR\_Layout.01.dgn  
 09-JUL-2020





**EXISTING STRUCTURE**  
 The existing steel beam and prestressed box beam bridge was built in 1952 with 17 spans: 39'-6", 3 @ 41'-0", 61'-5", 93'-1 1/4", 93'-1 3/4", 3 @ 41'-0", 2 @ 60'-10 3/8", 36'-4 1/2", and 2 @ 41'-0". The existing bridge was widened in 1992 with 2 @ 24'-0" min. clear roadway reinforced concrete decks. Existing structure to be removed except Abutment 1 retaining wall to remain.

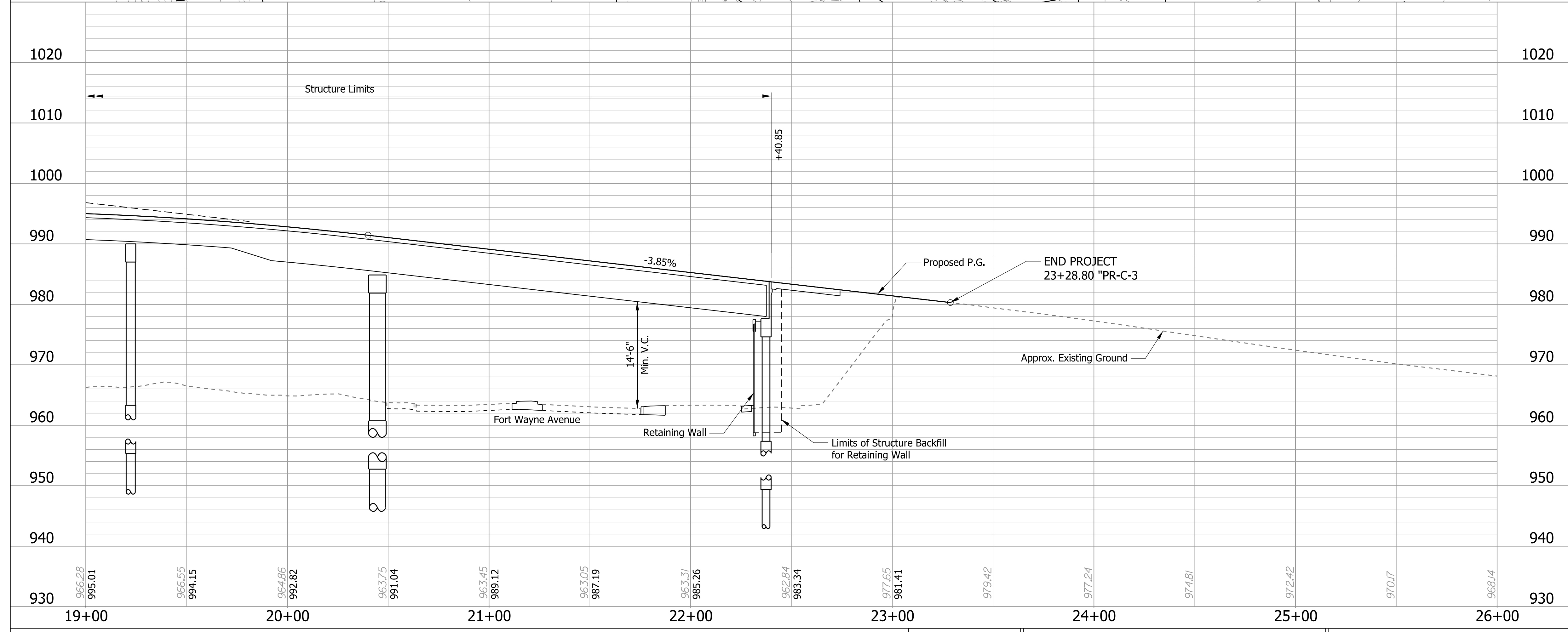
**EARTHWORK TABULATION**

Fill + 20%	= Cys
Common Excavation	= Cys
Usable Waterway Excavation (70%)	= Cys
Surplus Foundation Excavation (70%)	= Cys
Borrow	= Cys
Total Waterway Excavation	= Cys
Excavation Unclassified	= Cys
Benching (Estimated)	= Cys

No direct payment for Benching. Benching will not be paid for as Common Excavation.

**NOTES:**  
 For R/W and Additional information see Roadway Plan & Profile sheets.  
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 7 SPANS: 65'-10", 75'-0", 89'-0", 69'-0", 112'-0", 122'-6" & 193'-6"  
 52'-6" CLEAR ROADWAY;  
 SKEW: VARIES 0° MIN. TO 9° MAX.  
 US 27 OVER ELM PLACE, ALLEY, N E STREET, NORFOLK SOUTHERN RAILROAD, FORT WAYNE AVE, & N F STREET WAYNE COUNTY



**NOTE TO REVIEWER:**  
 Foundations to be designed upon receipt of geotechnical report and are shown here for reference only.

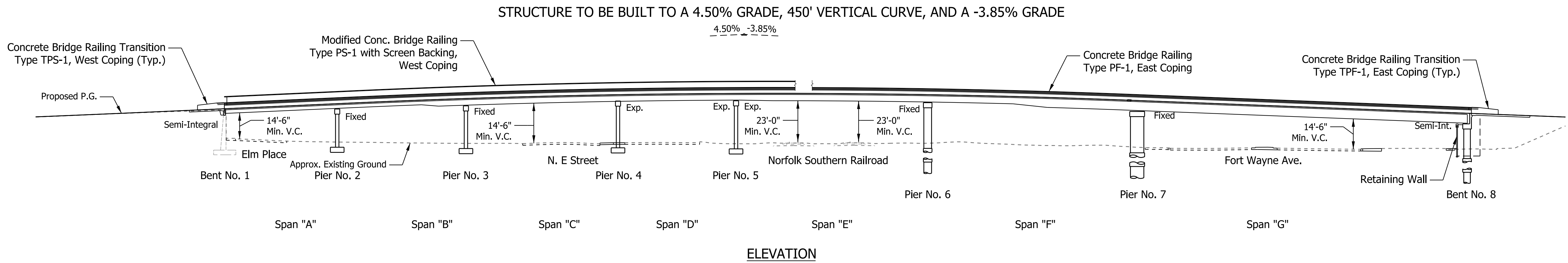
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: CEP	DRAWN: JEW	
CHECKED: RAM	CHECKED: RAM	

INDIANA DEPARTMENT OF TRANSPORTATION	
LAYOUT	

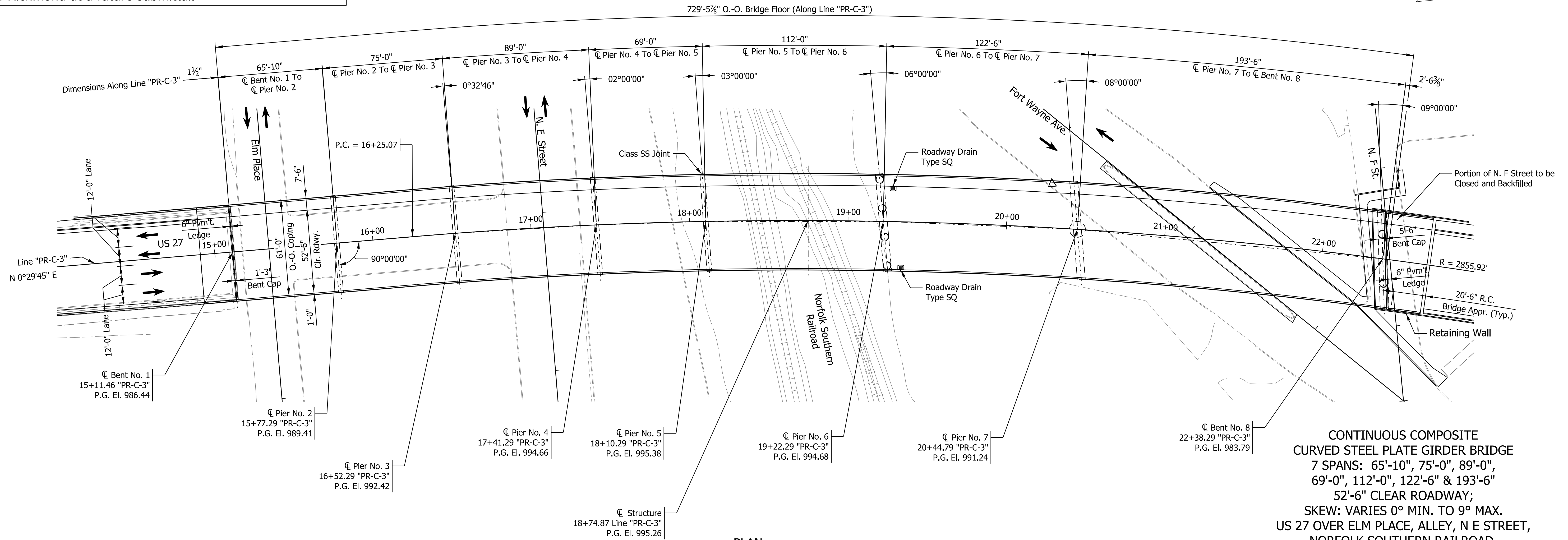
HORIZONTAL SCALE	BRIDGE FILE
1"=30'-0"	027-89-02814
VERTICAL SCALE	DESIGNATION
1"=10'-0"	1702941
SURVEY BOOK	SHEETS
ELECTRONIC	13 of 20
CONTRACT	PROJECT
B-41510	1702941

IP\_PWP:dms32629\US 27\_BR\_Layout.02.dgn  
 09-JUL-2020





**NOTE TO REVIEWER:**  
 Proposed Condition for area beneath bridge between Elm Place and N. E Street to be determined through coordination with City of Richmond at a future submittal.



**CONTINUOUS COMPOSITE CURVED STEEL PLATE GIRDER BRIDGE**  
 7 SPANS: 65'-10", 75'-0", 89'-0", 69'-0", 112'-0", 122'-6" & 193'-6"  
 52'-6" CLEAR ROADWAY;  
 SKEW: VARIES 0° MIN. TO 9° MAX.  
 US 27 OVER ELM PLACE, ALLEY, N E STREET, NORFOLK SOUTHERN RAILROAD, FORT WAYNE AVE, & N F STREET WAYNE COUNTY

RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER _____ DATE _____	INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE 3/32" = 1'-0"	BRIDGE FILE 027-89-02814
	GENERAL PLAN		VERTICAL SCALE 3/32" = 1'-0"	DESIGNATION 1702941
SURVEY BOOK ELECTRONIC			14	SHEETS of 20
DESIGNED: CEP    DRAWN: EWM			CONTRACT B-41510	PROJECT 1702941
CHECKED: RAM    CHECKED: RAM				

IP\_PWP:dms32629\US 27\_BR\_General Plan.01.dgn  
 09-JUL-2020

# Appendix C

## Early Coordination

January 15, 2020

Sample Early  
Coordination Letter

[Address Block]

Re: Des. No.: 1702941  
Description: US 27 over the Norfolk Southern Railroad (NSRR) and Local Streets  
City of Richmond, Wayne County, Indiana

[Greeting],

The Indiana Department of Transportation (INDOT) is planning a bridge replacement project on US 27 over the Norfolk Southern Railroad (NSRR) and local streets in the City of Richmond, Wayne County, Indiana. Specifically, the project is located in the United States Geological Survey (USGS) Topographic Richmond Quadrangle Map, in Sections 32 and 33, Township 14 North, Range 1 West (39.83472222°, -84.89027778°). Environmental analysis is being conducted for this project. The project is funded, in part, by the Federal Highway Administration (FHWA). This letter is part of the early coordination phase of the environmental review process. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above designation number and description in your reply. We will incorporate your comments into a study of the project's environmental impacts.

