

Indiana Department of Transportation

County Jefferson Route SR 62 Des. No. 1701457

**FHWA-Indiana Environmental Document
CATEGORICAL EXCLUSION / ENVIRONMENTAL ASSESSMENT FORM
GENERAL PROJECT INFORMATION**

Road No./County:	State Route (SR) 62/Jefferson County
Designation Number:	1701457
Project Description/Termini:	Bridge Replacement, 0.35 mi. east of SR 250 Termini: 450 ft. south of structure to 750 ft. north of structure

After completing this form, I conclude that this project qualifies for the following type of Categorical Exclusion (FHWA must review/approve if Level 4 CE):

X	Categorical Exclusion, Level 2 – The proposed action meets the criteria for Categorical Exclusion Manual Level 2 - table 1, CE Level Thresholds. Required Signatories: ESM (Environmental Scoping Manager)
	Categorical Exclusion, Level 3 – The proposed action meets the criteria for Categorical Exclusion Manual Level 3 - table 1, CE Level Thresholds. Required Signatories: ESM, ES (Environmental Services Division)
	Categorical Exclusion, Level 4 – The proposed action meets the criteria for Categorical Exclusion Manual Level 4 - table 1, CE Level Thresholds. Required Signatories: ESM, ES, FHWA
	Environmental Assessment (EA) – EAs require a separate FONSI. Additional research and documentation is necessary to determine the effects on the environment. Required Signatories: ES, FHWA

Note: For documents prepared by or for Environmental Services Division, it is not necessary for the ESM of the district in which the project is located to release for public involvement or sign for approval.

Approval _____
 ESM Signature _____ Date _____ ES Signature _____ Date _____

 FHWA Signature _____ Date _____

Release for Public Involvement

DGD _____ 2021.04.01 13:46:34 -04'00'
 ESM Initials _____ Date _____ ES Initials _____ Date _____

Certification of Public Involvement _____
 Office of Public Involvement _____ Date _____

Note: Do not approve until after Section 106 public involvement and all other environmental requirements have been satisfied.

INDOT ES/District Env.
 Reviewer Signature: _____ Date: _____

Name and Organization of CE/EA Preparer: Mathew Aldridge/Burgess & Niple, Inc.

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Part I - PUBLIC INVOLVEMENT

Every Federal action requires some level of public involvement, providing for early and continuous opportunities throughout the project development process. The level of public involvement should be commensurate with the proposed action.

Does the project have a historic bridge processed under the Historic Bridges PA*? Yes No
If No, then: Opportunity for a Public Hearing Required? Yes No

*A public hearing is required for all historic bridges processed under the Historic Bridges Programmatic Agreement between INDOT, FHWA, SHPO, and the ACHP.

Discuss what public involvement activities (legal notices, letters to affected property owners and residents (i.e. notice of entry), meetings, special purpose meetings, newspaper articles, etc.) have occurred for this project.

Remarks: Notice of Entry letters were mailed to potentially affected property owners near the project area on March 15, 2018... Project Does Meet The project will meet the minimum requirements described in the current Indiana Department of Transportation (INDOT) Public Involvement Manual...

Public Controversy on Environmental Grounds Will the project involve substantial controversy concerning community and/or natural resource impacts? Yes No

Remarks: No controversy At this time, there is no substantial public controversy concerning impacts to the community or to natural resources.

Part II - General Project Identification, Description, and Design Information

Sponsor of the Project: Indiana Department of Transportation (INDOT) INDOT District: Seymour
Local Name of the Facility: SR 62

Funding Source (mark all that apply): Federal [X] State [X] Local [] Other* []

*If other is selected, please identify the funding source:

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PURPOSE AND NEED:

Describe the transportation problem that the project will address. The solution to the traffic problem should NOT be discussed in this section. (Refer to the CE Manual, Section IV.B.2. Purpose and Need)

Need

The primary need for the replacement and realignment is based on the structure's condition, deterioration, and nonstandard design features. The bridge is on a skew that results in a substandard curve on the east approach to the bridge. At the east end of the bridge, North Copeland Ridge Road intersects with SR 62 from the north in the middle of the before mentioned curve. The posted speed of SR 62 at the location of the bridge is 45 m.p.h.; however, the substandard geometrics of the roadway makes the current condition only adequate for 20 m.p.h. Crash records were obtained from INDOT for the most recent 7-year period (2010-2017) along this stretch of SR 62. There were 5 vehicular crashes, four were cars running off the road and one was a sideswipe. There were three injuries and no fatalities.

Purpose

The purpose of the project is to provide safe transportation over Toddy's Branch with a bridge and roadway that meet current design standards such as alignment, superelevation, site distance, and bridge width before operational function and safety of the public are compromised.

PROJECT DESCRIPTION (PREFERRED ALTERNATIVE):

County: Jefferson Municipality: N/A

Limits of Proposed Work: Approximately 450 ft. south of Structure 062-39-10270 and 750 ft. north of the structure.

Total Work Length: 0.23 Mile(s) Total Work Area: 3.813 Acre(s)

Is an Interchange Modification Study / Interchange Justification Study (IMS/IJS) required?
If yes, when did the FHWA grant a conditional approval for this project?

Table with Yes/No columns and a Date field. 'No' is checked.

1If an IMS or IJS is required; a copy of the approved CE/EA document must be submitted to the FHWA with a request for final approval of the IMS/IJS.

In the remarks box below, describe existing conditions, provide in detail the scope of work for the project, including the preferred alternative. Include a discussion of logical termini. Discuss any major issues for the project and how the project will improve safety or roadway deficiencies if these are issue.

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Location

The structure carrying SR 62 over Toddy’s Branch is located 0.35 mi east of SR 250 in Shelby Township, Jefferson County, Indiana. The project is located within Section 18, Township 5N, Range 11E. The center point of the project is located at 38.847339, -85.348678 (Appendix B, page B-2).

Existing Conditions

The existing structure carries two lanes of SR 62 over Toddy’s Branch. It is classified as a Rural Major Collector with rolling terrain. The existing structure is a composite prestressed concrete box beam bridge with one span that is 59 ft. 10 in. long and a width of 31 ft. 5 in. The existing approach has two 10 ft. travel lanes with up to 2 ft. aggregate shoulders. The asphalt pavement flares to a width of 15 ft. 8 in. near the bridge. The useable shoulder width is approximately 3 ft. The existing approach roadway has a very sharp horizontal curve that begins at the north end of the bridge and a sharp vertical grade change at the south end of the bridge. There is an intersection of SR 62 with North Copeland Ridge Road immediately off the northwest corner of the bridge. The design speed has a posted legal speed limit of 45 m.p.h. This horizontal curve at the north end of the bridge has an advisory sign of 20 m.p.h. Guardrail is present on all four approaches with nonstandard terminal assemblies. The most recent Structure Inventory and Appraisal Report, dated March 8, 2016, noted cracking, efflorescence, spalling, and exposed rebar. The adjacent land use is agriculture north of the bridge along SR 62 and vacant land and residential property south of the bridge.

Crashes within the project area are attributed to the current roadway and bridge design such as alignment, superelevation, site distance, and the bridge width.

Preferred Alternative

This alternate involves replacing the bridge on a new alignment and profile. Placing the bridge on a new alignment will provide a bridge with none of the deficiencies of the existing structure. The proposed profile will provide the desired 2 ft. of freeboard above the Q100 water surface elevation. The proposed roadway section will consist of two 12 ft. lanes and 3 ft. 4 in. shoulders paved to the face of new guardrail. North Copeland Ridge Road will be relocated to fit the newly aligned SR 62. With the new vertical and horizontal alignment the new bridge and approach roadway will meet all current design standards. The project impacts will only include what is necessary to replace the existing bridge and provide hydraulic support for that bridge. This project is not dependent on any other project to be constructed. The project plans are included in Appendix B, page B-12.

Maintenance of traffic (MOT) will include using the existing bridge and roadway as long as possible while the new bridge is constructed on the new alignment. A detour will be used when the road is closed to complete construction. The duration of the closure is approximately 3 months. The detour length is about 36 miles, and utilizes SR 250, US 421, and SR 129. MOT is covered in detail in the MOT section below.

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OTHER ALTERNATIVES CONSIDERED:

Describe all discarded alternatives, including the Do-Nothing Alternative and an explanation of why each discarded alternative was not selected.

No-Build Alternative:

While doing nothing is possible, this alternate does not address the deficiencies present with the current structure and roadway geometrics and does not satisfy the purpose and need, therefore was removed from consideration.

Structure Replacement not Meeting Level One Design Criteria (Existing Alignment):

This alternate involves replacing the bridge on the existing horizontal alignment and vertical profile. Traffic would be detoured, and design exceptions would be used for violations of level 1 criteria for horizontal curvature, horizontal sight distance, vertical sight distance and vertical profile. This would replace the structure at less cost but continue the current substandard design features. This alternate will only address the structure deficiencies present in the current structure, not satisfying the purpose and need, therefore replacing the bridge with Design Exception Alternative #2 is not recommended.

Replace Bridge with Steel Beam Superstructure and meet Level 1 Design Criteria (New Alignment):

This alternate involves replacing the bridge on a new alignment and profile. There would be no design exceptions required for violations of level 1 criteria. Placing the bridge on a new alignment will remove all deficiencies present in the current structure. The profile for this alternate would be 9 in. lower than the preferred alternative using deeper concrete beams. The steel beams are higher cost than the preferred alternative concrete box beams. While satisfying the purpose and need of the project, this alternative is less cost effective than the preferred alternative, therefore this alternative was removed from consideration.

The Do Nothing Alternative is not feasible, prudent or practicable because (Mark all that apply):

It would not correct existing capacity deficiencies;

It would not correct existing safety hazards;

It would not correct the existing roadway geometric deficiencies;

It would not correct existing deteriorated conditions and maintenance problems; or

It would result in serious impacts to the motoring public and general welfare of the economy.

Other (Describe)

X
X
X

ROADWAY CHARACTER:

Functional Classification:	Rural Major Collector		
Current ADT:	<u>1,242</u>	VPD (2022)	<u>1,415</u>
Design Hour Volume (DHV):	<u>100</u>	Truck Percentage (%)	<u>16.47</u>
Designed Speed (mph):	<u>45</u>	Legal Speed (mph):	<u>45</u>

Existing **Proposed**

Number of Lanes:	2	2
Type of Lanes:	Paved	Paved
Pavement Width:	13 ft.	15-16 ft.
Shoulder Width:	3 ft.	3-4 ft.
Median Width:	0 ft.	0 ft.
Sidewalk Width:	N/A ft.	N/A ft.

Setting:	<input type="checkbox"/> Urban	<input type="checkbox"/> Suburban	<input checked="" type="checkbox"/> Rural
Topography:	<input type="checkbox"/> Level	<input checked="" type="checkbox"/> Rolling	<input type="checkbox"/> Hilly

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If the proposed action has multiple roadways, this section should be filled out for each roadway.

DESIGN CRITERIA FOR BRIDGES:

Structure/NBI Number(s): 062-039-05946 B/22460 Sufficiency Rating: 80.1 - 3/8/2016 Bridge Inspection Report
 (Rating, Source of Information)

	Existing	Proposed	
Bridge Type:	Composite Prestressed Concrete Box Beam	Concrete AASHTO Type II Beams	
Number of Spans:	1	1	
Weight Restrictions:	n/a	n/a	ton
Height Restrictions:	n/a	n/a	ft.
Curb to Curb Width:	31.5	30	ft.
Outside to Outside Width:	n/a	n/a	ft.
Shoulder Width:	0-3	2-3	ft.
Length of Channel Work:			ft.

Describe bridges and structures; provide specific location information for small structures.

Remarks:

Presence

The structure 062-39-05946 B/NBI No.: 22460 carries SR 62 over Toddy's Branch. The existing structure is a composite prestressed concrete box beam bridge with 1 span that is 59 ft. 10 in. long and has a clear roadway width of 31 ft. 5 in.

The bridge was built in 1968. The most recent Structure Inventory and Appraisal Report, dated March 8, 2016, listed a NBI sufficiency rating of 80.1 and a structural evaluation rating of 5. There is some leaking and efflorescence between the box beams. Beam #7 has one hairline crack and Beam #1 has one crack with delamination. There is vertical cracking with some spalls with exposed rebar in the abutments. There are cracks with efflorescence in the wingwalls. There is a spall at the top of the southeast wingwall. The bridge is proposed to be replaced in its entirety on a new alignment. The Structure Number for the replacement bridge will be 062-39-10270.

Will the structure be rehabilitated or replaced as part of the project? Yes No N/A

If the proposed action has multiple bridges or small structures, this section should be filled out for each structure.

MAINTENANCE OF TRAFFIC (MOT) DURING CONSTRUCTION:

Is a temporary bridge proposed?	Yes		No
Is a temporary roadway proposed?	<input type="checkbox"/>		<input checked="" type="checkbox"/>
Will the project involve the use of a detour or require a ramp closure? (describe in remarks)	<input type="checkbox"/>		<input checked="" type="checkbox"/>
Provisions will be made for access by local traffic and so posted.	<input checked="" type="checkbox"/>		<input type="checkbox"/>
Provisions will be made for through-traffic dependent businesses.	<input checked="" type="checkbox"/>		<input type="checkbox"/>
Provisions will be made to accommodate any local special events or festivals.	<input checked="" type="checkbox"/>		<input type="checkbox"/>
Will the proposed MOT substantially change the environmental consequences of the action?	<input type="checkbox"/>		<input checked="" type="checkbox"/>
Is there substantial controversy associated with the proposed method for MOT?	<input type="checkbox"/>		<input checked="" type="checkbox"/>

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Remarks:

The MOT for the project will require most of the new bridge and approach construction to be completed with traffic maintained on the existing bridge with the new bridge constructed on a new alignment. The proposed method to maintain traffic for the pavement connections on each end, the completion of the NW corner of the abutment, south abutment, and placement of the bridge superstructure is to close the road and utilize a detour for approximately 3 months. The detour length is about 36 miles, and utilizes SR 250, US 421, and SR 129.

Maintenance of traffic will be coordinated with the other 5 projects bundled in this contract. Closures cannot take place concurrently, as access to the adjacent properties that exist between the projects need to be maintained.

The closure will pose a temporary inconvenience to traveling motorists (including school buses and emergency services); all inconveniences will cease upon project completion. Delays will occur during construction but will cease with project completion.

It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access.

ESTIMATED PROJECT COST AND SCHEDULE:

Engineering: \$ 295,365 (2018) Right-of-Way: \$ 10,000 (2021) Construction: \$ 1,931,006 (2023)

Anticipated Start Date of Construction: Spring 2023

Date project incorporated into STIP Fiscal Year (FY) 2020 – 2024 Indiana State Transportation Improvement Plan (STIP) approved July 2, 2019. FY 2018 – 2021 STIP Amendment 18-02, approved July 31, 2017.