**Purpose and Need:** The needs for this project are due to the deteriorated condition of the current structure, INDOT Structure No. 027-89-02136 B, and pedestrian facilities on the bridge that do not meet current standards. According to the April 9, 2019 *Bridge Inspection Report*, although the superstructure has a rating of 7 (out of 9; good condition), the substructure was rated 4 (poor condition). Concrete spalls are falling from the existing bridge substructure onto the roads and sidewalks below. Spalling, cracking, and delaminating concrete was noted throughout the structure. The existing 17-span noncontinuous bridge layout places unprotected piers in the middle of busy streets, and there are areas of collision damage. The pedestrian facilities on the bridge are substandard, lack fencing/handrails, and do not meet Americans with Disabilities Act (ADA) requirements. Additionally, there are existing concrete staircases on all four quadrants of the bridge that are substandard and deteriorating.

The purpose of the project is to extend the service life of the US 27 crossing over the NSRR and local streets by at least 50 years, and to reduce exposure to pedestrians by providing a pedestrian facility that meets current standards, including ADA.

**Existing Conditions:** US 27 serves as a main north-south route for the downtown Richmond area, and the existing bridge is approximately 0.3 miles north of westbound US 40. In addition to crossing NSRR, the US 27 bridge crosses five local streets. Of those five, N. E Street and Fort Wayne Avenue are both major collectors; two are local roads, Elm Place and N. F Street; and the fifth is an alley completely covered by the existing bridge that connects Elm Place and N. E Street. Site location maps and project area photographs are attached.

The existing 17-span complex bridge carries two 12-foot wide travel lanes in each direction, with 4.5-foot wide sidewalks on each side. The US 27 bridge has undergone rehabilitation twice since it was built, including repairs in 1978 and 1992.

Early Coordination Letter  
US 27 over NSRR and Local Streets  
January 15, 2020

**Proposed Project:** The recommended alternative for this project would replace the existing structure with an 8-span bridge on the same alignment, with no anticipated grade change. The segment of N. F Street located at the north bridge abutment would be permanently closed to traffic and filled. The existing sidewalks on the bridge would be replaced with an ADA-compliant pedestrian facility on the west side of the bridge. The eastern sidewalk and staircases would be removed, while the northwestern staircase would remain. Guardrail would be replaced as-needed. Less than 0.5 acre of right-of-way acquisition is anticipated. During construction, the facility would be closed to traffic and a detour would be provided.

**Environmental Concerns:** The project area is urban and drained by city sewers. Therefore, no water resources are anticipated. All applicable permits will be applied for and acquired before construction can begin. Parsons will continue to work in coordination with the INDOT Ecology and Waterway Permitting Office to determine the presence and impacts to ecological resources (if any).

This project is within the range of the federally endangered Indiana bat (*Myotis sodalis*) and federally threatened northern long-eared bat (*Myotis septentrionalis*). The project area is urban and the nearest suitable summer habitat is over 1,200 feet to the west, along the Whitewater River gorge. The Indiana Bat and Northern Long-eared Bat Range-Wide Programmatic Informal Consultation is anticipated to apply to this project. Project information was uploaded to the United States Fish and Wildlife Service's (USFWS) Information for Planning and Consultation (IPaC) website to identify if any species listed or proposed to be listed may be present in the area of the proposed action. No species, other than bats, were identified.

The project area contains designated historic districts which are protected under Section 106 of the National Historic Preservation Act. The Section 106 process involves efforts to identify historic properties potentially affected by the undertaking, assess its effects and seek ways to avoid, minimize or mitigate any adverse effects on historic properties. For more information regarding the protection of historic resources, please see the Advisory Council on Historic Preservation's guide: Protecting Historic Properties: A Citizen's Guide to Section 106 Review available online at <https://www.achp.gov/digital-library-section-106-landing/citizens-guide-section-106-review>. If you did not receive an invitation to be a consulting party for Section 106, and would like to be included, please let us know.

Please respond with your comments on any environmental impacts associated with this project. **Should we not receive your response within thirty (30) calendar days from the date of this letter, it will be assumed that your agency feels that there will be no adverse effects incurred as a result of the proposed project.** However, should you find that an extension to the response time is necessary, a reasonable amount may be granted upon request. The INDOT Project Manager, Nathan Riggs, can be contacted at (317) 467-3986 or via email at [nriggs@indot.in.gov](mailto:nriggs@indot.in.gov). If you have any questions regarding this matter, please contact me at (317) 616-1021 or via e-mail at [Keaton.Veldkamp@parsons.com](mailto:Keaton.Veldkamp@parsons.com). Thank you in advance for your input.

Sincerely,



Keaton Veldkamp  
Associate Environmental Planner  
Parsons

Attachments: Graphics

Attachments Omitted  
to Avoid Duplication



Early Coordination Letter  
US 27 over NSRR and Local Streets  
January 15, 2020

Sent via email on 1/15/2020  
unless otherwise noted

The following agencies received Early Coordination Letters:

U.S. Fish and Wildlife Service  
Bloomington Indiana Field Office  
620 South Walker Street  
Bloomington, IN 47403

Indiana Geological and Water Survey  
611 North Walnut Grove  
Bloomington, IN 47405  
(Electronic Coordination) **Electronic Coordination on 1/15/2020**

Federal Highway Administration  
Federal Office Building  
575 N Pennsylvania Street, Room 254  
Indianapolis, IN 46204

Indiana Department of Transportation  
Greenfield District Office  
32 South Broadway  
Greenfield, IN 46140

Field Environmental Officer  
Chicago Regional Office  
US Department of Housing & Urban Development  
Metcalf Fed. Bldg.  
77 W Jackson Blvd. Room 2401  
Chicago, IL 60604

City of Richmond  
MS4 Coordinator  
2380 Liberty Avenue  
Richmond, IN 47374

Regional Environmental Coordinator  
Midwest Regional Office  
National Park Service **Hard copy mailed on 1/15/2020**  
601 Riverfront Drive  
Omaha, Nebraska 68102

City of Richmond, Parks & Recreation Board  
50 North 5<sup>th</sup> Street  
Richmond, IN 47374 **Hard copy mailed on 1/15/2020**

Natural Resources Conservation Service  
State Conservation Office  
6013 Lakeside Boulevard  
Indianapolis, IN 46278

Wayne County Highway Department  
8198 US 40  
Centerville, IN 47330

Wayne County Surveyor  
401 East Main Street, 2<sup>nd</sup> Floor  
Richmond, IN 47374 **Hard copy mailed on 1/15/2020**

Indiana Department of Transportation  
Public Hearings Manager  
100 North Senate Avenue, Room 642  
Indianapolis, IN 46204

City of Richmond: Infrastructure & Development  
Community Development  
Public Works & Engineering  
50 North 5<sup>th</sup> Street  
Richmond, IN 47374

Indiana Department of Natural Resources  
Division of Fish and Wildlife  
Room W264, IGC South  
402 W Washington Street  
Indianapolis, IN 46204

Mayor of Richmond  
50 North 5<sup>th</sup> Street  
Richmond, IN 47374

Indiana Department of Environmental Management  
100 North Senate Avenue **Electronic Coordination on 1/15/2020**  
Indianapolis, IN 46204  
(Electronic Coordination)

Richmond City Council (via email)  
Wayne County Trustees (via email) **Hard copy mailed on 1/15/2020**

Richmond Community Schools  
300 Hub Etchison Parkway  
Richmond, IN 47374

Early Coordination Letter  
US 27 over NSRR and Local Streets  
January 15, 2020

Seton Catholic Schools  
700 North A Street  
Richmond, IN 47374

Richmond Fire Department  
1010 South 5<sup>th</sup> Street  
Richmond, IN 47374

Hard copy mailed  
on 1/15/2020

Richmond Police Department  
50 North 5<sup>th</sup> Street  
Richmond, IN 47374

Hard copy mailed  
on 1/15/2020

Wayne County Emergency Management  
401 East Main Street  
Richmond, IN 47374

Rose View Transit-City of Richmond  
50 North 5<sup>th</sup> Street  
Richmond, IN 47374

Norfolk Southern Corporation  
Three Commercial Place  
Norfolk, Virginia 23510

Emailed  
Steve.Aufdenkampe@nscorp.com and  
Eldridge.Chambers@nscorp.com  
on 1/15/2020

State Senator Jeff Raatz  
Senate District 27  
200 West Washington Street  
Indianapolis, IN 46204

State Representative Brad Barrett  
House District 56  
200 West Washington Street  
Indianapolis, IN 46204

Hard copy mailed  
on 1/15/2020

**THIS IS NOT A PERMIT**

**State of Indiana**  
**DEPARTMENT OF NATURAL RESOURCES**  
**Division of Fish and Wildlife**  
**Early Coordination/Environmental Assessment**

**DNR #:** ER-22135

**Request Received:** January 15, 2020

**Requestor:** Parsons  
Keaton Veldkamp  
101 West Ohio Street, Suite 2121  
Indianapolis, IN 46204

**Project:** US 27 bridge (#027-89-02136 B) replacement over Norfolk Southern Railroad and local streets, about 0.3 mile north of US 40, Richmond; Des #1702941

**County/Site info:** Wayne

The Indiana Department of Natural Resources has reviewed the above referenced project per your request. Our agency offers the following comments for your information and in accordance with the National Environmental Policy Act of 1969.

If our agency has regulatory jurisdiction over the project, the recommendations contained in this letter may become requirements of any permit issued. If we do not have permitting authority, all recommendations are voluntary.

**Regulatory Assessment:** Formal approval by the Department of Natural Resources under the regulatory programs administered by the Division of Water is not required for this project.

**Natural Heritage Database:** The Natural Heritage Program's data have been checked. To date, no plant or animal species listed as state or federally threatened, endangered, or rare have been reported to occur in the project vicinity.

**Fish & Wildlife Comments:** The measures below should be implemented to avoid, minimize, or compensate for impacts to fish, wildlife, and botanical resources:

1. Revegetate all bare and disturbed areas with a mixture of grasses (excluding all varieties of tall fescue) and legumes as soon as possible upon completion; low endophyte tall fescue may be used in the ditch bottom and side slopes only.
2. Appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from entering the stream or leaving the construction site; maintain these measures until construction is complete and all disturbed areas are stabilized.
3. Plant five trees, at least 2 inches in diameter-at-breast height, for each tree which is removed that is ten inches or greater in diameter-at-breast height.

**Contact Staff:** Christie L. Stanifer, Environ. Coordinator, Fish & Wildlife  
Our agency appreciates this opportunity to be of service. Please contact the above staff member at (317) 232-4080 if we can be of further assistance.



**Date:** February 11, 2020

Christie L. Stanifer  
Environ. Coordinator  
Division of Fish and Wildlife



## Veldkamp, Keaton

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**From:** McWilliams, Robin <robin\_mcwilliams@fws.gov>  
**Sent:** Thursday, January 23, 2020 10:12 AM  
**To:** Veldkamp, Keaton  
**Subject:** Re: [EXTERNAL] Early Coordination, Des. No 1702941, US 27 Bridge Replacement over NSRR, Richmond, IN

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

Dear Keaton,

This responds to your recent letter, requesting our comments on the aforementioned project.

These comments have been prepared under the authority of the Fish and Wildlife Coordination Act (16 U.S.C. 661 et. seq.) and are consistent with the intent of the National Environmental Policy Act of 1969, the Endangered Species Act of 1973, and the U. S. Fish and Wildlife Service's Mitigation Policy.

The project is within the range of the Indiana bat (*Myotis sodalis*) and northern long-eared bat (*Myotis septentrionalis*) and should follow the new Indiana bat/northern long-eared bat programmatic consultation process, if applicable (*i.e.* a federal transportation nexus is established). We will review that information once it is received.

Based on a review of the information you provided, the U.S. Fish and Wildlife Service has no objections to the project as currently proposed. However, should new information arise pertaining to project plans or a revised species list be published, it will be necessary for the Federal agency to reinstate consultation. Standard recommendations are provided below.

We appreciate the opportunity to comment at this early stage of project planning. If project plans change such that fish and wildlife habitat may be affected, please re-coordinate with our office as soon as possible. If you have any questions about our recommendations, please call (812) 334-4261 x. 207.

Sincerely,  
Robin McWilliams Munson

Standard Recommendations:

1. Do not clear trees or understory vegetation outside the construction zone boundaries. **(This restriction is not related to the "tree clearing" restriction for potential Indiana Bat habitat.)**
2. Restrict below low-water work in streams to placement of culverts, piers, pilings and/or footings, shaping of the spill slopes around the bridge abutments, and placement of riprap.

Culverts should span the active stream channel, should be either embedded or a 3-sided or open-arch culvert, and be installed where practicable on an essentially flat slope. When an open-bottom culvert or arch is used in a stream, which has a good

natural bottom substrate, such as gravel, cobbles and boulders, the existing substrate should be left undisturbed beneath the culvert to provide natural habitat for the aquatic community.

3. Restrict channel work and vegetation clearing to the minimum necessary for installation of the stream crossing structure.
4. Minimize the extent of hard armor (riprap) in bank stabilization by using bioengineering techniques whenever possible. If riprap is utilized for bank stabilization, extend it below low-water elevation to provide aquatic habitat.
5. Implement temporary erosion and sediment control methods within areas of disturbed soil. All disturbed soil areas upon project completion will be vegetated following INDOT's standard specifications.
6. Avoid all work within the inundated part of the stream channel (in perennial streams and larger intermittent streams) during the fish spawning season (April 1 through June 30), except for work within sealed structures such as caissons or cofferdams that were installed prior to the spawning season. No equipment shall be operated below Ordinary High Water Mark during this time unless the machinery is within the caissons or on the cofferdams.
7. Evaluate wildlife crossings under bridge/culverts projects in appropriate situations. Suitable crossings include flat areas below bridge abutments with suitable ground cover, high water shelves in culverts, amphibian tunnels and diversion fencing.

Robin McWilliams Munson

U.S. Fish and Wildlife Service  
620 South Walker Street  
Bloomington, Indiana 46403  
812-334-4261 x. 207 Fax: 812-334-4273

Monday, Tuesday - 7:30a-3:00p  
Wednesday, Thursday - telework 8:30a-3:00p

On Wed, Jan 15, 2020 at 10:43 AM Veldkamp, Keaton <[Keaton.Veldkamp@parsons.com](mailto:Keaton.Veldkamp@parsons.com)> wrote:

RE: US 27 Bridge Replacement over Norfolk Southern Railroad and Local Streets

Richmond, Wayne County, IN

Des. No. 1702941

Ms. McWilliams,

Please see the attached early coordination letter for this project.

Thank you,

## Veldkamp, Keaton

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**From:** Terrill, Todd <tterrill@rcs.k12.in.us>  
**Sent:** Wednesday, January 15, 2020 10:51 AM  
**To:** Veldkamp, Keaton  
**Subject:** [EXTERNAL] RE: Early Coordination, Des. No 1702941, US 27 Bridge Replacement over NSRR, Richmond, IN

Keaton,

Thank you for the information.

Dr. Todd Terrill  
Superintendent of Schools  
Richmond Community Schools  
300 Hub Etchison Parkway  
Richmond, IN 47374  
Phone: (765) 973-3335  
Email: [tterrill@rcs.k12.in.us](mailto:tterrill@rcs.k12.in.us)



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**From:** Veldkamp, Keaton <Keaton.Veldkamp@parsons.com>  
**Sent:** Wednesday, January 15, 2020 10:39 AM  
**To:** Terrill, Todd <tterrill@rcs.k12.in.us>  
**Subject:** Early Coordination, Des. No 1702941, US 27 Bridge Replacement over NSRR, Richmond, IN

RE: US 27 Bridge Replacement over Norfolk Southern Railroad and Local Streets  
Richmond, Wayne County, IN  
Des. No. 1702941

Mr. Terrill,

Please see the attached early coordination letter for this project.

Thank you,

**Keaton Veldkamp**  
Associate Environmental Planner  
101 West Ohio Street, Suite 2121 - Indianapolis, IN 46204  
[Keaton.Veldkamp@parsons.com](mailto:Keaton.Veldkamp@parsons.com) P: 317.616.1021

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## Organization and Project Information

**Project ID:**

**Des. ID:** 1702941

**Project Title:** US 27 Bridge Replacement over the Norfolk Southern Railroad and Local Streets

**Name of Organization:** Parsons

**Requested by:** Keaton Veldkamp

## Environmental Assessment Report

1. Geological Hazards:

- Moderate liquefaction potential
- Floodway

2. Mineral Resources:

- Bedrock Resource: Low Potential
- Sand and Gravel Resource: Low Potential

3. Active or abandoned mineral resources extraction sites:

- None documented in the area

\*All map layers from Indiana Map ([maps.indiana.edu](http://maps.indiana.edu))

### **DISCLAIMER:**

This document was compiled by Indiana University, Indiana Geological Survey, using data believed to be accurate; however, a degree of error is inherent in all data. This product is distributed "AS-IS" without warranties of any kind, either expressed or implied, including but not limited to warranties of suitability to a particular purpose or use. No attempt has been made in either the design or production of these data and document to define the limits or jurisdiction of any federal, state, or local government. The data used to assemble this document are intended for use only at the published scale of the source data or smaller (see the metadata links below) and are for reference purposes only. They are not to be construed as a legal document or survey instrument. A detailed on-the-ground survey and historical analysis of a single site may differ from these data and this document.

This information was furnished by Indiana Geological Survey

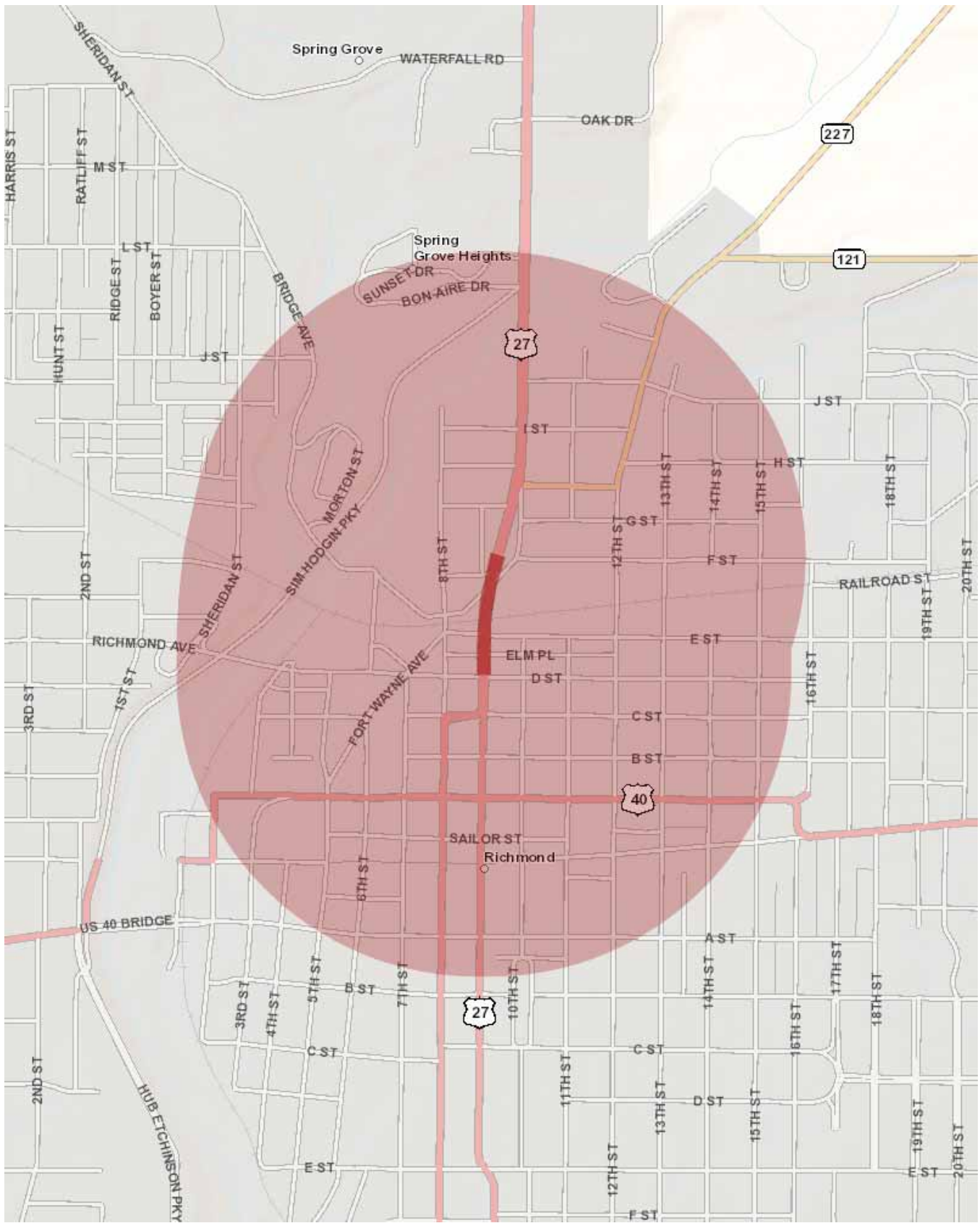
Address: 420 N. Walnut St., Bloomington, IN 47404