Is the project in an MPO Area? Yes No

If yes,

Name of MPO _____

Location of Project in TIP _____

Date of incorporation by reference into the STIP _____

RIGHT OF WAY:

Land Use Impacts	Amount (acres)	
	Permanent	Temporary
Residential	0.094	0.165
Commercial	-	-
Agricultural	1.922	0.057
Forest	0.800	-
Wetlands	-	-
Other:	-	-
Other:	-	-
TOTAL	2.816	0.222

This is page 7 of 25 Project name: SR 62 over Toddy's Branch Bridge Replacement Date: March 2, 2021

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Describe both Permanent and Temporary right-of-way and describe their current use. Typical and Maximum right-of-way widths (existing and proposed) should also be discussed. Any advance acquisition or reacquisition, either known or suspected, and there impacts on the environmental analysis should be discussed.

Remarks:

There are no plans available for the existing roadway so per Indiana statues, the assumed state right-of way (ROW) is the edge of the existing asphalt pavement. There are three private property owners adjacent to the structure. There is one home with some outbuildings located near the southwest corner of the existing bridge. The land on the other areas adjacent to the project are vacant with tall grass.

Right-of-way (ROW) required
 The project requires approximately 2.816 acres of permanent ROW from residential and agricultural fields. The project also requires approximately 0.222 acre of temporary ROW near the residential properties for driveway construction to the west of SR 62 and some agricultural field to the east for channel work. No relocation of residences or businesses will be required.

If the scope of work or permanent or temporary ROW amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately.

Part III – Identification and Evaluation of Impacts of the Proposed Action

SECTION A – ECOLOGICAL RESOURCES

	<u>Presence</u>	<u>Impacts</u>	
		<u>Yes</u>	<u>No</u>
Streams, Rivers, Watercourses & Jurisdictional Ditches	X	X	
Federal Wild and Scenic Rivers			
State Natural, Scenic or Recreational Rivers			
Nationwide Rivers Inventory (NRI) listed			
Outstanding Rivers List for Indiana			
Navigable Waterways			

Remarks:

Presence, with impacts
 Based on a desktop review, a site visit on July 9, 2019 by B&N, the aerial map of the project area (Appendix B, page B-4), and the water resources map in the Red Flag Investigation (RFI) report (Appendix E, page E-2), there are twenty three (23) river and stream segments located within the 0.5 mile search radius. There are two (2) streams, Toddy’s Branch and an unnamed tributary to Toddy’s Branch (UNT-1), both present within the project area.

Waters Report
 A *Waters of the U.S. Determination / Wetland Delineation Report* (WOTUS Report) was approved by INDOT Ecology and Waterway Permitting Office on November 22, 2019. Please refer to Appendix F, page F-2 for the WOTUS Report. It was determined that there are two (2) potentially jurisdictional streams located within the project area. The U.S. Army Corps of Engineers (USACE) makes all final determinations regarding jurisdiction.

No Federal, Wild and Scenic Rivers; State Natural, Scenic, and Recreational Rivers; Outstanding Rivers for Indiana; navigable waterways or National Rivers Inventory waterways are present within or adjacent to the

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project area.

Toddy's Branch is a perennial stream that runs east to west through the project area. It has an ordinary high-water mark (OHWM) width of 17.33 ft and an OHWM depth of 1.83 ft. It has an estimated upstream drainage area of 4.053 mi² according to USGS Stream Stats. It is dominated by bedrock and cobble substrates, which were slightly embedded. Instream cover was minimal. This stream has been channelized over the years within the project area to increase land available for agriculture production. The riparian corridor is wooded along the east side and absent on the west with residential and pasture fields surrounding the area. Bank erosion is extensive upstream and downstream with a large section of the bank that is actively eroding just upstream. Permanent impacts will include 210 linear ft. of Toddy's Branch and temporary impacts will include 100 linear ft. within the permanent impact length, with 0.05 acre of impact below OHWM. This will include the placement of riprap around the newly constructed abutments and wingwalls. A Regional General Permit will likely be needed. Mitigation is not anticipated.

UNT-1 flows from east to west, into Toddy's Branch at the location of the existing structure. UNT to Toddy's Branch is an ephemeral channel with an OHWM width of 7.5 ft and an OHWM depth of 0.83 ft. It is dominated by cobble substrate which were moderately embedded. It contains a narrow wooded riparian buffer within the project area and contains some log jams and highly eroded banks. The newly aligned bridge will be moved to the east of the current alignment and further away from UNT to Toddy's Branch. Permanent impacts will include 30 linear ft. to UNT to Toddy's Branch and temporary impacts will include 30 linear ft. A Regional General Permit will likely be needed. Mitigation is not anticipated.

Early Coordination
 Early coordination letters were sent to on June 12, 2018. U.S. Fish & Wildlife Service (USFWS) responded on June 13, 2018, with standard recommendations (Appendix C, page C-17). The Indiana Department of Natural Resources - Division of Fish & Wildlife (IDNR-DFW) responded on June 13, 2018 with recommendations to avoid and minimize impacts to fish, wildlife, and botanical resources to the greatest extent possible, and compensate for impacts. They recommended a crossing structure, establishing vegetation for bank stabilization, and avoiding channel relocation (Appendix C, page C-13). An automated letter was generated from IDEM on June 15, 2018 (Appendix C, page C-5). All applicable recommendations are included in the Environmental Commitments section of this CE document.

Other Surface Waters	<u>Presence</u>	<u>Impacts</u>	
		Yes	No
Reservoirs	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Lakes	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Farm Ponds	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Detention Basins	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Storm Water Management Facilities	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Other: _____	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Remarks:

No presence, no impact
 Based on a desktop review, a site visit on July 9, 2019 by B&N, the aerial map of the project area (Appendix B, page B-4), and the water resource map in the RFI report (Appendix E, page E-2) there are seven (7) lakes within the 0.5 mile search radius. No other surface waters are present within the project area; therefore, no impacts are expected.

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Early Coordination
 Early coordination letters were sent to on June 12, 2018. U.S. Fish & Wildlife Service (USFWS) responded on June 13, 2018, with standard recommendations (Appendix C, page C-17). The Indiana Department of Natural Resources - Division of Fish & Wildlife (IDNR-DFW) responded on June 13, 2018 with recommendations to avoid and minimize impacts to fish, wildlife, and botanical resources to the greatest extent possible, and compensate for impacts. They recommended a crossing structure, establishing vegetation for bank stabilization, and avoiding channel relocation (Appendix C, page C-13). An automated letter was generated from IDEM on June 15, 2018 (Appendix C, page C-5). All applicable recommendations are included in the Environmental Commitments section of this CE document.

Wetlands Presence Impacts

Total wetland area: 0.017 acre(s) Total wetland area impacted: 0.017 acre(s)

(If a determination has not been made for non-isolated/isolated wetlands, fill in the total wetland area impacted above.)

Wetland No.	Classification	Total Size (Acres)	Impacted Acres	Comments
1	PEMIC	0.017	0.017	Jurisdictional Poor Quality

Wetlands (<i>Mark all that apply</i>)	<u>Documentation</u>	<u>ES Approval Dates</u>
Wetland Determination	<input checked="" type="checkbox"/>	<input type="text" value="November 22, 2019"/>
Wetland Delineation	<input checked="" type="checkbox"/>	<input type="text" value="November 22, 2019"/>
USACE Isolated Waters Determination	<input type="checkbox"/>	<input type="text"/>
Mitigation Plan	<input type="checkbox"/>	<input type="text"/>

Improvements that will not result in any wetland impacts are not practicable because such avoidance would result in (Mark all that apply and explain):

Substantial adverse impacts to adjacent homes, business or other improved properties;	<input type="checkbox"/>
Substantially increased project costs;	<input checked="" type="checkbox"/>
Unique engineering, traffic, maintenance, or safety problems;	<input checked="" type="checkbox"/>
Substantial adverse social, economic, or environmental impacts, or	<input type="checkbox"/>
The project not meeting the identified needs.	<input checked="" type="checkbox"/>

Measures to avoid, minimize, and mitigate wetland impacts need to be discussed in the remarks box.

Remarks: []

Presence, with impacts less than one acre
 Based on a review of the National Wetlands Inventory (NWI) online mapper (<https://www.fws.gov/wetlands/data/Mapper.html>), a site visit on July 9, 2019 by B&N, the USGS topographic map (Appendix B, page B-3), and the RFI report (Appendix E, page E-2) there are fifteen (15) wetlands located within the 0.5 mile search radius. There is one wetland present within the project area.

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Waters Report

A Waters of the U.S. Determination / Wetland Delineation Report was approved by INDOT Ecology and Waterway Permitting Office on November 22, 2019. It was determined that there is one (1) potentially jurisdictional wetland within the project area. The USACE makes all final determinations regarding jurisdiction.

The wetland is located along the banks of Toddy’s Branch to the east of SR 62. It has an emergent vegetative community, is approximately 0.017 acre, and is poor quality. The entire wetland will be impacted by the new alignment and bank stabilization. As the wetland is within the area for the new alignment, avoidance is not possible. Mitigation is not anticipated.

Early Coordination

Early coordination letters were sent to on June 12, 2018. U.S. Fish & Wildlife Service (USFWS) responded on June 13, 2018, with standard recommendations (Appendix C, page C-17). The Indiana Department of Natural Resources - Division of Fish & Wildlife (IDNR-DFW) responded on June 13, 2018 with recommendations to avoid and minimize impacts to fish, wildlife, and botanical resources to the greatest extent possible, and compensate for impacts. They recommended a crossing structure, establishing vegetation for bank stabilization, avoiding channel relocation, and coordinating with the Indiana Department of Environmental Management (IDEM) for wetland impacts (Appendix C, page C-13). An automated letter was generated from IDEM on June 15, 2018 (Appendix C, page C-5). All applicable recommendations are included in the Environmental Commitments section of this CE document.

Terrestrial Habitat

Unique or High Quality Habitat

Presence

X

Impacts

Yes	No
X	

Use the remarks box to identify each type of habitat and the acres impacted (i.e. forested, grassland, farmland, lawn, etc).

Remarks:

Presence, with impacts

Based on a desktop review, a site visit on July 9, 2019 by B&N, the aerial map of the project area (Appendix B, page B-4, there is forested and residential mowed lawn habitats. The grassland habitat surrounds the project and has been used for agriculture production. Total ground disturbance for this habitat will be approximately 0.77 acre. The forest habitat is found surrounding Toddy’s Branch to the north and east of SR 62. This habitat is dominated by American Sycamore (*Platanus occidentalis*) trees. There will be approximately 0.8 acre of tree removal for the new alignment and line of sight for SR 62. Avoidance alternatives would not be practicable because without the tree removal, sight lines are impaired and could pose a safety issue surrounding the project area. Mitigation is not anticipated.

Early Coordination

Early coordination letters were sent to on June 12, 2018. U.S. Fish & Wildlife Service (USFWS) responded on June 13, 2018, with standard recommendations (Appendix C, page C-17). The Indiana Department of Natural Resources - Division of Fish & Wildlife (IDNR-DFW) responded on June 13, 2018 with recommendations to avoid and minimize impacts to fish, wildlife, and botanical resources to the greatest extent possible, and compensate for impacts. They recommended a crossing structure, establishing vegetation for bank stabilization, avoiding channel relocation, and coordinating with the Indiana Department of Environmental Management (IDEM) for wetland impacts (Appendix C, page C-13). An automated letter was generated from IDEM on June 15, 2018 (Appendix C, page C-5). All applicable recommendations are included in the Environmental Commitments section of this CE document.

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If there are high incidences of animal movements observed in the project area, or if bridges and other areas appear to be the sole corridor for animal movement, consideration of utilizing wildlife crossings should be taken.

Karst

Is the proposed project located within or adjacent to the potential Karst Area of Indiana?
 Are karst features located within or adjacent to the footprint of the proposed project?

Yes	No
<input type="checkbox"/>	X
<input type="checkbox"/>	X
<input type="checkbox"/>	<input type="checkbox"/>

If yes, will the project impact any of these karst features?

Use the remarks box to identify any karst features within the project area. (Karst investigation must comply with the Karst MOU, dated October 13, 1993)

Remarks:

Outside karst area
 Based on a desktop review, the project is located outside the designated karst region of Indiana as outlined in the October 13, 1993 Memorandum of Understanding (MOU). According to the topo map of the project area (Appendix B, page B-3), the RFI report (Appendix E, page E-2) there are no karst features identified within or adjacent to the project area. In the early coordination response, the Indiana Geological Survey (IGS) indicated the potential for karst features to exist in the project area (Appendix C, page C-19). IGS did identify a 1% Annual Chance Flood Hazard, low bedrock resource potential, no sand and gravel resources, and no active or abandoned mineral resource extraction sites. Response from IGS has been communicated with the designer on August 28, 2020. No impacts are expected.

Threatened or Endangered Species

Within the known range of any federal species
 Any critical habitat identified within project area
 Federal species found in project area (based upon informal consultation)
 State species found in project area (based upon consultation with IDNR)

	<u>Presence</u>		<u>Impacts</u>	
	Yes	No	Yes	No
Within the known range of any federal species	X	<input type="checkbox"/>	X	<input type="checkbox"/>
Any critical habitat identified within project area	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Federal species found in project area (based upon informal consultation)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
State species found in project area (based upon consultation with IDNR)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Is Section 7 formal consultation required for this action? Yes No **X**

Remarks:

Based on a desktop review and the approved RFI report (Appendix E, page E-2), the IDNR Jefferson Endangered, Threatened and Rare (ETR) Species List has been checked and is included in (Appendix E, page E-11). The highlighted species on the list reflect the federal and state identified ETR species located within the county. According to the IDNR-DFW early coordination response letter dated June 13, 2018 (Appendix C, page C-13), the Natural Heritage Program's Database has been checked, to date, no plant or animal species listed as state or federally threatened, endangered, or rare have been reported to occur in the project vicinity.

Indiana Bat and Northern Long-Eared Bat

Bats, Programmatic Informal Consultation – Not Likely to Adversely Affect
 Project information was submitted through the USFWS's Information for Planning and Consultation (IPaC) portal, and an official species list was generated (Appendix C, page C-38). The project is within range of the federally endangered Indiana bat (*Myotis sodalis*) and the federally threatened northern long-eared bat (NLEB) (*Myotis septentrionalis*). No additional species were found within or adjacent to the project area other than the Indiana bat and northern long-eared bat.