Email: IGSEnvir@indiana.edu

Phone: 812 855-7428

Date: January 15, 2020





## Metadata:

- [https://maps.indiana.edu/metadata/Geology/Seismic\\_Earthquake\\_Liquefaction\\_Potential.html](https://maps.indiana.edu/metadata/Geology/Seismic_Earthquake_Liquefaction_Potential.html)
- [https://maps.indiana.edu/metadata/Geology/Industrial\\_Minerals\\_Sand\\_Gravel\\_Resources.html](https://maps.indiana.edu/metadata/Geology/Industrial_Minerals_Sand_Gravel_Resources.html)
- [https://maps.indiana.edu/metadata/Hydrology/Floodplains\\_FIRM.html](https://maps.indiana.edu/metadata/Hydrology/Floodplains_FIRM.html)
- [https://maps.indiana.edu/metadata/Geology/Bedrock\\_Geology.html](https://maps.indiana.edu/metadata/Geology/Bedrock_Geology.html)



# Indiana Department of Environmental Management

*We Protect Hoosiers and Our Environment.*

100 North Senate Avenue - Indianapolis, IN 46204  
(800) 451-6027 - (317) 232-8603 - www.idem.IN.gov

INDOT

100 North Senate Avenue  
Indianapolis , IN 46204

Date

Parsons

Keaton Veldkamp

101 West Ohio St

Indianapolis , IN 46204

To Engineers and Consultants Proposing Roadway Construction Projects:

RE: The Indiana Department of Transportation (INDOT) is planning a bridge replacement project on US 27 over the Norfolk Southern Railroad (NSRR) and local streets in the City of Richmond, Wayne County, Indiana. The needs for this project are due to the deteriorated condition of the current structure, INDOT Structure No. 027-89-02136 B, and pedestrian facilities on the bridge that do not meet current standards. The purpose of the project is to extend the service life of the US 27 crossing over the NSRR and local streets by at least 50 years, and to reduce exposure to pedestrians by providing a pedestrian facility that meets current standards, including ADA. US 27 serves as a main north-south route for the downtown Richmond area, and the existing bridge is approximately 0.3 miles north of westbound US 40. In addition to crossing NSRR, the US 27 bridge crosses five local streets. Of those five, N. E Street and Fort Wayne Avenue are both major collectors; two are local roads, Elm Place and N. F Street; and the fifth is an alley completely covered by the existing bridge that connects Elm Place and N. E Street. The existing 17-span complex bridge carries two 12-foot wide travel lanes in each direction, with 4.5-foot wide sidewalks on each side. The US 27 bridge has undergone rehabilitation twice since it was built, including repairs in 1978 and 1992. The recommended alternative for this project would replace the existing structure with an 8 span bridge on the same alignment, with no anticipated grade change. The segment of N. F Street located at the north bridge abutment would be permanently closed to traffic and filled. The existing sidewalks on the bridge would be replaced with an ADA-compliant pedestrian facility on the west side of the bridge. The eastern sidewalk and staircases would be removed, while the northwestern staircase would remain. Guardrail would be replaced as-needed. Less than 0.5 acre of right-of-way acquisition is anticipated. During construction, the facility would be closed to traffic and a detour would be provided. The project area is urban and drained by city sewers. Therefore, no water resources are anticipated. All applicable permits will be applied for and acquired before construction can begin. Parsons will continue to work in coordination with the INDOT Ecology and Waterway Permitting Office to determine the presence and impacts to ecological resources (if any).

This letter from the Indiana Department of Environmental Management (IDEM) serves as a standardized response to enquiries inviting IDEM comments on roadway construction, reconstruction, or other improvement projects within existing roadway corridors when the proposed scope of the project is beneath the threshold requiring a formal National Environmental Policy Act-mandated Environmental Assessment or Environmental Impact Statement. As the letter attempts to address all roadway-related environmental topics of potential concern, it is possible that not every topic addressed in the letter will be applicable to your particular roadway project.



For additional information on specific roadway-related topics of interest, please visit the appropriate Web pages cited below, many of which provide contact information for persons within the various program areas who can answer questions not fully addressed in this letter. Also please be mindful that some environmental requirements may be subject to change and so each person intending to include a copy of this letter in their project documentation packet is advised to download the most recently revised version of the letter; found at: <http://www.in.gov/idem/5283.htm> (<http://www.in.gov/idem/5283.htm>).

To ensure that all environmentally-related issues are adequately addressed, IDEM recommends that you read this letter in its entirety, and consider each of the following issues as you move forward with the planning of your proposed roadway construction, reconstruction, or improvement project:

## WATER AND BIOTIC QUALITY

1. Section 404 of the Clean Water Act requires that you obtain a permit from the U.S. Army Corps of Engineers (USACE) before discharging dredged or fill materials into any wetlands or other waters, such as rivers, lakes, streams, and ditches. Other activities regulated include the relocation, channelization, widening, or other such alteration of a stream, and the mechanical clearing (use of heavy construction equipment) of wetlands. Thus, as a project owner or sponsor, it is your responsibility to ensure that no wetlands are disturbed without the proper permit. Although you may initially refer to the U.S. Fish and Wildlife Service National Wetland Inventory maps as a means of identifying potential areas of concern, please be mindful that those maps do not depict jurisdictional wetlands regulated by the USACE or the Department of Environmental Management. A valid jurisdictional wetlands determination can only be made by the USACE, using the 1987 Wetland Delineation Manual.

USACE recommends that you have a consultant check to determine whether your project will abut, or lie within, a wetland area. To view a list of consultants that have requested to be included on a list posted by the USACE on their Web site, see USACE Permits and Public Notices (<http://www.lrl.usace.army.mil/orf/default.asp>) (<http://www.lrl.usace.army.mil/orf/default.asp>) and then click on "Information" from the menu on the right-hand side of that page. Their "Consultant List" is the fourth entry down on the "Information" page. Please note that the USACE posts all consultants that request to appear on the list, and that inclusion of any particular consultant on the list does not represent an endorsement of that consultant by the USACE, or by IDEM.

Much of northern Indiana (Newton, Lake, Porter, LaPorte, St. Joseph, Elkhart, LaGrange, Steuben, and Dekalb counties; large portions of Jasper, Starke, Marshall, Noble, Allen, and Adams counties; and lesser portions of Benton, White, Pulaski, Kosciusko, and Wells counties) is served by the USACE District Office in Detroit (313-226-6812). The central and southern portions of the state (large portions of Benton, White, Pulaski, Kosciusko, and Wells counties; smaller portions of Jasper, Starke, Marshall, Noble, Allen, and Adams counties; and all other Indiana counties located in north-central, central, and southern Indiana) are served by the USACE Louisville District Office (502-315-6733).

Additional information on contacting these U.S. Army Corps of Engineers (USACE) District Offices, government agencies with jurisdiction over wetlands, and other water quality issues, can be found at <http://www.in.gov/idem/4396.htm> (<http://www.in.gov/idem/4396.htm>). IDEM recommends that impacts to wetlands and other water resources be avoided to the fullest extent.

2. In the event a Section 404 wetlands permit is required from the USACE, you also must obtain a Section 401 Water Quality Certification from the IDEM Office of Water Quality Wetlands Program. To learn more about the Wetlands Program, visit: <http://www.in.gov/idem/4384.htm> (<http://www.in.gov/idem/4384.htm>).



3. If the USACE determines that a wetland or other water body is isolated and not subject to Clean Water Act regulation, it is still regulated by the state of Indiana . A State Isolated Wetland permit from IDEM's Office of Water Quality (OWQ) is required for any activity that results in the discharge of dredged or fill materials into isolated wetlands. To learn more about isolated wetlands, contact the OWQ Wetlands Program at 317-233-8488.
4. If your project will involve over a 0.5 acre of wetland impact, stream relocation, or other large-scale alterations to water bodies such as the creation of a dam or a water diversion, you should seek additional input from the OWQ Wetlands Program staff. Consult the Web at: <http://www.in.gov/idem/4384.htm> (<http://www.in.gov/idem/4384.htm>) for the appropriate staff contact to further discuss your project.
5. Work within the one-hundred year floodway of a given water body is regulated by the Department of Natural Resources, Division of Water. The Division issues permits for activities regulated under the follow statutes:
  - IC 14-26-2 Lakes Preservation Act 312 IAC 11
  - IC 14-26-5 Lowering of Ten Acre Lakes Act No related code
  - IC 14-28-1 Flood Control Act 310 IAC 6-1
  - IC 14-29-1 Navigable Waterways Act 312 IAC 6
  - IC 14-29-3 Sand and Gravel Permits Act 312 IAC 6
  - IC 14-29-4 Construction of Channels Act No related code

For information on these Indiana (statutory) Code and Indiana Administrative Code citations, see the DNR Web site at: <http://www.in.gov/dnr/water/9451.htm> (<http://www.in.gov/dnr/water/9451.htm>) . Contact the DNR Division of Water at 317-232-4160 for further information.

The physical disturbance of the stream and riparian vegetation, especially large trees overhanging any affected water bodies should be limited to only that which is absolutely necessary to complete the project. The shade provided by the large overhanging trees helps maintain proper stream temperatures and dissolved oxygen for aquatic life.

6. For projects involving construction activity (which includes clearing, grading, excavation and other land disturbing activities) that result in the disturbance of one (1), or more, acres of total land area, contact the Office of Water Quality – Watershed Planning Branch (317/233-1864) regarding the need for of a Rule 5 Storm Water Runoff Permit. Visit the following Web page
  - <http://www.in.gov/idem/4902.htm> (<http://www.in.gov/idem/4902.htm>)

To obtain, and operate under, a Rule 5 permit you will first need to develop a Construction Plan (<http://www.in.gov/idem/4917.htm#constreq> (<http://www.in.gov/idem/4917.htm#constreq>)), and as described in 327 IAC 15-5-6.5 (<http://www.in.gov/legislative/iac/T03270/A00150> [PDF] (<http://www.in.gov/legislative/iac/T03270/A00150.PDF>), pages 16 through 19). Before you may apply for a Rule 5 Permit, or begin construction, you must submit your Construction Plan to your county Soil and Water Conservation District (SWCD) (<http://www.in.gov/isda/soil/contacts/map.html> (<http://www.in.gov/isda/soil/contacts/map.html>)).

Upon receipt of the construction plan, personnel of the SWCD or the Indiana Department of Environmental Management will review the plan to determine if it meets the requirements of 327 IAC 15-5. Plans that are deemed deficient will require re-submittal. If the plan is sufficient you will be notified and instructed to submit the verification to IDEM as part of the Rule 5 Notice of Intent (NOI) submittal. Once construction begins, staff of the SWCD or Indiana Department of Environmental Management will perform inspections of activities at the site for compliance with the regulation.

Please be mindful that approximately 149 Municipal Separate Storm Sewer System (MS4) areas are now being established by various local governmental entities throughout the state as part of the implementation of Phase II federal storm water requirements. All of these MS4 areas will eventually take responsibility for Construction Plan review, inspection, and enforcement. As these MS4 areas obtain program approval from IDEM, they will be added to a list of MS4 areas posted on the IDEM Website at: <http://www.in.gov/idem/4900.htm> (<http://www.in.gov/idem/4900.htm>).

If your project is located in an IDEM-approved MS4 area, please contact the local MS4 program about meeting their storm water requirements. Once the MS4 approves the plan, the NOI can be submitted to IDEM.

Regardless of the size of your project, or which agency you work with to meet storm water requirements, IDEM recommends that appropriate structures and techniques be utilized both during the construction phase, and after completion of the project, to minimize the impacts associated with storm water runoff. The use of appropriate planning and site development and appropriate storm water quality measures are recommended to prevent soil from leaving the construction site during active land disturbance and for post construction water quality concerns. Information and assistance regarding storm water related to construction activities are available from the Soil and Water Conservation District (SWCD) offices in each county or from IDEM.

7. For projects involving impacts to fish and botanical resources, contact the Department of Natural Resources - Division of Fish and Wildlife (317/232-4080) for addition project input.
8. For projects involving water main construction, water main extensions, and new public water supplies, contact the Office of Water Quality - Drinking Water Branch (317-308-3299) regarding the need for permits.
9. For projects involving effluent discharges to waters of the State of Indiana , contact the Office of Water Quality - Permits Branch (317-233-0468) regarding the need for a National Pollutant Discharge Elimination System (NPDES) permit.
10. For projects involving the construction of wastewater facilities and sewer lines, contact the Office of Water Quality - Permits Branch (317-232-8675) regarding the need for permits.

## AIR QUALITY

The above-noted project should be designed to minimize any impact on ambient air quality in, or near, the project area. The project must comply with all federal and state air pollution regulations. Consideration should be given to the following:

1. Regarding open burning, and disposing of organic debris generated by land clearing activities; some types of open burning are allowed (<http://www.in.gov/idem/4148.htm> (<http://www.in.gov/idem/4148.htm>)) under specific conditions. You also can seek an open burning variance from IDEM.

However, IDEM generally recommends that you take vegetative wastes to a registered yard waste composting facility or that the waste be chipped or shredded with composting on site (you must register with IDEM if more than 2,000 pounds is to be composted; contact 317/232-0066). The finished compost can then be used as a mulch or soil amendment. You also may bury any vegetative wastes (such as leaves, twigs, branches, limbs, tree trunks and stumps) onsite, although burying large quantities of such material can lead to subsidence problems, later on.

Reasonable precautions must be taken to minimize fugitive dust emissions from construction and demolition activities. For example, wetting the area with water, constructing wind barriers, or treating dusty areas with

chemical stabilizers (such as calcium chloride or several other commercial products). Dirt tracked onto paved roads from unpaved areas should be minimized.

Additionally, if construction or demolition is conducted in a wooded area where blackbirds have roosted or abandoned buildings or building sections in which pigeons or bats have roosted for 3-5 years precautionary measures should be taken to avoid an outbreak of histoplasmosis. This disease is caused by the fungus *Histoplasma capsulatum*, which stems from bird or bat droppings that have accumulated in one area for 3-5 years. The spores from this fungus become airborne when the area is disturbed and can cause infections over an entire community downwind of the site. The area should be wetted down prior to cleanup or demolition of the project site. For more detailed information on histoplasmosis prevention and control, please contact the Acute Disease Control Division of the Indiana State Department of Health at (317) 233-7272.

2. The U.S. EPA and the Surgeon General recommend that people not have long-term exposure to radon at levels above 4 pCi/L. (For a county-by-county map of predicted radon levels in Indiana, visit: <http://www.in.gov/idem/4145.htm> (<http://www.in.gov/idem/4145.htm>).

The U.S. EPA further recommends that all homes (and apartments within three stories of ground level) be tested for radon. If in-home radon levels are determined to be 4 pCi/L, or higher, EPA recommends a follow-up test. If the second test confirms that radon levels are 4 pCi/L, or higher, EPA recommends the installation of radon-reduction measures. (For a list of qualified radon testers and radon mitigation (or reduction) specialists visit: [http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon\\_testers\\_mitigators\\_list.pdf](http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon_testers_mitigators_list.pdf) ([http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon\\_testers\\_mitigators\\_list.pdf](http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon_testers_mitigators_list.pdf))). It also is recommended that radon reduction measures be built into all new homes, particularly in areas like Indiana that have moderate to high predicted radon levels.

To learn more about radon, radon risks, and ways to reduce exposure visit:

<http://www.in.gov/isdh/regsvcs/radhealth/radon.htm> (<http://www.in.gov/isdh/regsvcs/radhealth/radon.htm>), <http://www.in.gov/idem/4145.htm> (<http://www.in.gov/idem/4145.htm>), or <http://www.epa.gov/radon/index.html> (<http://www.epa.gov/radon/index.html>).

3. With respect to asbestos removal: all facilities slated for renovation or demolition (except residential buildings that have (4) four or fewer dwelling units and which will not be used for commercial purposes) must be inspected by an Indiana-licensed asbestos inspector prior to the commencement of any renovation or demolition activities. If regulated asbestos-containing material (RACM) that may become airborne is found, any subsequent demolition, renovation, or asbestos removal activities must be performed in accordance with the proper notification and emission control requirements.

If no asbestos is found where a renovation activity will occur, or if the renovation involves removal of less than 260 linear feet of RACM off of pipes, less than 160 square feet of RACM off of other facility components, or less than 35 cubic feet of RACM off of all facility components, the owner or operator of the project does not need to notify IDEM before beginning the renovation activity.

For questions on asbestos demolition and renovation activities, you can also call IDEM's Lead/Asbestos section at 1-888-574-8150.

However, in all cases where a demolition activity will occur (even if no asbestos is found), the owner or operator must still notify IDEM 10 working days prior to the demolition, using the form found at <http://www.in.gov/icpr/webfile/formsdiv/44593.pdf> (<http://www.in.gov/icpr/webfile/formsdiv/44593.pdf>).

Anyone submitting a renovation/demolition notification form will be billed a notification fee based upon the amount of friable asbestos containing material to be removed or demolished. Projects that involve the removal of more than 2,600 linear feet of friable asbestos containing materials on pipes, or 1,600 square feet or 400 cubic feet of friable asbestos containing material on other facility components, will be billed a fee of \$150 per project; projects below these amounts will be billed a fee of \$50 per project. All notification remitters will be billed on a quarterly basis.

For more information about IDEM policy regarding asbestos removal and disposal, visit: <http://www.in.gov/idem/4983.htm> (<http://www.in.gov/idem/4983.htm>).

4. With respect to lead-based paint removal: IDEM encourages all efforts to minimize human exposure to lead-based paint chips and dust. IDEM is particularly concerned that young children exposed to lead can suffer from learning disabilities. Although lead-based paint abatement efforts are not mandatory, any abatement that is conducted within housing built before January 1, 1978, or a child-occupied facility is required to comply with all lead-based paint work practice standards, licensing and notification requirements. For more information about lead-based paint removal visit: <http://www.in.gov/isdh/19131.htm> (<http://www.in.gov/isdh/19131.htm>).
5. Ensure that asphalt paving plants are permitted and operate properly. The use of cutback asphalt, or asphalt emulsion containing more than seven percent (7%) oil distillate, is prohibited during the months April through October. See 326 IAC 8-5-2, Asphalt Paving Rule (<http://www.ai.org/legislative/iac/T03260/A00080.PDF> (<http://www.ai.org/legislative/iac/T03260/A00080.PDF>)).
6. If your project involves the construction of a new source of air emissions or the modification of an existing source of air emissions or air pollution control equipment, it will need to be reviewed by the IDEM Office of Air Quality (OAQ). A registration or permit may be required under 326 IAC 2 (View at: [www.ai.org/legislative/iac/t03260/a00020.pdf](http://www.ai.org/legislative/iac/t03260/a00020.pdf) (<http://www.ai.org/legislative/iac/t03260/a00020.pdf>)). New sources that use or emit hazardous air pollutants may be subject to Section 112 of the Clean Air Act and corresponding state air regulations governing hazardous air pollutants.
7. For more information on air permits visit: <http://www.in.gov/idem/4223.htm> (<http://www.in.gov/idem/4223.htm>), or to initiate the IDEM air permitting process, please contact the Office of Air Quality Permit Reviewer of the Day at (317) 233-0178 or OAMPROD atdem.state.in.us.

## LAND QUALITY

In order to maintain compliance with all applicable laws regarding contamination and/or proper waste disposal, IDEM recommends that:

1. If the site is found to contain any areas used to dispose of solid or hazardous waste, you need to contact the Office of Land Quality (OLQ) at 317-308-3103.
2. All solid wastes generated by the project, or removed from the project site, need to be taken to a properly permitted solid waste processing or disposal facility. For more information, visit <http://www.in.gov/idem/4998.htm> (<http://www.in.gov/idem/4998.htm>).
3. If any contaminated soils are discovered during this project, they may be subject to disposal as hazardous waste. Please contact the OLQ at 317-308-3103 to obtain information on proper disposal procedures.
4. If PCBs are found at this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding management of any PCB wastes from this site.

5. If there are any asbestos disposal issues related to this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding the management of asbestos wastes (Asbestos removal is addressed above, under Air Quality).
6. If the project involves the installation or removal of an underground storage tank, or involves contamination from an underground storage tank, you must contact the IDEM Underground Storage Tank program at 317/308-3039. See: <http://www.in.gov/idem/4999.htm> (<http://www.in.gov/idem/4999.htm>).

## FINAL REMARKS

Should you need to obtain any environmental permits in association with this proposed project, please be mindful that IC 13-15-8 requires that you notify all adjoining property owners and/or occupants within ten days your submittal of each permit application. However, if you are seeking multiple permits, you can still meet the notification requirement with a single notice if all required permit applications are submitted with the same ten day period.