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The project qualifies for the *Range-wide Programmatic Informal Consultation for the Indiana bat and northern long-eared bat (NLEB)*, dated May 2016 (revised February 2018), between FHWA, Federal Railroad Administration (FRA), Federal Transit Administration (FTA), and USFWS. An effect determination key was completed on September 18, 2019, and based on the responses provided, the project was found to “*may affect, but not likely to adversely affect*” the Indiana bat and/or the NLEB. INDOT reviewed and verified the effect finding on September 18, 2019 and requested USFWS’s review of the finding (Appendix C, page C-22). No response was received from USFWS within the 14-day review period; therefore, it was concluded they concur with the finding. Avoidance and Mitigation Measures (AMMs) are included as firm commitments in the *Environmental Commitments* section of this document.

This precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act, as amended. If new information on endangered species at the site becomes available, or if project plans are changed, USFWS will be contacted for consultation.

SECTION B – OTHER RESOURCES

	<u>Presence</u>	<u>Impacts</u>	
		Yes	No
Drinking Water Resources			
Wellhead Protection Area	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Public Water System(s)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Residential Well(s)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Source Water Protection Area(s)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Sole Source Aquifer (SSA)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

If a SSA is present, answer the following:

	Yes	No
Is the Project in the St. Joseph Aquifer System?	<input type="checkbox"/>	<input type="checkbox"/>
Is the FHWA/EPA SSA MOU Applicable?	<input type="checkbox"/>	<input type="checkbox"/>
Initial Groundwater Assessment Required?	<input type="checkbox"/>	<input type="checkbox"/>
Detailed Groundwater Assessment Required?	<input type="checkbox"/>	<input type="checkbox"/>

Remarks:

Sole Source Aquifer
Outside of Sole Source Aquifer (SSA)
 The project is located in Jefferson County, which is not located within the area of the St. Joseph Sole Source Aquifer, the only legally designated sole source aquifer in the state of Indiana. Therefore, the FHWA/EPA Sole Source Aquifer Memorandum of Understanding (MOU) is not applicable to this project. Therefore, a detailed groundwater assessment is not needed, and no impacts are expected.

Wellhead Protection Area and Source Water
Not located in a Wellhead Protection Area or Source Water Area
 The Indiana Department of Environmental Management’s Wellhead Proximity Determinator website (<http://www.in.gov/idem/cleanwater/pages/wellhead/>) was accessed on December 12, 2019 by B&N. This project is not located within a Wellhead Protection Area or Source Water Area. No impacts are expected.

Water Wells
No wells present, no impacts
 The Indiana Department of Natural Resources Water Well Record Database website (<https://www.in.gov/dnr/water/3595.htm>) was accessed on December 12, 2019 by B&N. No wells are located near this project. Therefore, no impacts are expected.

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Urban Area Boundary
Not in an Urban Area Boundary Location
 Based on a desktop review of the INDOT MS4 website (<https://entapps.indot.in.gov/MS4/>) by B&N on December 12, 2019 and the RFI report, this project is not located in an Urban Area Boundary location. No impacts are expected.

Public Water System
Not in a Public Water System Location
 Based on a desktop review, a site visit on July 9, 2019 by B&N, the aerial map of the project area (Appendix B, page B-4), no public water systems were identified. Therefore, no impacts are expected.

Flood Plains	<u>Presence</u>	<u>Impacts</u>	
		Yes	No
Longitudinal Encroachment	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Transverse Encroachment	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Project located within a regulated floodplain	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Homes located in floodplain within 1000' up/downstream from project	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Discuss impacts according to classification system described in the "Procedural Manual for Preparing Environmental Studies".

Remarks:

In floodplain
 Based on a desktop review of The Indiana Department of Natural Resources Indiana Floodway Information Portal website (<http://dnrmaps.dnr.in.gov/appsphp/fdms/>) by B&N on January 4, 2021, and the RFI report; this project is located in a regulatory floodplain as determined from approved IDNR floodplain maps (Appendix I, page I-6). An early coordination letter was sent on January 4, 2021, to the local Floodplain Administrator. Coordination is ongoing and will occur into the next stages of the project. A firm commitment has been added to the commitments section. This project qualifies as a Category 5 per the current INDOT CE Manual, which states:

There will be no substantial impacts on natural and beneficial floodplain values; there will be no substantial change in flood risk; and there will be no substantial increase in potential for interruption or termination of emergency services or emergency evacuation routes; therefore, it has been determined that this encroachment is not substantial. A hydraulic design study that addresses various structure size alternatives has been approved by INDOT Hydraulics. A summary of this study will be included with the Field Check Plans.

Due to the rural nature of the project area and the drainage area of Toddy's Branch under 50 square miles, a DNR Construction in a Floodway Permit is not required.

Farmland	<u>Presence</u>	<u>Impacts</u>	
		Yes	No
Agricultural Lands	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Prime Farmland (per NRCS)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Total Points (from Section VII of CPA-106/AD-1006* _____
 *If 160 or greater, see CE Manual for guidance.

See CE Manual for guidance to determine which NRCS form is appropriate for your project.

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Remarks:

No presence, no impact
 Based on a desktop review, a site visit on July 9, 2019 by B&N, the aerial map of the project area (Appendix B, page B-4), there is no land that meets the definition of farmland under the Farmland Protection Policy Act (FPPA) within or adjacent to the project area. The requirements of the FPPA do not apply to this project; therefore, no impacts are expected. An early coordination letter was sent on June 12, 2018, to Natural Resources Conservation Services (NRCS). NRCS stated that the Des. 1701457 will not cause conversion of prime farmland (Appendix C, page C-44).

SECTION C – CULTURAL RESOURCES

	Category	Type	INDOT Approval Dates	N/A
Minor Projects PA Clearance	B	6	1/14/2021	

Eligible and/or Listed Resource Present

Results of Research

Archaeology	<input type="checkbox"/>
NRHP Buildings/Site(s)	<input type="checkbox"/>
NRHP District(s)	<input type="checkbox"/>
NRHP Bridge(s)	<input type="checkbox"/>

Project Effect

No Historic Properties Affected No Adverse Effect Adverse Effect

Documentation Prepared

Documentation (mark all that apply)

		ES/FHWA Approval Date(s)	SHPO Approval Date(s)
Historic Properties Short Report	<input type="checkbox"/>		
Historic Property Report	<input type="checkbox"/>		
Archaeological Records Check/ Review	<input type="checkbox"/>		
Archaeological Phase Ia Survey Report	<input checked="" type="checkbox"/>	2/10/2021	2/3/2021
Archaeological Phase Ic Survey Report	<input type="checkbox"/>		
Archaeological Phase II Investigation Report	<input type="checkbox"/>		
Archaeological Phase III Data Recovery	<input type="checkbox"/>		
APE, Eligibility and Effect Determination	<input type="checkbox"/>		
800.11 Documentation	<input type="checkbox"/>		

Memorandum of Agreement (MOA) **MOA Signature Dates** (List all signatories)

Describe all efforts to document cultural resources, including a detailed summary of the Section 106 process, using the categories outlined in the remarks box. The completion of the Section 106 process requires that a Legal Notice be published in local newspapers. Please indicate the publication date, name of paper(s) and the comment period deadline. Likewise include any further Section 106 work which must be completed at a later date, such as mitigation or deep trenching.

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Remarks:

An Archaeological Records Check and Phase Ia Reconnaissance Report was conducted by Green 3/SJCA and approved on January 14, 2021. The records check had no recorded archaeological sites within one-mile of the project area. No cultural materials were located during the reconnaissance (Appendix D, page D-5).

Minor Project PA Category B projects

On January 14, 2021 the INDOT Cultural Resource Office (CRO) determined that this project falls within the guidelines of Category B, Type 6 under the Minor Projects Programmatic Agreement, (Appendix D, page D-2). Other minor actions if deemed appropriate for coverage under this MPPA, by consultation and mutual agreement between INDOT, FHWA, and the SHPO. INDOT CRO Historian performed a desktop review of available information and no listed resources located with 0.25 mile of the project area. The adjacent structures were evaluated with none appearing to possess the historic significance or material integrity required to be considered National Register of Historic Places (NRHP) eligible. The bridge was not included in the *Indiana Historic Bridge Inventory*. The Advisory Council on Historic Preservation (ACHP) issued the *Program Comment for Streamlining Section 106 Review for Actions Affecting Post-1945 Concrete and Steel Bridges (Program Comment)*. The *Program Comment* applies to this bridge because it has not been previously listed in or determined eligible for listing in the NRHP and it is not located in or adjacent to a historic district. Based on the available information, as summarized above, no above-ground concerns exist.

A Phase Ia Archaeological Survey was conducted by SJCA, dated December 7, 2020. The Phase Ia Archaeological Survey located no archaeological sites within the project area, and it is recommended that the project be allowed to proceed as planned.

If any archaeological artifacts or human remains are uncovered during construction, demolition, or earth moving activities, construction in the immediate area of the find will be stopped, and the INDOT Cultural Resources Office and the Division of Historic Preservation and Archaeology will be notified immediately.

(Appendix D, page D-5). No further consultation is required. This completes the Section 106 process and the responsibilities of the FHWA under Section 106 have been fulfilled.

SECTION D – SECTION 4(f) RESOURCES/ SECTION 6(f) RESOURCES

Section 4(f) Involvement (mark all that apply)

	<u>Presence</u>		<u>Use</u>	
		Yes	No	
Parks & Other Recreational Land				
Publicly owned park	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Publicly owned recreation area	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Other (school, state/national forest, bikeway, etc.)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
	<u>Evaluations Prepared</u>		<u>FHWA Approval date</u>	
Programmatic Section 4(f)*	<input type="checkbox"/>			
“De minimis” Impact*	<input type="checkbox"/>			
Individual Section 4(f)	<input type="checkbox"/>			<input type="text"/>
	<u>Presence</u>		<u>Use</u>	
		Yes	No	
Wildlife & Waterfowl Refuges				
National Wildlife Refuge	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
National Natural Landmark	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
State Wildlife Area	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	

This is page 16 of 25 Project name: SR 62 over Toddy’s Branch Bridge Replacement Date: March 2, 2021

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State Nature Preserve

Evaluations Prepared

Programmatic Section 4(f)*
“De minimis” Impact*
Individual Section 4(f)

FHWA Approval date

Historic Properties

Sites eligible and/or listed on the NRHP

Presence

Use

Yes

No

Evaluations Prepared

Programmatic Section 4(f)*
“De minimis” Impact*
Individual Section 4(f)

FHWA Approval date

**FHWA approval of the environmental document also serves as approval of any Section 4f Programmatic and/or De minimis evaluation(s) discussed below.*

Discuss Programmatic Section 4(f) and “de minimis” Section 4(f) impacts in the remarks box below. Individual Section 4(f) documentation must be separate Draft and Final documents. For further discussions on Programmatic, “de minimis” and Individual Section 4(f) evaluations please refer to the “Procedural Manual for the Preparation of Environmental Studies”. Discuss proposed alternatives that satisfy the requirements of Section 4(f).

Remarks:

No presence, no impact
 Section 4(f) of the U.S. Department of Transportation Act of 1966 prohibits the use of certain public and historic lands for federally funded transportation facilities unless there is no feasible and prudent alternative. The law applies to significant publicly owned parks, recreation areas, wildlife / waterfowl refuges, and NRHP eligible or listed historic properties regardless of ownership. Lands subject to this law are considered Section 4(f) resources.

Based on a desktop review, site visits on July 9, 2019 by B&N, the aerial map of the project area (Appendix B, page B-4), and the RFI report (Appendix E, page E-2) there are no 4(f) resources located within the 0.5 mile search radius. There are no Section 4(f) resources within or adjacent to the project area. Therefore, no use is expected.

Section 6(f) Involvement

Presence

Use

Section 6(f) Property

Yes

No

Discuss proposed alternatives that satisfy the requirements of Section 6(f). Discuss any Section 6(f) involvement.

Remarks:

No presence or presence, no impact
 The U.S. Land and Water Conservation Fund Act of 1965 established the Land and Water Conservation Fund (LWCF), which was created to preserve, develop, and assure accessibility to outdoor recreation resources. Section 6(f) of this Act prohibits conversion of lands purchased with LWCF monies to a non-recreation use.

A review of 6(f) properties from the Land and Water Conservation Fund (LWCF) property list (<https://www.in.gov/indot/2523.htm>) revealed a total of ten (10) properties in Jefferson County (Appendix I,

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page I-2). None of these properties are located within or adjacent to the project area. Therefore, there will be no impacts to 6(f) resources as a result of this project.

SECTION E – Air Quality

Air Quality

Conformity Status of the Project

	Yes	No
Is the project in an air quality non-attainment or maintenance area?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
If YES, then:		
Is the project in the most current MPO TIP?	<input type="checkbox"/>	<input type="checkbox"/>
Is the project exempt from conformity?	<input type="checkbox"/>	<input type="checkbox"/>
If the project is NOT exempt from conformity, then:		
Is the project in the Transportation Plan (TP)?	<input type="checkbox"/>	<input type="checkbox"/>
Is a hot spot analysis required (CO/PM)?	<input type="checkbox"/>	<input type="checkbox"/>

Level of MSAT Analysis required?

Level 1a Level 1b Level 2 Level 3 Level 4 Level 5

Remarks:

Project Bundled in Contract
The FY 2020-2024 Statewide Transportation Improvement Program (STIP) is listed based on the lead DES number in the contract. The lead DES number for this contract is 1701455. The FY 2020-2024 STIP includes DES number 1701457 by reference with the contract number B-40421. (Appendix H, page H-2)

This project is also included in the FY 2018-2021 Indiana STIP.(Appendix H, page H-3).

Attainment area
This project is located in Jefferson County, which is currently in attainment for all criteria pollutants according to IDEM Air Quality in Indiana Nonattainment Status for Indiana Counties (<https://www.in.gov/idem/airquality/2339.htm>). Therefore, the conformity procedures of 40 CFR Part 93 do not apply.

MSAT Level 1a Analysis
This project is of a type qualifying as a categorical exclusion (Group 1) under 23 CFR 771.117(c), or exempt under the Clean Air Act conformity rule under 40 CFR 93.126, and as such, a Mobile Source Air Toxics analysis is not required.

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SECTION F - NOISE

Noise **Yes** **No**
 Is a noise analysis required in accordance with FHWA regulations and INDOT's traffic noise policy?

	No	Yes/ Date
ES Review of Noise Analysis	<input type="checkbox"/>	<input type="checkbox"/>

Remarks:

Type III Project
 This project is a Type III project. In accordance with 23 CFR 772 and the current *Indiana Department of Transportation Traffic Noise Analysis Procedure*, this action does not require a formal noise analysis.

SECTION G – COMMUNITY IMPACTS

Regional, Community & Neighborhood Factors

	Yes	No
Will the proposed action comply with the local/regional development patterns for the area?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Will the proposed action result in substantial impacts to community cohesion?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will the proposed action result in substantial impacts to local tax base or property values?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will construction activities impact community events (festivals, fairs, etc.)?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Does the community have an approved transition plan?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
If No, are steps being made to advance the community's transition plan?	<input type="checkbox"/>	<input type="checkbox"/>
Does the project comply with the transition plan? (explain in the remarks box)	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Remarks:

The closure will pose a temporary inconvenience to traveling motorists (including school buses and emergency services); however, no significant delays are anticipated, and all inconveniences will cease upon project completion. Delays will occur during construction but will cease with project completion. There will be temporary but not substantial impacts to community cohesion, local tax base, or community events as the MOT will follow the official detour. There is an approved transition plan for Jefferson County.