Should the scope of the proposed project be expanded to the extent that a National Environmental Policy Act Environmental Assessment (EA) or Environmental Impact Statement (EIS) is required, IDEM will actively participate in any early interagency coordination review of the project.

Meanwhile, please note that this letter does not constitute a permit, license, endorsement or any other form of approval on the part of the Indiana Department of Environmental Management regarding any project for which a copy of this letter is used. Also note that it is the responsibility of the project engineer or consultant using this letter to ensure that the most current draft of this document, which is located at <http://www.in.gov/idem/5284.htm> (<http://www.in.gov/idem/5284.htm>), is used.

---

## Signature(s) of the Applicant

I acknowledge that the following proposed roadway project will be financed in part, or in whole, by public monies.

## Project Description

The Indiana Department of Transportation (INDOT) is planning a bridge replacement project on US 27 over the Norfolk Southern Railroad (NSRR) and local streets in the City of Richmond, Wayne County, Indiana. The needs for this project are due to the deteriorated condition of the current structure, INDOT Structure No. 027-89-02136 B, and pedestrian facilities on the bridge that do not meet current standards. The purpose of the project is to extend the service life of the US 27 crossing over the NSRR and local streets by at least 50 years, and to reduce exposure to pedestrians by providing a pedestrian facility that meets current standards, including ADA. US 27 serves as a main north-south route for the downtown Richmond area, and the existing bridge is approximately 0.3 miles north of westbound US 40. In addition to crossing NSRR, the US 27 bridge crosses five local streets. Of those five, N. E Street and Fort Wayne Avenue are both major collectors; two are local roads, Elm Place and N. F Street; and the fifth is an alley completely covered by the existing bridge that connects Elm Place and N. E Street. The existing 17-span complex bridge carries two 12-foot wide travel lanes in each direction, with 4.5-foot wide sidewalks on each side. The US 27 bridge has undergone rehabilitation twice since it was built, including repairs in 1978 and 1992. The recommended alternative for this project would replace the existing structure with an 8 span bridge on the same alignment, with no anticipated grade change. The segment of N. F Street located at the north bridge abutment would be permanently closed to traffic and filled. The existing sidewalks on the bridge would be replaced with an ADA-compliant pedestrian facility on the west side of the bridge. The eastern sidewalk and



staircases would be removed, while the northwestern staircase would remain. Guardrail would be replaced as-needed. Less than 0.5 acre of right-of-way acquisition is anticipated. During construction, the facility would be closed to traffic and a detour would be provided. The project area is urban and drained by city sewers. Therefore, no water resources are anticipated. All applicable permits will be applied for and acquired before construction can begin. Parsons will continue to work in coordination with the INDOT Ecology and Waterway Permitting Office to determine the presence and impacts to ecological resources (if any).

With my signature, I do hereby affirm that I have read the letter from the Indiana Department of Environment that appears directly above. In addition, I understand that in order to complete that project in which I am interested, with a minimum of impact to the environment, I must consider all the issues addressed in the aforementioned letter, and further, that I must obtain any required permits.

Date: 3/9/2021

Signature of the INDOT  
Project Engineer or Other Responsible Agent \_\_\_\_\_



Date: 1/15/2020

Signature of the  
For Hire Consultant \_\_\_\_\_



Keaton Veldkamp



# United States Department of the Interior



## FISH AND WILDLIFE SERVICE

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

Phone: (812) 334-4261 Fax: (812) 334-4273

<http://www.fws.gov/midwest/Endangered/section7/s7process/step1.html>

In Reply Refer To:

March 18, 2020

Consultation Code: 03E12000-2020-SLI-0585

Event Code: 03E12000-2020-E-04900

Project Name: Des 1702941 US 27 over NSRR Bridge Replacement

Subject: Updated list of threatened and endangered species that may occur in your proposed project location, and/or may be affected by your proposed project

To Whom It May Concern:

The attached species list identifies any federally threatened, endangered, proposed and candidate species that may occur within the boundary of your proposed project or may be affected by your proposed project. The list also includes designated critical habitat if present within your proposed project area or affected by your project. This list is provided to you as the initial step of the consultation process required under section 7(c) of the Endangered Species Act, also referred to as Section 7 Consultation.

Section 7 of the Endangered Species Act of 1973 requires that actions authorized, funded, or carried out by Federal agencies not jeopardize federally threatened or endangered species or adversely modify designated critical habitat. To fulfill this mandate, Federal agencies (or their designated non-federal representative) must consult with the Service if they determine their project “may affect” listed species or critical habitat.

Under 50 CFR 402.12(e) (the regulations that implement Section 7 of the Endangered Species Act) the accuracy of this species list should be verified after 90 days. This verification can be completed formally or informally. You may verify the list by visiting the ECOS-IPaC website <http://ecos.fws.gov/ipac/> at regular intervals during project planning and implementation and completing the same process you used to receive the attached list. As an alternative, you may contact this Ecological Services Field Office for updates.

Please use the species list provided and visit the U.S. Fish and Wildlife Service's Region 3 Section 7 Technical Assistance website at - <http://www.fws.gov/midwest/endangered/section7/s7process/index.html>. This website contains step-by-step instructions which will help you

determine if your project will have an adverse effect on listed species and will help lead you through the Section 7 process.

For all **wind energy projects** and **projects that include installing towers that use guy wires or are over 200 feet in height**, please contact this field office directly for assistance, even if no federally listed plants, animals or critical habitat are present within your proposed project or may be affected by your proposed project.

Although no longer protected under the Endangered Species Act, be aware that bald eagles are protected under the Bald and Golden Eagle Protection Act (16 U.S.C. 668 *et seq.*) and Migratory Bird Treaty Act (16 U.S.C. 703 *et seq.*), as are golden eagles. Projects affecting these species may require measures to avoid harming eagles or may require a permit. If your project is near an eagle nest or winter roost area, see our Eagle Permits website at <http://www.fws.gov/midwest/midwestbird/EaglePermits/index.html> to help you determine if you can avoid impacting eagles or if a permit may be necessary.

We appreciate your concern for threatened and endangered species. Please include the Consultation Tracking Number in the header of this letter with any request for consultation or correspondence about your project that you submit to our office.

Attachment(s):

- Official Species List



# Official Species List

This list is provided pursuant to Section 7 of the Endangered Species Act, and fulfills the requirement for Federal agencies to "request of the Secretary of the Interior information whether any species which is listed or proposed to be listed may be present in the area of a proposed action".

This species list is provided by:

**Indiana Ecological Services Field Office**

620 South Walker Street

Bloomington, IN 47403-2121

(812) 334-4261

## Project Summary

Consultation Code: 03E12000-2020-SLI-0585

Event Code: 03E12000-2020-E-04900

Project Name: Des 1702941 US 27 over NSRR Bridge Replacement

Project Type: BRIDGE CONSTRUCTION / MAINTENANCE

**Project Description:** The Indiana Department of Transportation (INDOT) is planning a bridge replacement project on US 27 over the Norfolk Southern Railroad (NSRR) and local streets in the City of Richmond, Wayne County, Indiana. US 27 serves as a main north-south route for the downtown area, and the existing bridge is approximately 0.3 miles north of westbound US 40. In addition to crossing NSRR, the US 27 bridge crosses five local streets. Of those five, N. E Street and Fort Wayne Avenue are both major collectors; two are local roads, Elm Place and N. F Street; and the fifth is an alley that connects Elm Place and N. E Street (completely covered by the existing bridge).

The existing 17-span complex bridge carries two 12-foot wide travel lanes in each direction, with 3-foot wide sidewalks on each side. This structure was originally built in 1952 to provide a grade-separated route for vehicle and pedestrian connectivity to downtown Richmond over a busy railyard with ten sets of tracks. Currently, there are two sets of tracks that pass under the US 27 bridge. The US 27 bridge has undergone rehabilitation twice since it was built, including repairs in 1978 and 1992.

The recommended alternative for this project would replace the existing structure with an 8 span bridge on the same alignment, with no grade change. The segment of N. F Street located at the north bridge abutment would be permanently closed to traffic and filled. The existing sidewalks and pedestrian staircases would be replaced with an ADA-compliant pedestrian facility on the east side of the bridge. Guardrail would be replaced. During construction, the facility would be closed to traffic and a detour would be provided.

Work for this project may occur year-round starting in the winter of 2022. Project limits extend approximately from approximately 10 feet north to 10 feet south of the bridge. Right-of-way is pending; less than 0.5 acre is anticipated. The project area is urban. Suitable summer habitat exists approximately 800 feet northwest of the project area along the Whitewater River. No tree trimming or clearing is anticipated. Permanent lighting will be upgraded, and the contractor may use temporary lighting during



# Endangered Species Act Species

There is a total of 2 threatened, endangered, or candidate species on this species list.

Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species. Note that 1 of these species should be considered only under certain conditions.

IPaC does not display listed species or critical habitats under the sole jurisdiction of NOAA Fisheries<sup>1</sup>, as USFWS does not have the authority to speak on behalf of NOAA and the Department of Commerce.

See the "Critical habitats" section below for those critical habitats that lie wholly or partially within your project area under this office's jurisdiction. Please contact the designated FWS office if you have questions.

- 
1. [NOAA Fisheries](#), also known as the National Marine Fisheries Service (NMFS), is an office of the National Oceanic and Atmospheric Administration within the Department of Commerce.

## Mammals

NAME	STATUS
Indiana Bat <i>Myotis sodalis</i> There is <b>final</b> critical habitat for this species. Your location is outside the critical habitat. Species profile: <a href="https://ecos.fws.gov/ecp/species/5949">https://ecos.fws.gov/ecp/species/5949</a> Species survey guidelines: <a href="https://ecos.fws.gov/ipac/guideline/survey/population/1/office/31440.pdf">https://ecos.fws.gov/ipac/guideline/survey/population/1/office/31440.pdf</a>	Endangered
Northern Long-eared Bat <i>Myotis septentrionalis</i> No critical habitat has been designated for this species. This species only needs to be considered under the following conditions: <ul style="list-style-type: none"><li>▪ Incidental take of the NLEB is not prohibited here. Federal agencies may consult using the 4(d) rule streamlined process. Transportation projects may consult using the programmatic process. See <a href="http://www.fws.gov/midwest/endangered/mammals/nleb/index.html">www.fws.gov/midwest/endangered/mammals/nleb/index.html</a></li></ul> Species profile: <a href="https://ecos.fws.gov/ecp/species/9045">https://ecos.fws.gov/ecp/species/9045</a>	Threatened

## Critical habitats

THERE ARE NO CRITICAL HABITATS WITHIN YOUR PROJECT AREA UNDER THIS OFFICE'S JURISDICTION.



## United States Department of the Interior



### FISH AND WILDLIFE SERVICE

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Bloomington, IN 47403-2121

Phone: (812) 334-4261 Fax: (812) 334-4273

<http://www.fws.gov/midwest/Endangered/section7/s7process/step1.html>

In Reply Refer To:

March 18, 2020

Consultation Code: 03E12000-2020-I-0585

Event Code: 03E12000-2020-E-04913

Project Name: Des 1702941 US 27 over NSRR Bridge Replacement

Subject: Concurrence verification letter for the 'Des 1702941 US 27 over NSRR Bridge Replacement' project under the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

To whom it may concern:

The U.S. Fish and Wildlife Service (Service) has received your request to verify that the **Des 1702941 US 27 over NSRR Bridge Replacement** (Proposed Action) may rely on the concurrence provided in the February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat (PBO) to satisfy requirements under Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended; 16 U.S.C 1531 *et seq.*).

Based on the information you provided (Project Description shown below), you have determined that the Proposed Action is within the scope and adheres to the criteria of the PBO, including the adoption of applicable avoidance and minimization measures, and may affect, but is not likely to adversely affect (NLAA) the endangered Indiana bat (*Myotis sodalis*) and/or the threatened Northern long-eared bat (*Myotis septentrionalis*).

The Service has 14 calendar days to notify the lead Federal action agency or designated non-federal representative if we determine that the Proposed Action does not meet the criteria for a NLAA determination under the PBO. If we do not notify the lead Federal action agency or designated non-federal representative within that timeframe, you may proceed with the Proposed Action under the terms of the NLAA concurrence provided in the PBO. This verification period allows Service Field Offices to apply local knowledge to implementation of the PBO, as we may identify a small subset of actions having impacts that were unanticipated. In such instances, Service Field Offices may request additional information that is necessary to verify inclusion of the proposed action under the PBO.

**For Proposed Actions that include bridge/structure removal, replacement, and/or maintenance activities:** If your initial bridge/structure assessments failed to detect Indiana bats, but you later detect bats during construction, please submit the Post Assessment Discovery of Bats at Bridge/Structure Form (User Guide Appendix E) to this Service Office. In these instances, potential incidental take of Indiana bats may be exempted provided that the take is reported to the Service.

If the Proposed Action is modified, or new information reveals that it may affect the Indiana bat and/or Northern long-eared bat in a manner or to an extent not considered in the PBO, further review to conclude the requirements of ESA Section 7(a)(2) may be required. If the Proposed Action may affect any other federally-listed or proposed species, and/or any designated critical habitat, additional consultation between the lead Federal action agency and this Service Office is required. If the proposed action has the potential to take bald or golden eagles, additional coordination with the Service under the Bald and Golden Eagle Protection Act may also be required. In either of these circumstances, please contact this Service Office.

## **Project Description**

The following project name and description was collected in IPaC as part of the endangered species review process.

### **Name**

Des 1702941 US 27 over NSRR Bridge Replacement

### **Description**



The Indiana Department of Transportation (INDOT) is planning a bridge replacement project on US 27 over the Norfolk Southern Railroad (NSRR) and local streets in the City of Richmond, Wayne County, Indiana. US 27 serves as a main north-south route for the downtown area, and the existing bridge is approximately 0.3 miles north of westbound US 40. In addition to crossing NSRR, the US 27 bridge crosses five local streets. Of those five, N. E Street and Fort Wayne Avenue are both major collectors; two are local roads, Elm Place and N. F Street; and the fifth is an alley that connects Elm Place and N. E Street (completely covered by the existing bridge).

The existing 17-span complex bridge carries two 12-foot wide travel lanes in each direction, with 3-foot wide sidewalks on each side. This structure was originally built in 1952 to provide a grade-separated route for vehicle and pedestrian connectivity to downtown Richmond over a busy railyard with ten sets of tracks. Currently, there are two sets of tracks that pass under the US 27 bridge. The US 27 bridge has undergone rehabilitation twice since it was built, including repairs in 1978 and 1992.

The recommended alternative for this project would replace the existing structure with an 8 span bridge on the same alignment, with no grade change. The segment of N. F Street located at the north bridge abutment would be permanently closed to traffic and filled. The existing sidewalks and pedestrian staircases would be replaced with an ADA-compliant pedestrian facility on the east side of the bridge. Guardrail would be replaced. During construction, the facility would be closed to traffic and a detour would be provided.

Work for this project may occur year-round starting in the winter of 2022. Project limits extend approximately from approximately 10 feet north to 10 feet south of the bridge. Right-of-way is pending; less than 0.5 acre is anticipated. The project area is urban. Suitable summer habitat exists approximately 800 feet northwest of the project area along the Whitewater River. No tree trimming or clearing is anticipated. Permanent lighting will be upgraded, and the contractor may use temporary lighting during construction.

A review of the USFWS GIS database for Indiana bat and Northern long-eared bat roosting, hibernacula and capture sites was conducted for Des 1702941 on April 25, 2019. There are no documented sites within a half mile the project area. The US 27 bridge was inspected for evidence of bats on March 12, 2020, and no evidence was reported.

# Determination Key Result

Based on your answers provided, this project(s) may affect, but is not likely to adversely affect the endangered Indiana bat and/or the threatened Northern long-eared bat, therefore, consultation with the U.S. Fish and Wildlife Service pursuant to Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended 16 U.S.C. 1531 *et seq.*) is required. However, also based on your answers provided, this project may rely on the concurrence provided in the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

## Qualification Interview

1. Is the project within the range of the Indiana bat<sup>[1]</sup>?

[1] See [Indiana bat species profile](#)

**Automatically answered**

Yes

2. Is the project within the range of the Northern long-eared bat<sup>[1]</sup>?

[1] See [Northern long-eared bat species profile](#)

**Automatically answered**

Yes

3. Which Federal Agency is the lead for the action?

A) *Federal Highway Administration (FHWA)*

4. Are *all* project activities limited to non-construction<sup>[1]</sup> activities only? (examples of non-construction activities include: bridge/abandoned structure assessments, surveys, planning and technical studies, property inspections, and property sales)

[1] Construction refers to activities involving ground disturbance, percussive noise, and/or lighting.

No

5. Does the project include *any* activities that are **greater than** 300 feet from existing road/rail surfaces<sup>[1]</sup>?

[1] Road surface is defined as the actively used [e.g. motorized vehicles] driving surface and shoulders [may be pavement, gravel, etc.] and rail surface is defined as the edge of the actively used rail ballast.

No

6. Does the project include *any* activities **within** 0.5 miles of a known Indiana bat and/or NLEB hibernaculum<sup>[1]</sup>?

[1] For the purpose of this consultation, a hibernaculum is a site, most often a cave or mine, where bats hibernate during the winter (see suitable habitat), but could also include bridges and structures if bats are found to be hibernating there during the winter.

*No*

7. Is the project located **within** a karst area?

*No*

8. Is there *any* suitable<sup>[1]</sup> summer habitat for Indiana Bat or NLEB **within** the project action area<sup>[2]</sup>? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] The action area is defined as all areas to be affected directly or indirectly by the Federal action and not merely the immediate area involved in the action (50 CFR Section 402.02). Further clarification is provided by the [national consultation FAQs](#).

*No*

9. Does the project include wetland or stream protection activities associated with compensatory wetland mitigation?

*No*

10. Does the project include slash pile burning?

*No*

11. Does the project include *any* bridge removal, replacement, and/or maintenance activities (e.g., any bridge repair, retrofit, maintenance, and/or rehabilitation work)?

*Yes*

12. Is there *any* suitable habitat<sup>[1]</sup> for Indiana bat or NLEB **within** 1,000 feet of the bridge? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's current [summer survey guidance](#) for our current definitions of suitable habitat.

*Yes*

13. Has a bridge assessment<sup>[1]</sup> been conducted **within** the last 24 months<sup>[2]</sup> to determine if the bridge is being used by bats?

[1] See [User Guide Appendix D](#) for bridge/structure assessment guidance

[2] Assessments must be completed no more than 2 years prior to conducting any work below the deck surface on all bridges that meet the physical characteristics described in the Programmatic Consultation, regardless of whether assessments have been conducted in the past. Due to the transitory nature of bat use, a negative result in one year does not guarantee that bats will not use that bridge/structure in subsequent years.

Yes

**SUBMITTED DOCUMENTS**

- *US 27 Bridge Inspection Form 3.12.20.pdf* <https://ecos.fws.gov/ipac/project/CXP4DXYM4FCHJOI3J4DOTOJO5Q/projectDocuments/20797268>

14. Did the bridge assessment detect *any* signs of Indiana bats and/or NLEBs roosting in/under the bridge (bats, guano, etc.)<sup>[1]</sup>?

[1] If bridge assessment detects signs of *any* species of bats, coordination with the local FWS office is needed to identify potential threatened or endangered bat species. Additional studies may be undertaken to try to identify which bat species may be utilizing the bridge prior to allowing *any* work to proceed.

Note: There is a small chance bridge assessments for bat occupancy do not detect bats. Should a small number of bats be observed roosting on a bridge just prior to or during construction, such that take is likely to occur or does occur in the form of harassment, injury or death, the PBO requires the action agency to report the take. Report all unanticipated take within 2 working days of the incident to the USFWS. Construction activities may continue without delay provided the take is reported to the USFWS and is limited to 5 bats per project.

No

15. Will the bridge removal, replacement, and/or maintenance activities include installing new or replacing existing **permanent** lighting?

Yes

16. Does the project include the removal, replacement, and/or maintenance of *any* structure other than a bridge? (e.g., rest areas, offices, sheds, outbuildings, barns, parking garages, etc.)

No

17. Will the project involve the use of **temporary** lighting *during* the active season?

Yes

18. Is there *any* suitable habitat **within** 1,000 feet of the location(s) where **temporary** lighting will be used?

*Yes*

19. Will the project install *any* new or replace any existing **permanent** lighting in addition to the lighting already indicated for habitat removal (including the removal or trimming of trees) or bridge/structure removal, replacement or maintenance activities?

*No*

20. Does the project include percussives or other activities (**not including tree removal/trimming or bridge/structure work**) that will increase noise levels above existing traffic/background levels?

*No*

21. Are *all* project activities that are **not associated with** habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives, limited to actions that DO NOT cause any additional stressors to the bat species?

Examples: lining roadways, unlighted signage , rail road crossing signals, signal lighting, and minor road repair such as asphalt fill of potholes, etc.

*Yes*

22. Will the project raise the road profile **above the tree canopy**?

*No*

23. Is the location of this project consistent with a No Effect determination in this key?

**Automatically answered**

*Yes, because the project action area is not within suitable Indiana bat and/or NLEB summer habitat and is outside of 0.5 miles of a hibernaculum.*

24. Is the bridge removal, replacement, or maintenance activities portion of this project consistent with a No Effect determination in this key?