Indirect and Cumulative Impacts **Yes** **No**
 Will the proposed action result in substantial indirect or cumulative impacts?

Remarks:

Indirect impacts are effects which are caused by the action and are later in time or farther removed in distance but are still reasonably foreseeable. Indirect effects may include growth inducing effects and other effects related to induced changes in the pattern of land use, population density, or growth rate. Cumulative impacts affect the environment which result from the incremental impact of the action when added to other past, present, and reasonably foreseeable future actions regardless of what agency or person undertakes such actions.

This project is not expected to have any significant community cohesion, indirect, or cumulative impacts. This project will not add capacity to the roadway, nor is it expected to change the surrounding properties.

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Public Facilities & Services

Will the proposed action result in substantial impacts on health and educational facilities, public and private utilities, emergency services, religious institutions, airports, public transportation or pedestrian and bicycle facilities? *Discuss how the maintenance of traffic will affect public facilities and services.*

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Remarks:

No presence, no impact
 Based on a desktop review, site visits on July 9, 2019 by B&N, the aerial map of the project area (Appendix B, page B-4), and the RFI report (Appendix E, page E-2) there are no public facilities within the 0.5 mile search radius. There are no public facilities within or adjacent to the project area. Access to all properties will be maintained during construction, therefore, no impacts are expected.

Environmental Justice (EJ) (Presidential EO 12898)

During the development of the project were EJ issues identified?
 Does the project require an EJ analysis?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>

If YES, then:

Are any EJ populations located within the project area?

<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Will the project result in adversely high or disproportionate impacts to EJ populations?

Remarks:

EJ Analysis, No EJ Populations
 Under FHWA Order 6640.23A, FHWA and the project sponsor, as a recipient of funding from FHWA, are responsible to ensure that their programs, policies, and activities do not have a disproportionately high and adverse effect on minority or low-income populations. Per the current INDOT Categorical Exclusion Manual, an Environmental Justice (EJ) Analysis is required for any project that has two or more relocations or 0.5 acre of additional permanent right-of-way. The project will require approximately 2.816 acres of ROW and approximately 0.222 acre of temporary ROW for the proposed realignment. Therefore, an EJ Analysis is required.

Potential EJ impacts are detected by locating minority and low-income populations relative to a reference population to determine if populations of EJ concern exists and whether there could be disproportionately high and adverse impacts to them. The reference population may be a county, city or town and is called the community of comparison (COC). In this project, the COC is Jefferson County. The community that overlaps the project area is called the affected community (AC). In this project, the AC is Census Tract 9660. An AC has a population of concern for EJ if the population is more than 50% minority or low-income or if the low-income or minority population is 125% of the COC. Data from the 2014-2018 American Community Survey (ACS) was obtained from the US Census Bureau Website <https://factfinder.census.gov/> on September 3, 2020 by B&N. The data collected for minority and low-income populations within the AC are summarized in the below table.

	COC – Jefferson County, Indiana	AC – Census Tract 9660, Jefferson County, Indiana
Percent Minority	7.17%	4.24%
125% of COC	8.96%	AC < 125% COC
EJ Population of Concern		No
Percent Low-Income	14.89%	9.90%
125% of COC	18.61%	AC < 125% COC
EJ Population of Concern		No

¹United States Census Bureau, 2014-2018 American Community Survey (ACS)

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AC, Census Tract 9660 has a percent minority of 4.24% which is below 50% and is below the 125% COC threshold. Therefore, the AC does not contain minority populations of EJ concern.

AC, Census Tract 9660 has a percent low-income of 9.90% which is below 50% and is below the 125% COC threshold. Therefore, the AC does not contain low-income populations of EJ concern.

The census data sheets, map, and calculations can be found in Appendix I, pages I-3 through I-5. No further environmental justice analysis is warranted.

Relocation of People, Businesses or Farms

Will the proposed action result in the relocation of people, businesses or farms?
 Is a Business Information Survey (BIS) required?
 Is a Conceptual Stage Relocation Study (CSRS) required?
 Has utility relocation coordination been initiated for this project?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>

Number of relocations: Residences: 0 Businesses: 0 Farms: 0 Other: 0

If a BIS or CSRS is required, discuss the results in the remarks box.

Remarks:

No Relocations
 No relocations of people, businesses, or farms will take place as a result of this project.

SECTION H – HAZARDOUS MATERIALS & REGULATED SUBSTANCES

Hazardous Materials & Regulated Substances (Mark all that apply)

Red Flag Investigation
 Phase I Environmental Site Assessment (Phase I ESA)
 Phase II Environmental Site Assessment (Phase II ESA)
 Design/Specifications for Remediation required?

Documentation

<input checked="" type="checkbox"/>
<input type="checkbox"/>
<input type="checkbox"/>
<input type="checkbox"/>

	No	Yes/ Date
ES Review of Investigations		Yes / December 15, 2019

Include a summary of findings for each investigation.

Remarks:

Presence, no impact
 Based on a review of GIS and available public records, a RFI was completed on December 13, 2019 by B&N (Appendix E, page E-2). One hazmat site is located within 0.5 mile of the project area, and no hazmat sites are located within the project area; however, no hazmat sites were identified in or within 0.5 mile of the project area that will impact the project. The nearest NPDES Facility is 0.35 mile south west from the project area. No impacts are expected because of distance or a No Further Action determination by Indiana Department of Environmental Management (IDEM). Further investigation for hazardous material concerns is not required at this time.

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SECTION I – PERMITS CHECKLIST

Permits (mark all that apply)

Likely Required

Army Corps of Engineers (404/Section10 Permit)

Individual Permit (IP)	<input type="checkbox"/>
Nationwide Permit (NWP)	<input type="checkbox"/>
Regional General Permit (RGP)	<input checked="" type="checkbox"/>
Pre-Construction Notification (PCN)	<input type="checkbox"/>
Other	<input type="checkbox"/>
Wetland Mitigation required	<input type="checkbox"/>
Stream Mitigation required	<input type="checkbox"/>

IDEM

Section 401 WQC	<input checked="" type="checkbox"/>
Isolated Wetlands determination	<input type="checkbox"/>
Rule 5	<input checked="" type="checkbox"/>
Other	<input type="checkbox"/>
Wetland Mitigation required	<input type="checkbox"/>
Stream Mitigation required	<input type="checkbox"/>

IDNR

Construction in a Floodway	<input type="checkbox"/>
Navigable Waterway Permit	<input type="checkbox"/>
Lake Preservation Permit	<input type="checkbox"/>
Other	<input type="checkbox"/>
Mitigation Required	<input type="checkbox"/>

US Coast Guard Section 9 Bridge Permit

Others (Please discuss in the remarks box below)

<input type="checkbox"/>

Remarks:

Permits
A Clean Water Act (CWA) Section 404/401 Regional General Permit from the USACE/IDEM will likely be required for this project. A Rule 5 permit from IDEM will likely be required for this project.

Applicable recommendations are included in the Environmental Commitments section of this document. If permits are found to be necessary, the conditions of the permit will be requirements of the project and will supersede these recommendations.

It is the responsibility of the project sponsor to identify and obtain all required permits.

SECTION J- ENVIRONMENTAL COMMITMENTS

The following information should be provided below: List all commitments, name of agency/organization requesting the commitment(s), and indicating which are firm and which are for further consideration. The commitments should be numbered.

Remarks:

Firm

- 1) If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately. (INDOT ESD and INDOT District)
- 2) It is the responsibility of the project sponsor to notify school corporations and emergency services at

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least two weeks prior to any construction that would block or limit access. (INDOT ESD)

- 3) GENERAL AMM 1
Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs. (USFWS)
- 4) LIGHTING AMM1
Direct temporary lighting away from suitable habitat during the active season. (USFWS)
- 5) TREE REMOVAL AMM 1
Modify all phases/aspects of the project (e.g., temporary work areas, alignments) to avoid tree removal. (USFWS)
- 6) TREE REMOVAL AMM 2
Apply time of year restrictions, (April 1 through September 30), for tree removal when bats are not likely to be present, or limit tree removal to 10 or fewer trees per project at any time of year within 100 feet of existing road/ rail surface and outside of documented roosting/foraging habitat or travel corridors; visual emergence survey must be conducted with no bats observed. (USFWS)
- 7) TREE REMOVAL AMM 3
Ensure tree removal is limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits). (USFWS)
- 8) TREE REMOVAL AMM 4
Do not remove documented Indiana bat or NLEB roosts that are still suitable for roosting, or trees within 0.25 miles of roosts, or documented foraging habitat any time of year. (USFWS)
- 9) USFWS Bridge/Structure Assessment shall take place no earlier than two (2) years prior to the start of construction. If construction will begin after February 23, 2023, an inspection of the structure by a qualified individual, must be performed. Inspection of the structure should check for presence of bats/bat indicators and/or presence of birds. The results of the inspection must indicate no signs of bats or birds. If signs of bats or birds are documented during this inspection, the INDOT District Environmental Manager must be contacted immediately. (INDOT ESD)
- 10) Coordination with the Jefferson County Floodplain Administrator will continue.(Designer)

For Further Consideration:

- 1) All plant material, mud, and debris should be removed, and all water drained from any equipment before entering or leaving the waterway to prevent the spread of aquatic and terrestrial invasive species. (IDNR)
- 2) Grouted riprap is not recommended due to negative impacts to fish, wildlife, and botanical resources. (IDNR)
- 3) Impacts to non-wetland forest of one (1) acre or more should be mitigated at a minimum 2:1 ratio. If less than one acre of non-wetland forest is removed in a rural setting, replacement should be at a 1:1 ratio based on area. Impacts to nonwetland forest under one (1) acre in an urban setting should be mitigated by planting five trees, at least 2 inches in diameter-at-breast height (dbh), for each tree which

Indiana Department of Transportation

County Jefferson Route SR 62 Des. No. 1701457

- is removed that is 10 inches dbh or greater (5:1 mitigation based on the number of large trees). (IDNR)
- 4) Operate equipment used to replace/rehabilitate/modify stream crossings from the existing roadways whenever possible. (IDNR)
 - 5) The new, replacement, or rehabbed structure should not create conditions that are less favorable for wildlife passage under the structure compared to the current conditions. (IDNR)
 - 6) Riprap must not be placed in the active thalweg channel or placed in the streambed in a manner that precludes fish or aquatic organism passage (riprap must not be placed above the existing streambed elevation). Riprap may be used only at the toe of the side slopes up to the ordinary high-water mark (OHWM). The banks above the OHWM must be restored, stabilized, and revegetated using geotextiles and a mixture of grasses, sedges, wildflowers, shrubs, and trees native to [site indicated] and specifically for stream bank/floodway stabilization purposes as soon as possible upon completion. (IDNR)
 - 7) Plant five trees, at least 2 inches in diameter-at-breast height, for each tree which is removed that is ten inches or greater in diameter-at-breast height. (IDNR) Do not cut any trees suitable for Indiana bat or Northern Long-eared bat roosting from April 1 through September 30. [RSP 107-B-040] (IDNR)
 - 8) Do not excavate in the low flow area except for the placement of piers, foundations, and riprap, or removal of the old structure. (IDNR)
 - 9) Avoid all work within the inundated part of the stream channel during the fish spawning season (April 1 through June 30); except for work within sealed structures such as caissons or cofferdams that were installed prior to the spawning season. No equipment shall be operated below Ordinary High-Water Mark during this time unless the machinery is within the caissons or on the cofferdams. (USFWS)
 - 10) Minimize the extent of hard armor (riprap) in bank stabilization by using bioengineering techniques whenever possible. If riprap is utilized for bank stabilization, extend it below low-water elevation to provide aquatic habitat.(USFWS)
 - 11) Restrict below low-water work in streams to placement of culverts, piers, pilings, and/or footings, shaping of the spill slopes around the bridge abutments, and placement of riprap. (USFWS)

Indiana Department of Transportation

County Jefferson Route SR 62 Des. No. 1701457

SECTION K- EARLY COORDINATION

Please list the date coordination was sent and all agencies that were contacted as a part of the development of this Environmental Study. Also, include the date of their response or indicate that no response was received. INDOT and FHWA are automatically considered early coordination participants and should only be listed if a response is received.

Remarks:

Agency	Sent	Response
Indiana Department of Environmental Management (IDEM)	June 12, 2018	June 15, 2018
US Army Corp of Engineers Louisville District (USACE)	June 12, 2018	NR
US Department of Housing and Urban Development (HUD)	June 12, 2018	NR
National Park Service Midwest Region (NPS)	June 12, 2018	NR
US Fish and Wildlife Service (USFWS)	June 12, 2018	June 13, 2018
Indiana Geological Survey (IGS)	June 12, 2018	August 28, 2020
Indiana Department of Natural Resources Division of Fish and Wildlife (IDNR-DFW)	June 12, 2018	June 13, 2018
Natural Resources Conservation Service	June 12, 2018	June 29, 2020
Jefferson County Highway Department	June 12, 2018	NR
Jefferson County Commissioner	June 12, 2018	NR
Jefferson County Council Members	June 12, 2018	NR
Jefferson County Floodplain Administrator*	January 4, 2021	On-going*

NR- No Response
 *Coordination will continue after the CE is approved.

A copy of the Early Coordination Letters and agency responses are provided in Appendix C.