**Automatically answered**

*Yes, because the bridge has been assessed using the criteria documented in the BA and no signs of bats were detected*



25. **General AMM 1**

Will the project ensure *all* operators, employees, and contractors working in areas of known or presumed bat habitat are aware of *all* FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable Avoidance and Minimization Measures?

Yes

26. **Lighting AMM 1**

Will *all* **temporary** lighting be directed away from suitable habitat during the active season?

Yes

## Project Questionnaire

1. Have you made a No Effect determination for *all* other species indicated on the FWS IPaC generated species list?

N/A

2. Have you made a May Affect determination for *any* other species on the FWS IPaC generated species list?

N/A

3. Please describe the proposed bridge work:

*The recommended alternative for this project would replace the existing structure with an 8 span bridge on the same alignment, with no grade change. The segment of N. F Street located at the north bridge abutment would be permanently closed to traffic and filled. The existing sidewalks and pedestrian staircases would be replaced with an ADA-compliant pedestrian facility on the east side of the bridge. Guardrail would be replaced. During construction, the facility would be closed to traffic and a detour would be provided.*

*Work for this project may occur year-round starting in the winter of 2022. Project limits extend approximately from approximately 10 feet north to 10 feet south of the bridge. Right-of-way is pending; less than 0.5 acre is anticipated. The project area is urban. Suitable summer habitat exists approximately 800 feet northwest of the project area along the Whitewater River. No tree trimming or clearing is anticipated. Permanent lighting will be upgraded, and the contractor may use temporary lighting during construction.*

4. Please state the timing of all proposed bridge work:

*Year-round starting in the winter of 2022*

5. Please enter the date of the bridge assessment:

*March 12, 2020*

## **Avoidance And Minimization Measures (AMMs)**

This determination key result includes the commitment to implement the following Avoidance and Minimization Measures (AMMs):

### **GENERAL AMM 1**

Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs.

### **LIGHTING AMM 1**

Direct temporary lighting away from suitable habitat during the active season.

## **Determination Key Description: FHWA, FRA, FTA Programmatic Consultation For Transportation Projects Affecting NLEB Or Indiana Bat**

This key was last updated in IPaC on December 02, 2019. Keys are subject to periodic revision.

This decision key is intended for projects/activities funded or authorized by the Federal Highway Administration (FHWA), Federal Railroad Administration (FRA), and/or Federal Transit Administration (FTA), which may require consultation with the U.S. Fish and Wildlife Service (Service) under Section 7 of the Endangered Species Act (ESA) for the endangered **Indiana bat** (*Myotis sodalis*) and the threatened **Northern long-eared bat** (NLEB) (*Myotis septentrionalis*).

This decision key should only be used to verify project applicability with the Service's [February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects](#). The programmatic biological opinion covers limited transportation activities that may affect either bat species, and addresses situations that are both likely and not likely to adversely affect either bat species. This decision key will assist in identifying the effect of a specific project/activity and applicability of the programmatic consultation. The programmatic biological opinion is not intended to cover all types of transportation actions. Activities outside the scope of the programmatic biological opinion, or that may affect ESA-listed species other than the Indiana bat or NLEB, or any designated critical habitat, may require additional ESA Section 7 consultation.

## APPENDIX D: Bridge/Structure Assessment Form

This form will be completed and submitted to the District Environmental Manager by the Contractor prior to conducting any work below the deck surface either from the underside; from activities above that bore down to the underside; from activities that could impact expansion joints; from deck removal on bridges; or from structure demolition for bridges/structures within 1000 feet of suitable bat habitat.

DOT Project #	Water Body	Date/Time of Inspection	Within 1,000ft of suitable bat habitat (circle one)
1702941	N/A	March 12, 2020	<input checked="" type="radio"/> Yes <input type="radio"/> No

Route	County	Federal Structure ID
US 27	Wayne	027-89-02136 B

**If the bridge/structure is 1,000 feet or more from suitable bat habitat (e.g., an urban or agricultural area without suitable foraging habitat or corridors linking the bridge to suitable foraging habitat), check box and STOP HERE. No assessment required.**

**Please submit to the U.S. Fish and Wildlife Service.**

Areas Inspected (Check all that apply) **\*As safely feasible**

Bridges		Culverts/Other Structures		Summary Info (circle all that apply)			
All vertical crevices sealed at the top and 0.5-1.25" wide & ≥4" deep	✓	Crevices, rough surfaces or imperfections in concrete		Human disturbance or traffic under bridge/in culvert or at the structure	<input checked="" type="radio"/> High	<input type="radio"/> Low	<input type="radio"/> None
All crevices >12" deep & not sealed	✓	Spaces between walls, ceiling joists		Possible corridors for netting	<input type="radio"/> None/poor	<input checked="" type="radio"/> Marginal	<input type="radio"/> Excellent
All guardrails	✓						
All expansion joints	✓						
Spaces between concrete end walls and the bridge deck	✓						

Last Revised May 31, 2017

Vertical surfaces on concrete I-beams	✓						
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**Evidence of Bats (Circle all that apply)** Presence of one or more indicators is sufficient evidence that bats may be using the structure.

None

Visual (e.g. survey, thermal, emergent etc.)

- Live \_\_ number seen
- Dead \_\_ number seen

Photo documentation Y/N

Audible

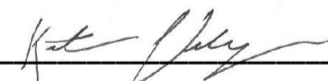
Guano

Odor Y/N

Photo documentation Y/N

Staining definitively from bats

Photo documentation Y/N

<b>Assessment Conducted By:</b> <u>Keaton Veldkamp</u>	<b>Signature(s):</b> 
<b>District Environmental Use Only:</b> Date Received by District Environmental Manager: _____	

### DOT Bat Assessment Form Instructions

1. Assessments must be completed no more than 2 years prior to conducting any work below the deck surface on all bridges, regardless of whether assessments have been conducted in the past.
2. Any bridge/structure suspected of providing habitat for any species of bat will be removed from work schedules until such time that the DOT has coordinated with the USFWS. Additional studies may be undertaken by the DOT to determine what species may be utilizing each structure identified as supporting bats prior to allowing any work to proceed.
3. Any questions should be directed to the District Environmental Manager.



**Photo 1** – View of the US 27 bridge facing south (3/12/2020).



**Photo 2** – View of the US 27 bridge facing south (3/12/2020).



**Photo 3** – View of the US 27 bridge facing north (3/12/2020).



**Photo 4** – View of the US 27 bridge facing northeast (3/12/2020).



# Bridge Inspection Report

027-89-02136 B  
US 27  
over  
N&S RR, 5 STREETS



Inspection Date: 04/21/2020

Inspected By: James F. Mickler

Inspection Type(s): Routine

Approach slabs: wide longitudinal cracks - 60' North, 80' South; patching at joints - 24' South & 43' North.

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**Paint:** \* Indicate if paint present , year painted & condition rating.

1 - Steel Beams	6 - Satisfactory Condition – minor areas of rust and chalking	1993
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Comments:

Paint: minor general corrosion to bottom flanges (40% in span E, 10% in span F, 5% in span G (over RR) & 5% in span M); 12 SF heavy corrosion at end of 3 beams over Bent #6; very minor corrosion in span N.

Paint Color: Light Blue. Contract #: B-19597.

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**Scour Analysis:** N/A      **Scour Critical:** N/A      **Scour POA?**

NBI 113 Scour Comment:

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**Endangered Species:** \* If yes, add one photo to the dropdown field

Bats: seen or heard under structure? \*

N - No evidence of bats

Birds/swallows/nests seen? Empty nests present? \*

Y - Birds and/or Nests Visible

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**BRIDGE Culvert Geometry:**

Barrel Length:

Height:

Width:

**INDOT Bridge/Small Structure Bat Inspection Data Sheet (Rev 4/29/2016)**

General Information		
Date of Inspection: 5/8/2021 Time of Inspection: 11:02 am	Initial Inspection <input type="checkbox"/> Follow-up Inspection <input checked="" type="checkbox"/> Construction <input type="checkbox"/>	Temp: 52 F Wind: Light Precip: None
County: Wayne	Inspected by: Juliet Port, Parsons	Sunrise: 6:34 am Sunset: 8:48 pm
GPS Northing: 39 50' 15" N Easting: 84 15' 26" W UTM Zone: 16	Contract Number: B-41510	Anticipated Start Date for Construction: 2023

Bridge or Culvert	Bridge or Culvert
Stream or Road Crossed: NS Railroad & Local Streets	Station: 18+74.87
Bridge/Culvert number: 027-89-02136 B	Number of Spans: 17
Type of Structure: <input checked="" type="checkbox"/> Concrete box beam <input checked="" type="checkbox"/> Steel beam <input type="checkbox"/> Concrete I-beam <input type="checkbox"/> Steel girder <input type="checkbox"/> Concrete bulb tee beam <input type="checkbox"/> Steel pony truss <input type="checkbox"/> Concrete arch <input type="checkbox"/> Welded steel thru girder <input type="checkbox"/> Concrete girder <input type="checkbox"/> Concrete box culvert <input type="checkbox"/> Concrete slab <input type="checkbox"/> Concrete pipe <input type="checkbox"/> Multi-plate arch <input type="checkbox"/> Corrugated steel pipe <input checked="" type="checkbox"/> Other (list): Complex hybrid	Material: <input checked="" type="checkbox"/> Concrete <input checked="" type="checkbox"/> Steel <input type="checkbox"/> Other (describe):  Shape: <input type="checkbox"/> Box Culvert <input type="checkbox"/> Pipe <input type="checkbox"/> Arch <input type="checkbox"/> Slab <input checked="" type="checkbox"/> Other (describe) Complex bridge
Searched entire structure? If not, why not? As safely feasible from ground surface. Did not access RR property.	Location of bats or signs of use (w/drawing and photos):
Bats Present? <input type="checkbox"/> Seen? <input type="checkbox"/> Heard?	Observed pigeons and nests in several locations north of the RR, primarily along Ft. Wayne Avenue.
In Clusters? Number of clusters:	
Number of bats in largest cluster:	
Approximate total number of bats found:	
Signs of previous bat use? <input type="checkbox"/> Guano <input type="checkbox"/> Staining	

If Bats Present
Date and Time Project Supervisor was notified:
Name of Project Supervisor notified:



**Photo 1** – View under the US 27 bridge facing southwest along Ft. Wayne Avenue (05/08/2021).



**Photo 2** – View of a pigeon nesting on the US 27 bridge over North D Street (5/08/2021).



**Photo 3** – View of several pigeons on the US 27 bridge on the north side of Ft. Wayne Avenue (05/08/2021).



**Photo 4** – View of pigeons on the US 27 bridge on the south side of Ft. Wayne Avenue (05/08/2021).