Appendices

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Appendix A

INDOT Supporting Documentation

Categorical Exclusion Level Thresholds

	PCE	Level 1	Level 2	Level 3	Level 4 ¹
Section 106	Falls within guidelines of Minor Projects PA	“No Historic Properties Affected”	“No Adverse Effect”	-	“Adverse Effect” Or Historic Bridge involvement ²
Stream Impacts	No construction in waterways or water bodies	< 300 linear feet of stream impacts	≥ 300 linear feet of stream impacts	-	Individual 404 Permit
Wetland Impacts	No adverse impacts to wetlands	< 0.1 acre	-	< 1 acre	≥ 1 acre
Right-of-way³	Property acquisition for preservation only or none	< 0.5 acre	≥ 0.5 acre	-	-
Relocations	None	-	-	< 5	≥ 5
Threatened/Endangered Species (Species Specific Programmatic for Indiana bat & northern long eared bat)	“No Effect”, “Not likely to Adversely Affect” (Without AMMs ⁴ or with AMMs required for all projects ⁵)	“Not likely to Adversely Affect” (With any other AMMs)	-	“Likely to Adversely Affect”	Project does not fall under Species Specific Programmatic
Threatened/Endangered Species (Any other species)	Falls within guidelines of USFWS 2013 Interim Policy	“No Effect”, “Not likely to Adversely Affect”	-	-	“Likely to Adversely Affect”
Environmental Justice	No disproportionately high and adverse impacts	-	-	-	Potential ⁶
Sole Source Aquifer	Detailed Assessment Not Required	-	-	-	Detailed Assessment
Floodplain	No Substantial Impacts	-	-	-	Substantial Impacts
Coastal Zone Consistency	Consistent	-	-	-	Not Consistent
National Wild and Scenic River	Not Present	-	-	-	Present
New Alignment	None	-	-	-	Any
Section 4(f) Impacts	None	-	-	-	Any
Section 6(f) Impacts	None	-	-	-	Any
Added Through Lane	None	-	-	-	Any
Permanent Traffic Alteration	None	-	-	-	Any
Coast Guard Permit	None	-	-	-	Any
Noise Analysis Required	No	-	-	-	Yes
Air Quality Analysis Required	No	-	-	-	Yes ⁷
Approval Level	Concurrence by INDOT District Environmental or Environmental Services	Yes	Yes	Yes	Yes
<ul style="list-style-type: none"> • District Env. Supervisor • Env. Services Division • FHWA 			Yes	Yes	Yes

¹Coordinate with INDOT Environmental Services. INDOT will then coordinate with the appropriate FHWA Environmental Specialist.

²Any involvement with a bridge processed under the Historic Bridge Programmatic Agreement.

³Permanent and/or temporary right-of-way.

⁴AMMs = Avoidance and Mitigation Measures.

⁵AMMs determined by the IPAC decision key to be needed that are listed in the USFWS *User’s Guide for the Range-wide Programmatic Consultation for Indiana bat and Northern long-eared bat* as “required for all projects”.

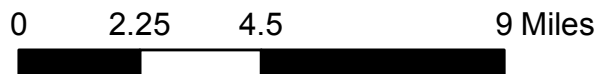
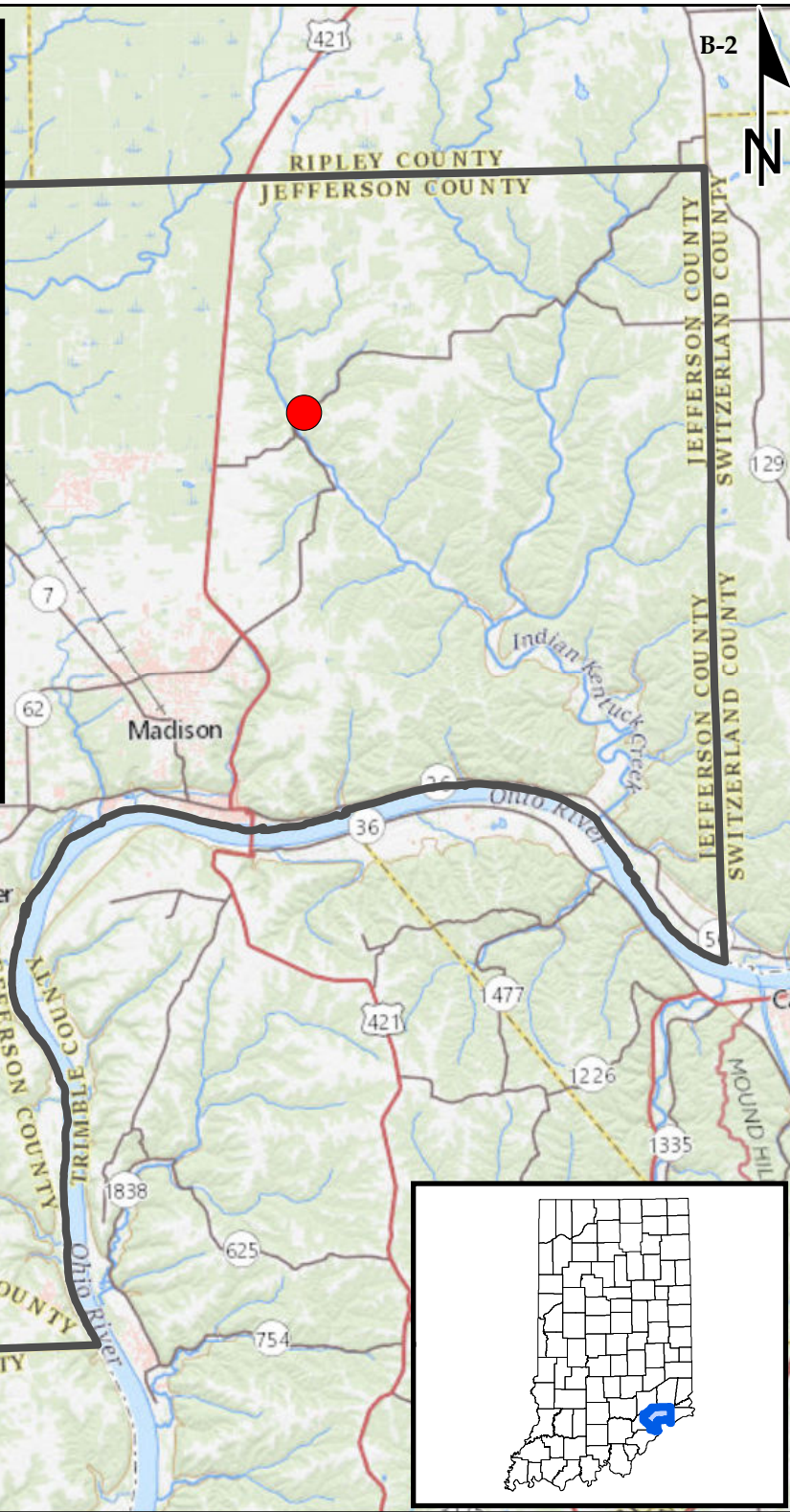
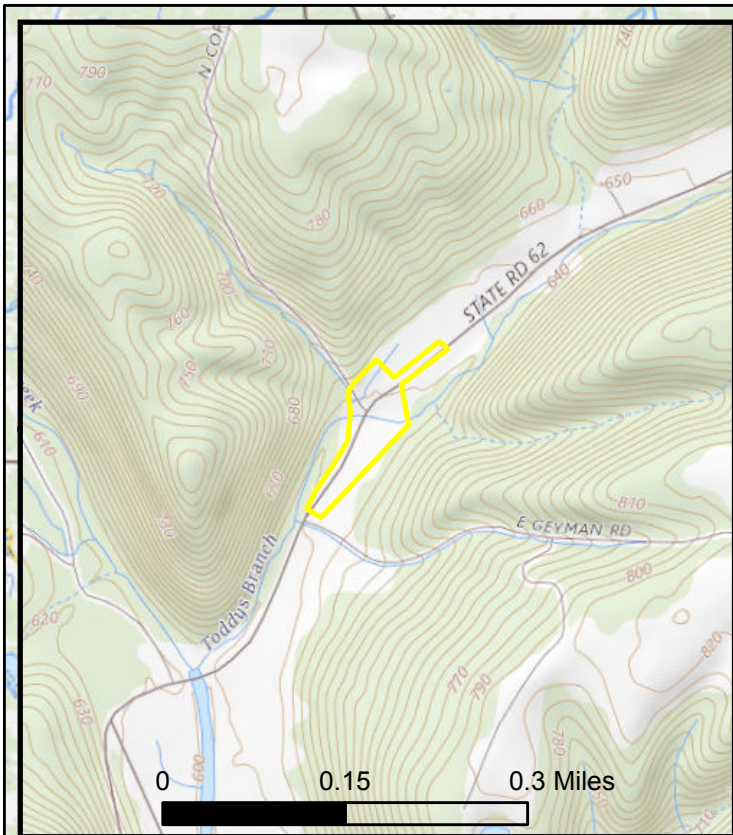
⁶Potential for causing a disproportionately high and adverse impact.

⁷Hot Spot Analysis and/or MSAT Quantitative Emission Analysis.

*Substantial public or agency controversy may require a higher-level NEPA document.

Appendix B

Graphics



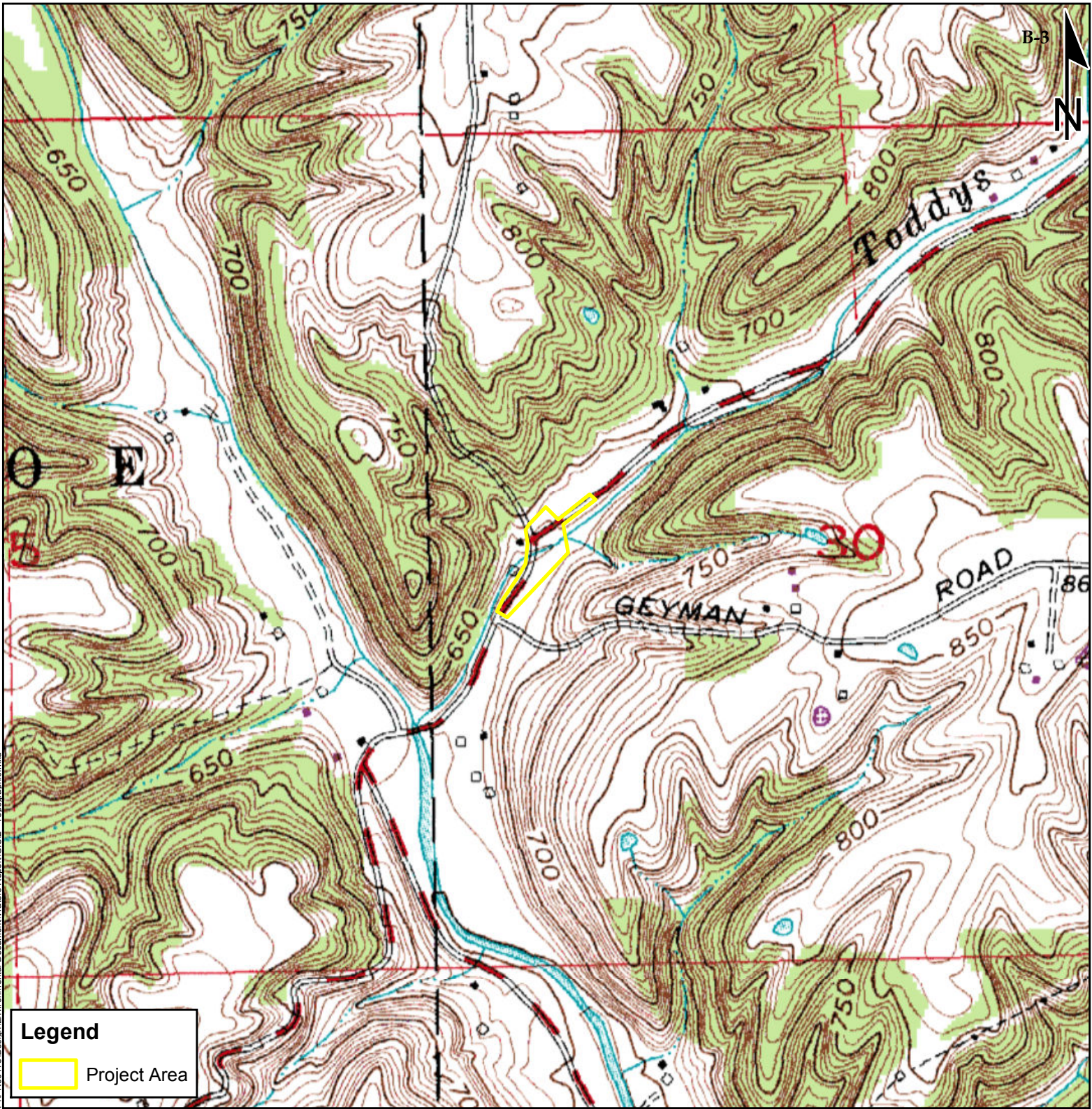
Attachment 1

Indiana Dept. of Transportation (INDOT)
 SR 62 over Toddy's Branch
 Bridge Replacement
 DES NO. 1701457
 Shelby Township, Jefferson County

Project Location Map


Sources:
Non Orthophotography
Data - Obtained from ESRI Online Services
Map Projection: UTM Zone 16 N **Map Datum:** NAD83
 Prepared By: Burgess & Niple

July 2019



Legend

 Project Area

0 375 750 1,500
 Feet

Sources:

Non Orthophotography

Data - Obtained from the State of Indiana Geographical Information Office Library

Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)

Map Projection: UTM Zone 16 N **Map Datum:** NAD83

Prepared By: Burgess & Niple

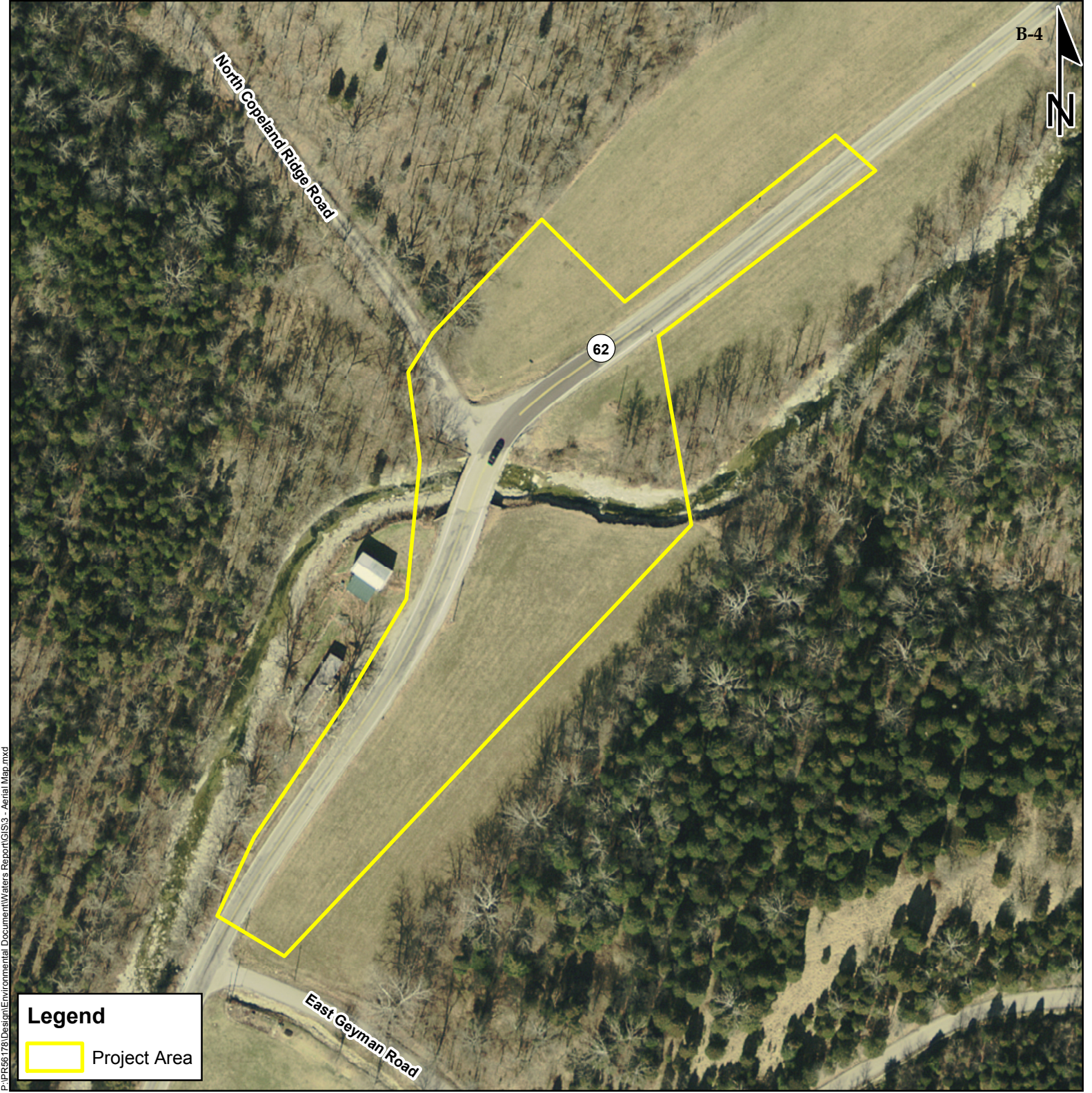
Attachment 2

Indiana Dept. of Transportation (INDOT)
 SR 62 over Toddy's Branch
 Bridge Replacement
 DES NO. 1701457
 Shelby Township, Jefferson County

USGS Topographic Map

July 2019

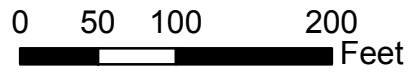
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P:\PR65178\Design\Environmental\Document\Waters_Report\GIS3 - Aerial Map.mxd

Legend

 Project Area



Sources:
Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83
 Prepared By: Burgess & Niple

Attachment 3
 Indiana Dept. of Transportation (INDOT)
 SR 62 over Toddy's Branch
 Bridge Replacement
 DES NO. 1701457
 Shelby Township, Jefferson County
Aerial Map
 July 2019

**INDIANA DEPARTMENT OF TRANSPORTATION (INDOT)
S.R. 62 OVER TODDY'S BRANCH IN JEFFERSON COUNTY, INDIANA
BRIDGE REPLACEMENT
DES. NO.: 1701457
BRIDGE ID #: 062-039-05946 B
SITE PHOTOGRAPHS
JULY 9, 2019**



Photo 1: Looking south at SR 62 bridge over downstream side of Toddy's Branch.



Photo 2: Looking northeast mowed agricultural/pasture field adjacent to SR 62.



Photo 3: Looking northeast mowed agricultural/pasture field adjacent to SR 62.



Photo 4: Looking south at SR 62 bridge over upstream side of Toddy's Branch.



Photo 5: Looking northwest upstream of UNT-1. Note log jams, steep banks.



Photo 6: Looking southeast downstream of UNT-1 towards the confluence with Toddy's Branch just below SR 62 bridge.

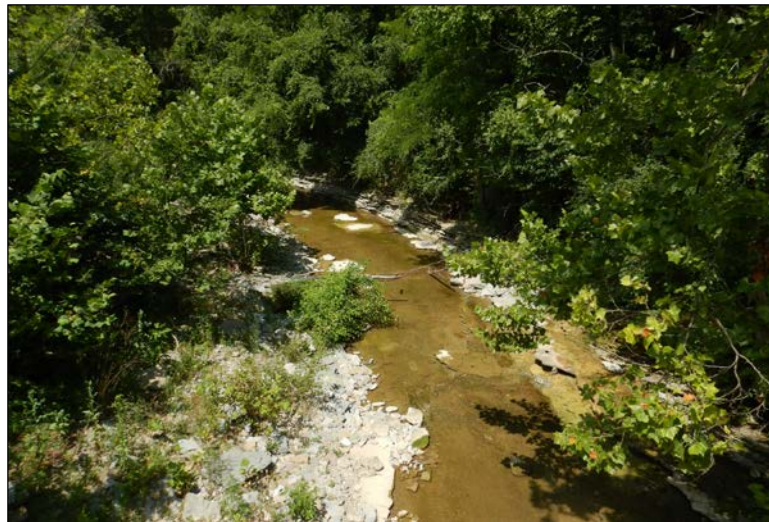


Photo 7: Looking downstream of Toddy's Branch from the Bridge carry SR 62. Note Bedrock dominated substrate.



Photo 8: Looking upstream of Toddy's Branch from the Bridge carrying SR 62. Note the bedrock dominated substrate and severely eroded river left bank.



Photo 9: Looking northeast from the southeast side of SR 62 bridge.



Photo 10: Looking southwest along SR 62 into a mowed agricultural/pasture field.



Photo 11: Looking toward SR 62 bridge from the upstream side of Toddy's Branch. Note bedrock dominated substrate.



Photo 12: Looking downstream of SR 62 bridge and out of the project area. Note river right bank erosion and bedrock.



Photo 13: Looking downstream on Toddy's Branch toward SR 62. Note severely eroded bank on the river left.



Photo 14: Looking upstream on Toddy's Branch just east of SR 62 at the severe bank erosion.



Photo 15: Looking upstream on Toddy's Branch at SR 62 bridge.



Photo 16: Looking north at UNT-1 and the confluence with Toddy's Branch.



Photo 17: Looking northeast at the narrow riparian buffer between Toddy's Branch and the mowed field. Dominated by Sycamore trees.



Photo 18: Looking at Wetland 1 Soil Data Plot 1.



Photo 19: Hydric soil present within Wetland 1.



Photo 20: Upland data plot of Wetland 1, Soil Data Plot 2.



Photo 21: Soil Data Plot 2. No Hydric soil present.



Photo 22: Looking east through wetland 1.



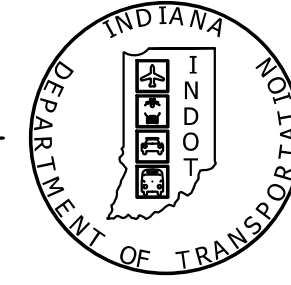
Photo 23: Looking west through Wetland 1.



Photo 24: Looking northeast through mowed field east of SR. 62.

PROJECT	DESIGNATION
1602259	1701457
CONTRACT	BRIDGE FILE
B-40421	062-39-10270

INDIANA DEPARTMENT OF TRANSPORTATION



BRIDGE PLANS

FOR SPANS OVER 20 FEET

ROUTE: SR 62
PROJECT NO.

AT: RP 201+51
1701457 P.E.
1701457 R/W
1701457 CONST.

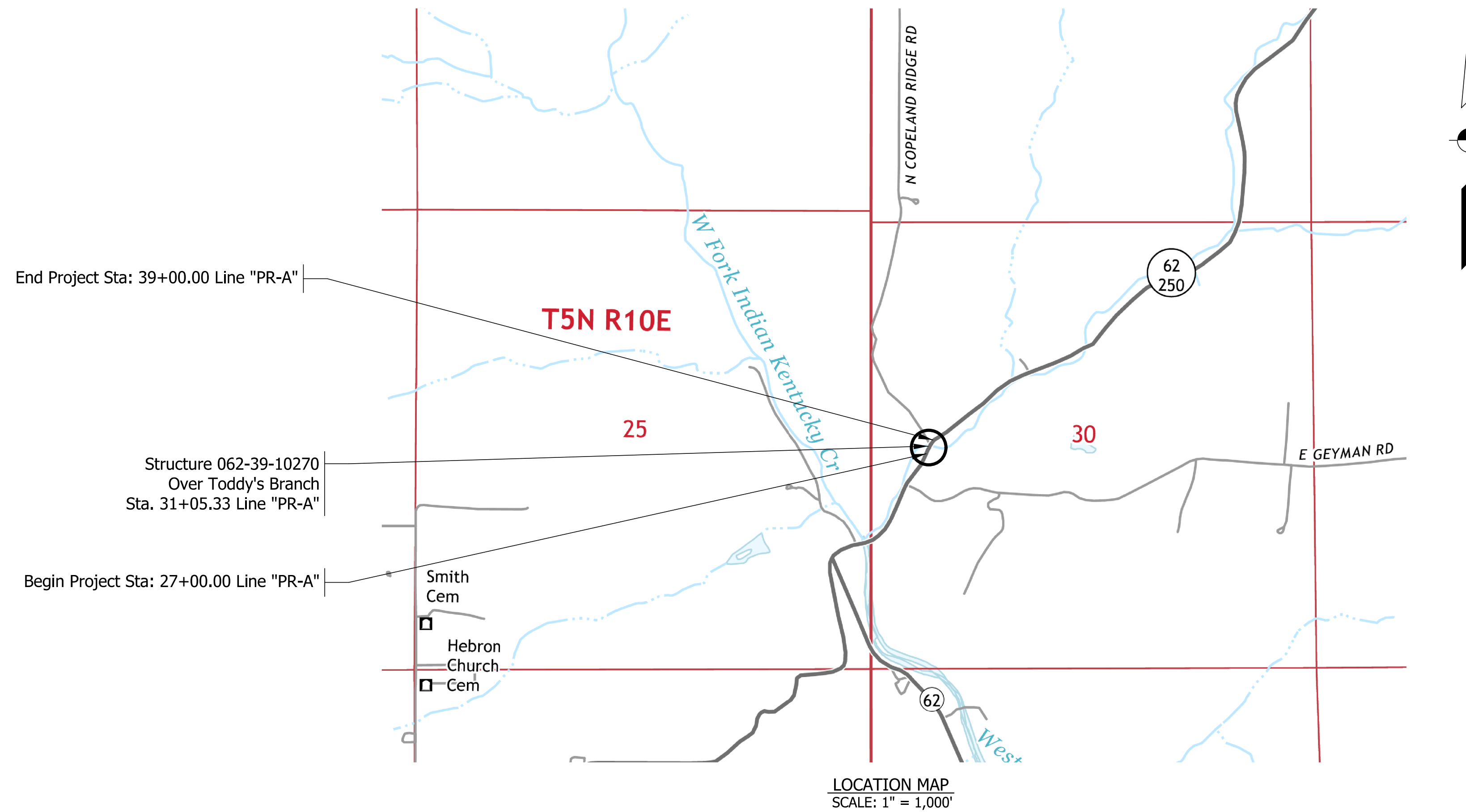
STRUCTURE INFORMATION

STRUCTURE	Des. #	TYPE	SPAN AND SKEW	OVER	STATION
062-39-10270	1701457	Simple Span Prestressed Concrete I-Beam Type II Bridge	59'-10" Skew 31°54'33" Lt.	Toddy's Branch	31+05.33 Line "PR-A"

KIN PROJECT INFORMATION

1602259	SR 62 over Toddy's Branch - Rehab
1700005	SR 62 over Salem Branch
1701455	SR 62 over East Fork Indian-Kentuck Creek (Lead Des #)
1701457	SR 62 over Toddy's Branch - Replace

Bridge Replacement on SR 62 over Toddy's Branch
Located 0.35 Miles East of SR 250 Intersection, West Junction
Section 30, T-5-N, R-11-E, Shelby Township, Jefferson County

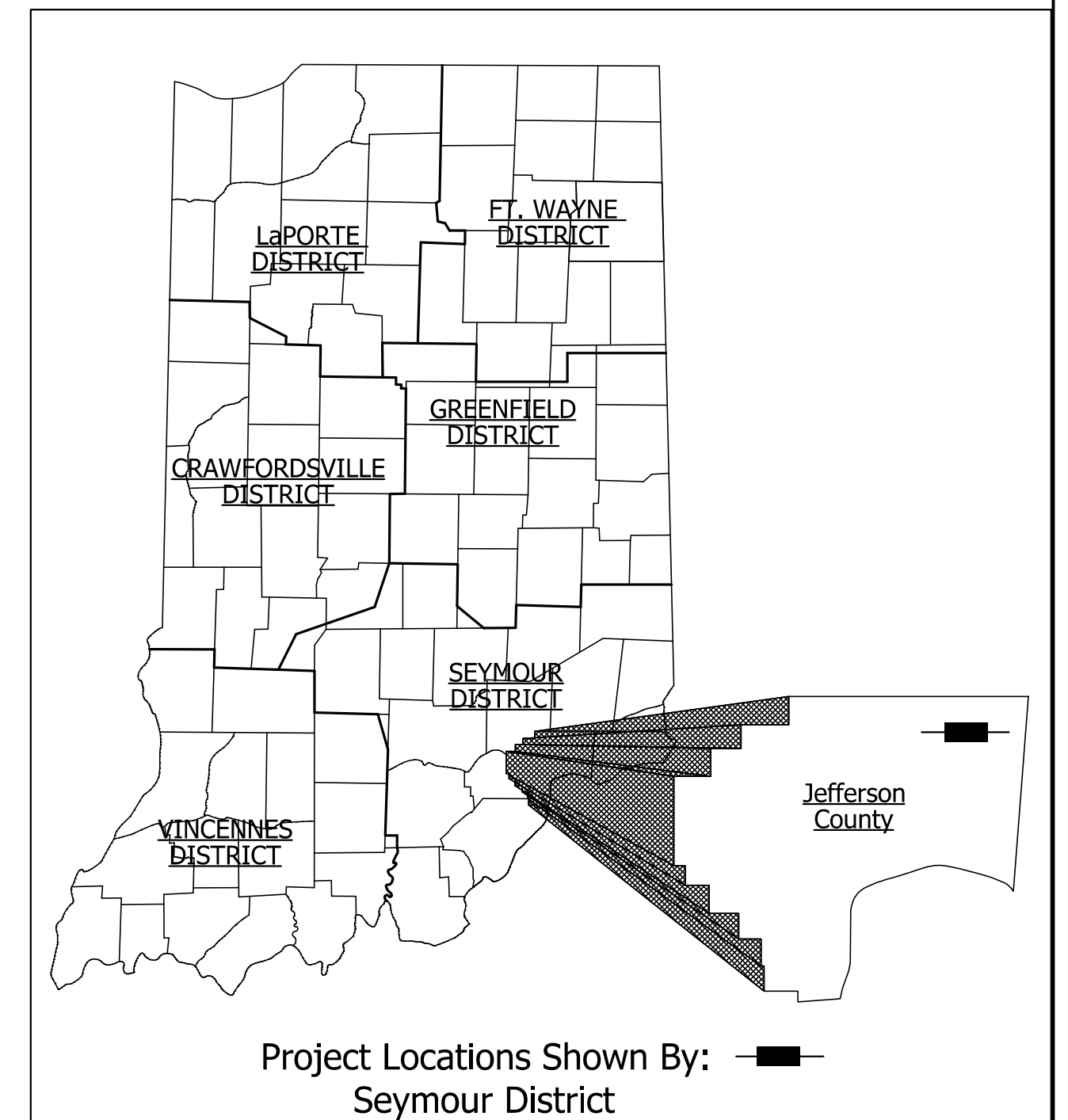


TRAFFIC DATA

A.A.D.T. (2022)	1,242 VPD
A.A.D.T. (2042)	1,415 VPD
D.H.V. (2042)	100 V.P.H.
DIRECTIONAL DISTRIBUTION	46 %
TRUCKS	14.35% A.A.D.T.
	16.47% DHV

DESIGN DATA

DESIGN SPEED	45 mph SR 62, 30 mph N Copeland Ridge Rd.
PROJECT DESIGN CRITERIA	3R (NON-FREEWAY)
FUNCTION CLASSIFICATION	Major Collector
RURAL/URBAN	Rural
TERRAIN	Rolling
ACCESS CONTROL	None



LATITUDE: 38° 50' 50.42" N LONGITUDE: 85° 20' 55.24" W

BRIDGE LENGTH: 0.012 MI.
ROADWAY LENGTH: 0.218 MI.
TOTAL LENGTH: 0.230 MI.
MAX GRADE: 2.85%

HUC_12 051401010204

INDIANA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS DATED 2020 TO BE USED WITH THESE PLANS

BURGESS & NIPL, INC.
251 N. ILLINOIS ST.
CAPITOL CENTER SUITE 920
INDIANAPOLIS, IN 46221
PHONE (317) 237-2760
FAX (317) 237-2755

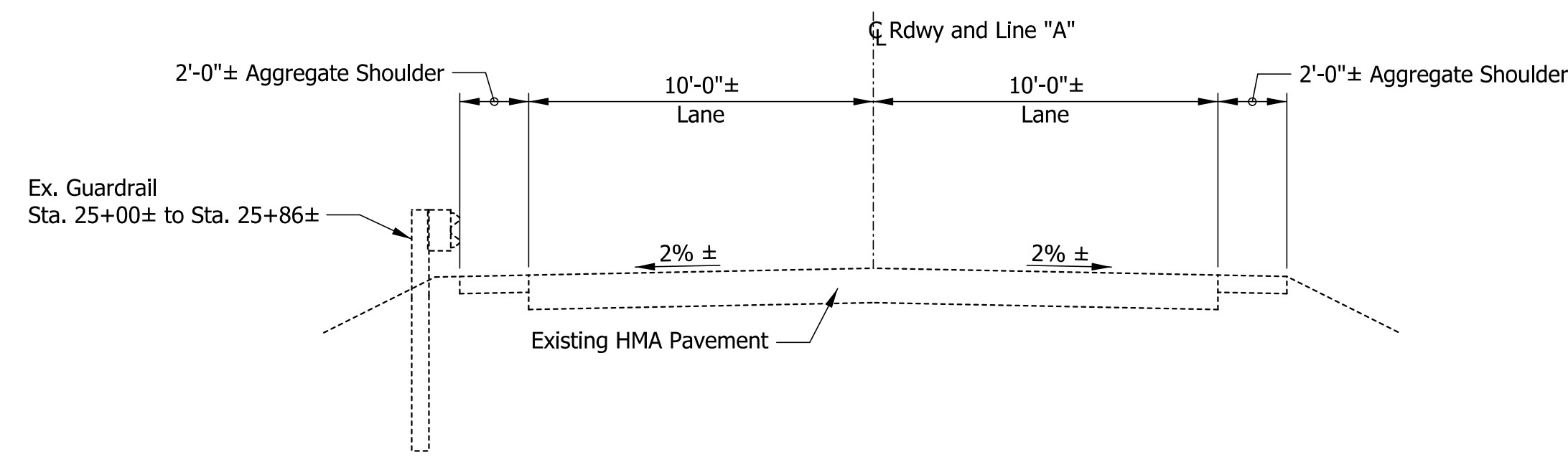
STAGE 2 PLANS SUBMITTAL
May 15, 2020

PLANS PREPARED BY:	BURGESS & NIPL, INC.	(317) 237-2760
		PHONE NUMBER
PREPARED BY:		DATE
APPROVED FOR LETTING:		DATE
	INDIANA DEPARTMENT OF TRANSPORTATION	

BRIDGE FILE	
062-39-10270	
DESIGNATION	
1701457	
DWG. NO.	SHEET NO.
TI-01	1 of 29
CONTRACT	PROJECT
B-40421	1701457

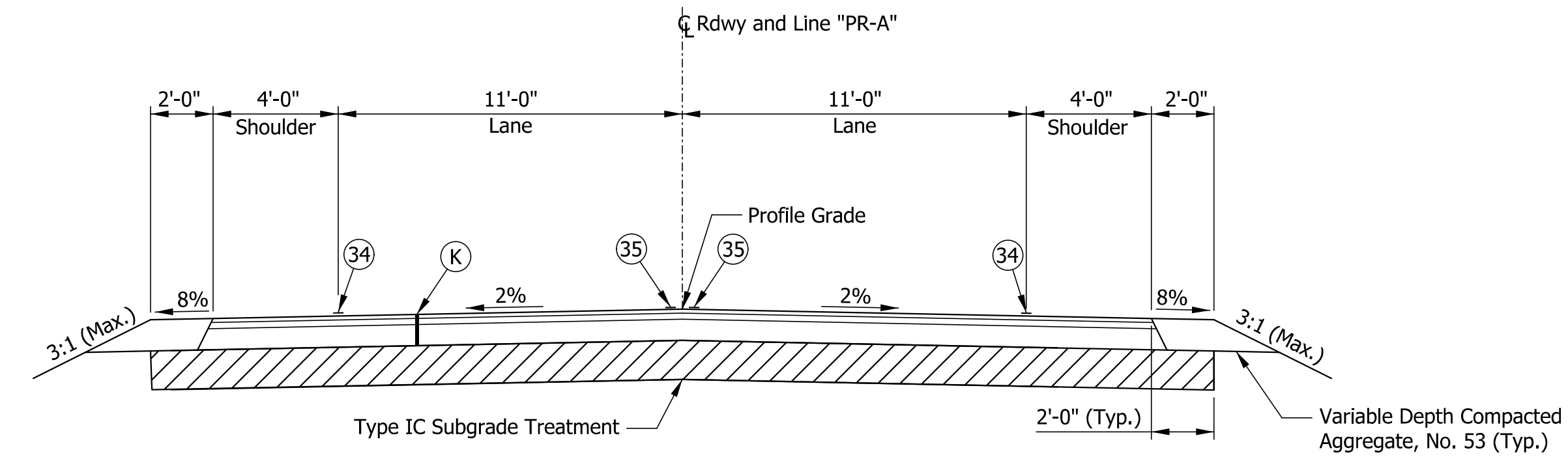
LEGEND

- ③④ Line, Paint, Solid, White, 4"
- ③⑤ Line, Paint, Solid, Yellow, 4"
- (K) xxx lb/syd, QC/QA-HMA, 2, 64, Surface, 9.5 mm on xxx lb/syd, QC/QA-HMA, 2, 64, Intermediate, 19.0 mm on x" Compacted Aggregate No. 53, Base



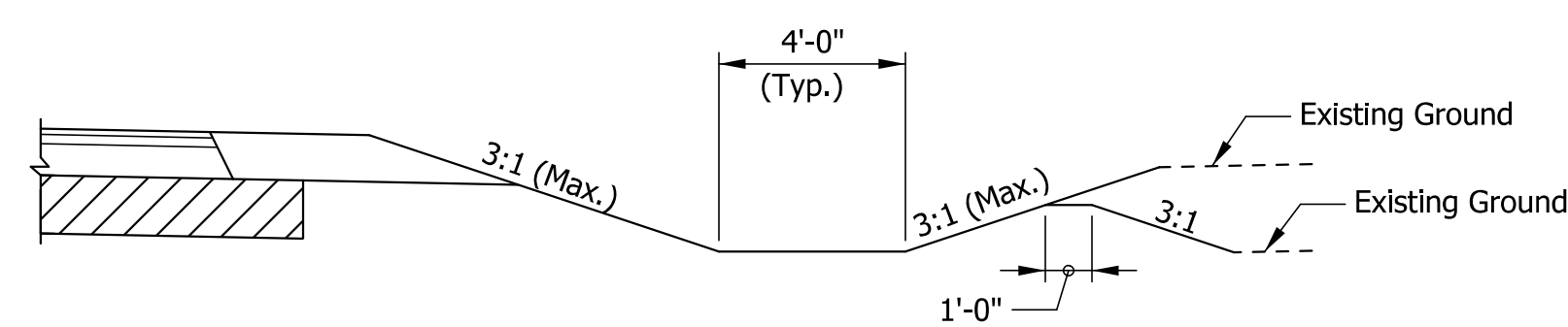
EXISTING SECTION - SR 62

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 Sta. 26+00.00 to Sta. 27+00.00 Line "PR-A"
 Sta. 38+10.00 to Sta. 38+50.00 Line "PR-A"



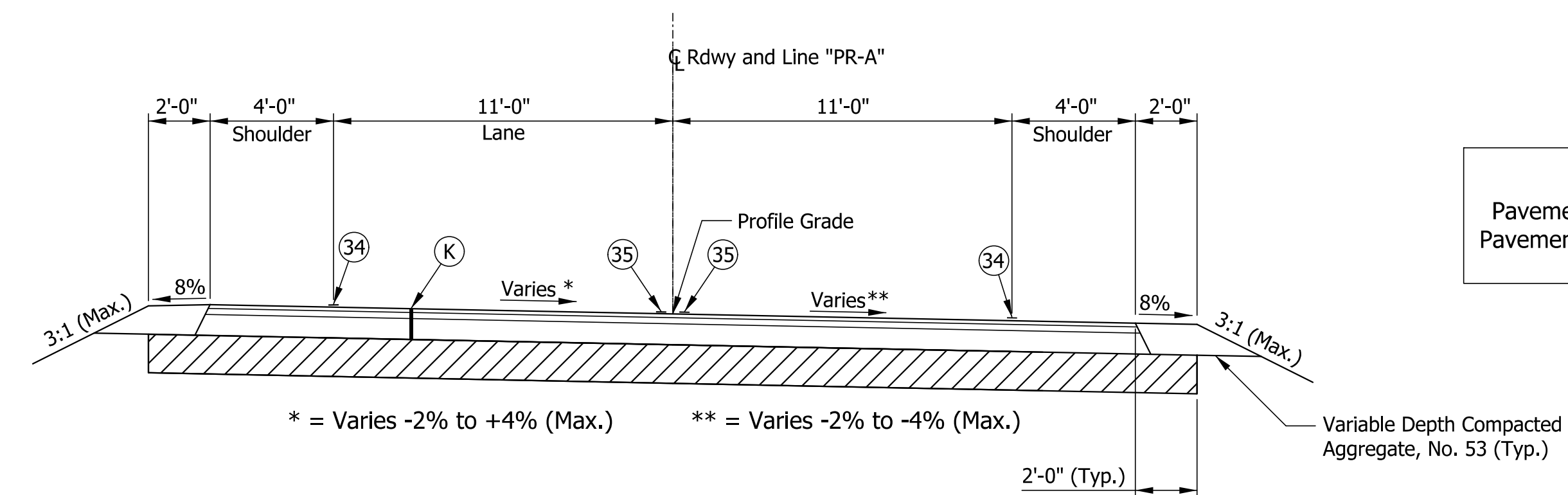
PROPOSED TYPICAL SECTION

Scale: 1/4" = 1'-0"
 Sta. 27+00.00 to Sta. 30+83.99 Line "PR-A"
 Sta. 37+77.42 to Sta. 38+10.00 Line "PR-A"



TYPICAL DITCH SECTION

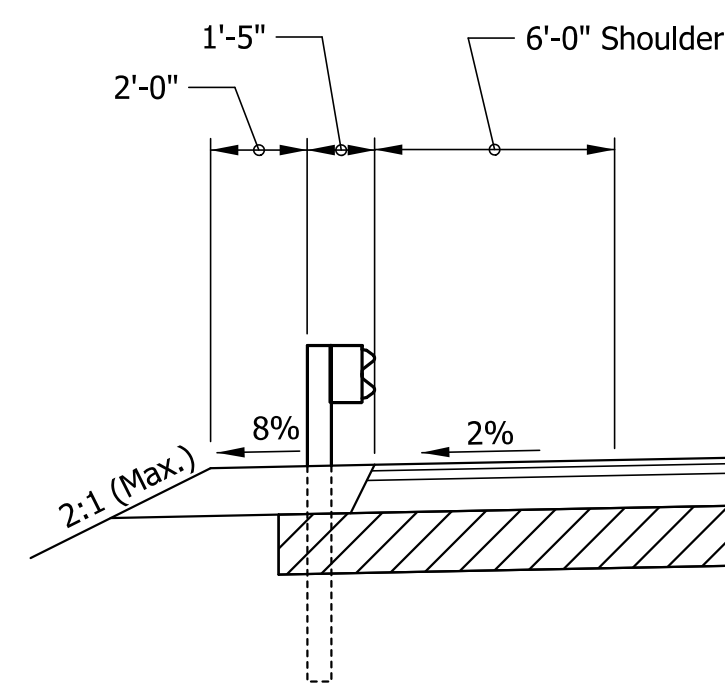
Scale: 1/4" = 1'-0"
 Sta. 27+75.00 to Sta. 28+42.00 Left
 Sta. 29+27.00 to Sta. 30+82.00 Left
 Sta. 31+93.00 to Sta. 37+00.00 Left
 Sta. 27+00.00 to Sta. 30+82.17 Right
 Line "PR-A"



PROPOSED TYPICAL SECTION

Scale: 1/4" = 1'-0"
 Sta. 30+83.99 to Sta. 37+77.42 Line "PR-A"
 Paving Exception Sta. 30+37.31 to Sta. 31+70.34 Line "PR-A"

NOTE TO REVIEWER
 Pavement Design will be requested after Stage 2.
 Pavement Legend will be completed when received.

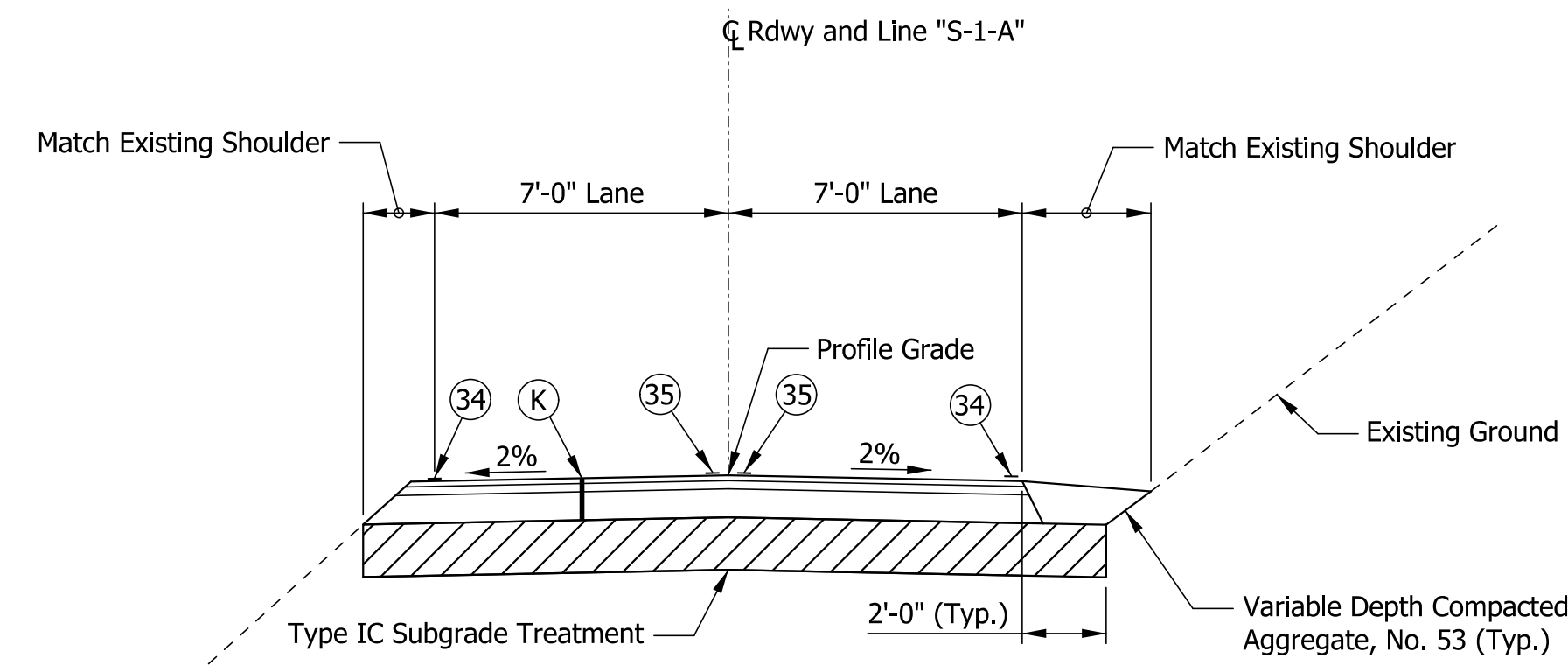


GUARDRAIL DETAIL

Scale: 1/4" = 1'-0"
 Sta. 29+35.54 to Sta. 30+40.56 Left
 Sta. 31+48.22 to Sta. 31+97.93 Left
 Sta. 29+58.22 to Sta. 30+63.24 Right
 Sta. 31+67.08 to Sta. 32+72.74 Right
 Line "PR-A"

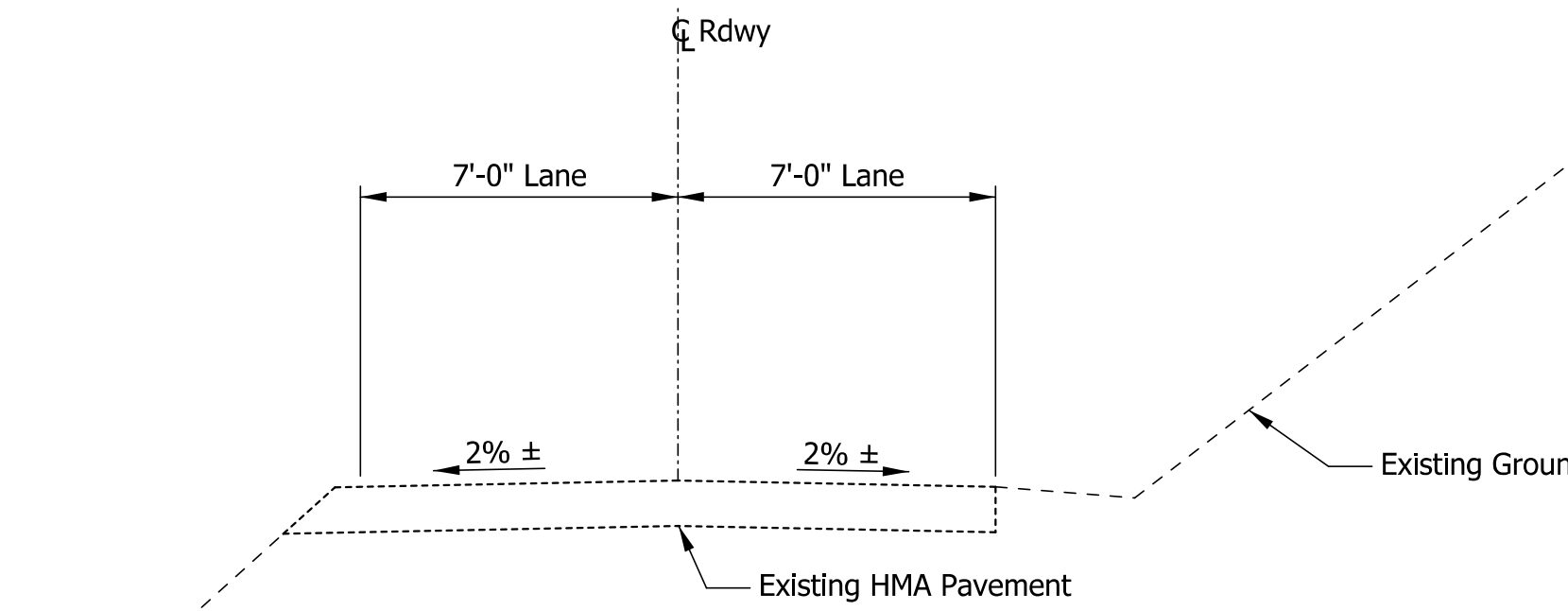
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RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER DATE	INDIANA DEPARTMENT OF TRANSPORTATION	HORIZONTAL SCALE As Noted BRIDGE FILE 062-39-10270	
		VERTICAL SCALE As Noted DESIGNATION 1701457	
DESIGNED: SJA	DRAWN: JDG	SURVEY BOOK	DWG. NO. TS-01
CHECKED: XAC	CHECKED: SJA	PROJECT	SHEET NO. 3 of 29
SR 62 OVER TODDY'S BRANCH TYPICAL SECTIONS		B-40421	1701457



TYPICAL SECTION - N COPELAND RIDGE ROAD

Scale: 1/4" = 1'-0"
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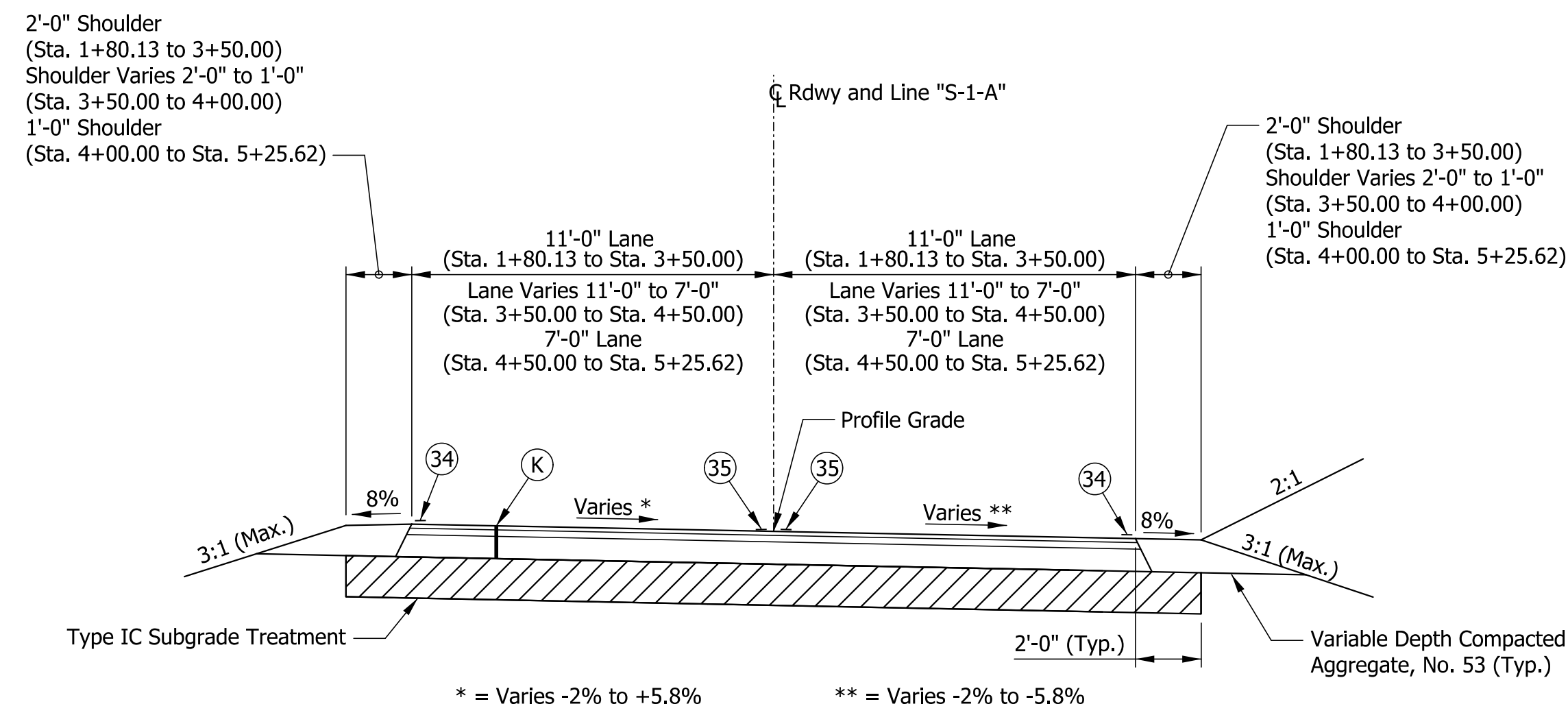


EXISTING SECTION - N COPELAND RIDGE ROAD

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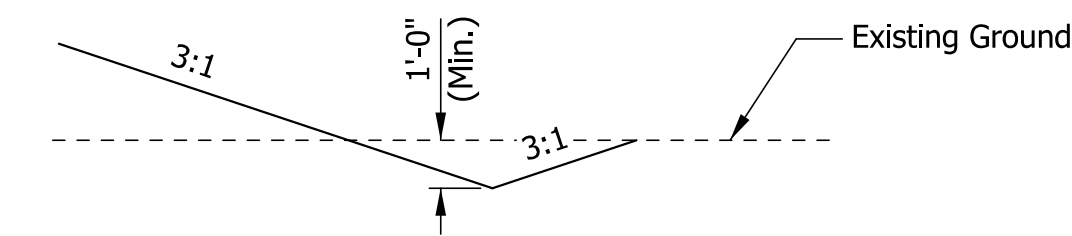
- LEGEND**
- ③④ Line, Paint, Solid, White, 4"
 - ③⑤ Line, Paint, Solid, Yellow, 4"
 - (K) xxx lb/syd, QC/QA-HMA, 2, 64, Surface, 9.5 mm on xxx lb/syd, QC/QA-HMA, 2, 64, Intermediate, 19.0 mm on x" Compacted Aggregate No. 53, Base

NOTE TO REVIEWER
Pavement Design has been requested.
Pavement Legend will be completed when received.



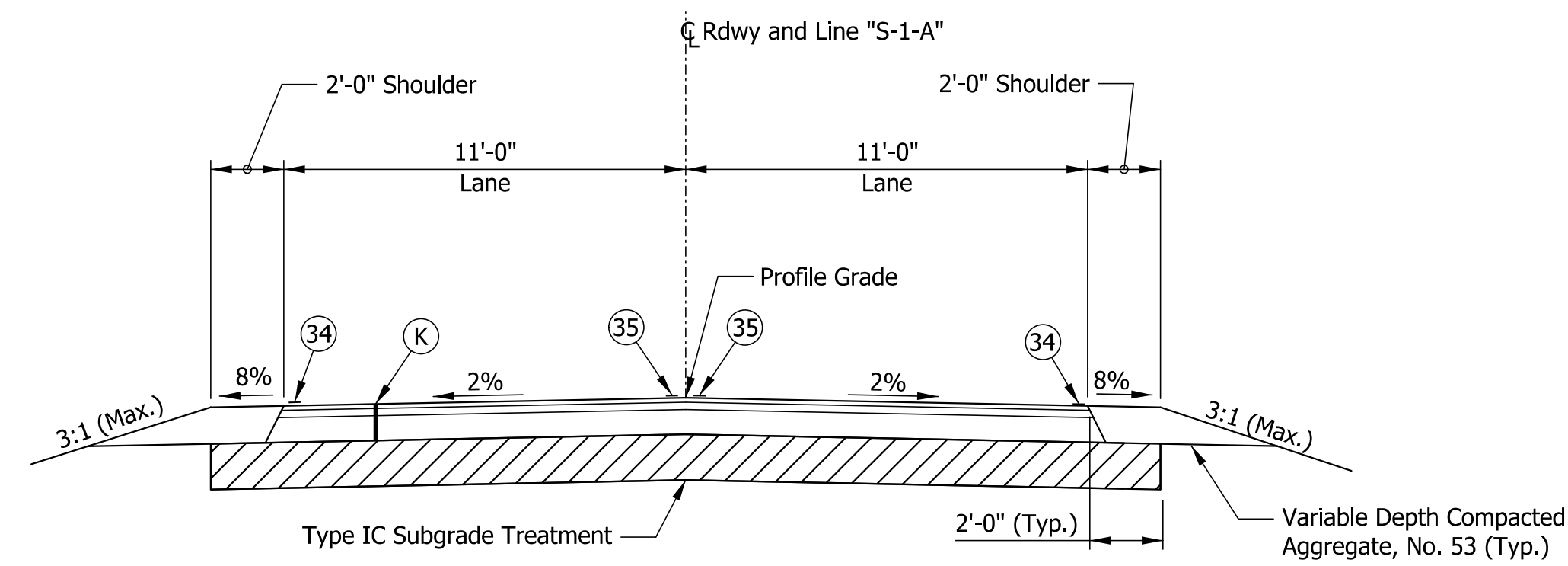
TYPICAL SECTION - N COPELAND RIDGE ROAD

Scale: 1/4" = 1'-0"
Sta. 1+80.13 to Sta. 5+25.62 Line "S-1-A"



TYPICAL DITCH SECTION

Scale: 1/4" = 1'-0"
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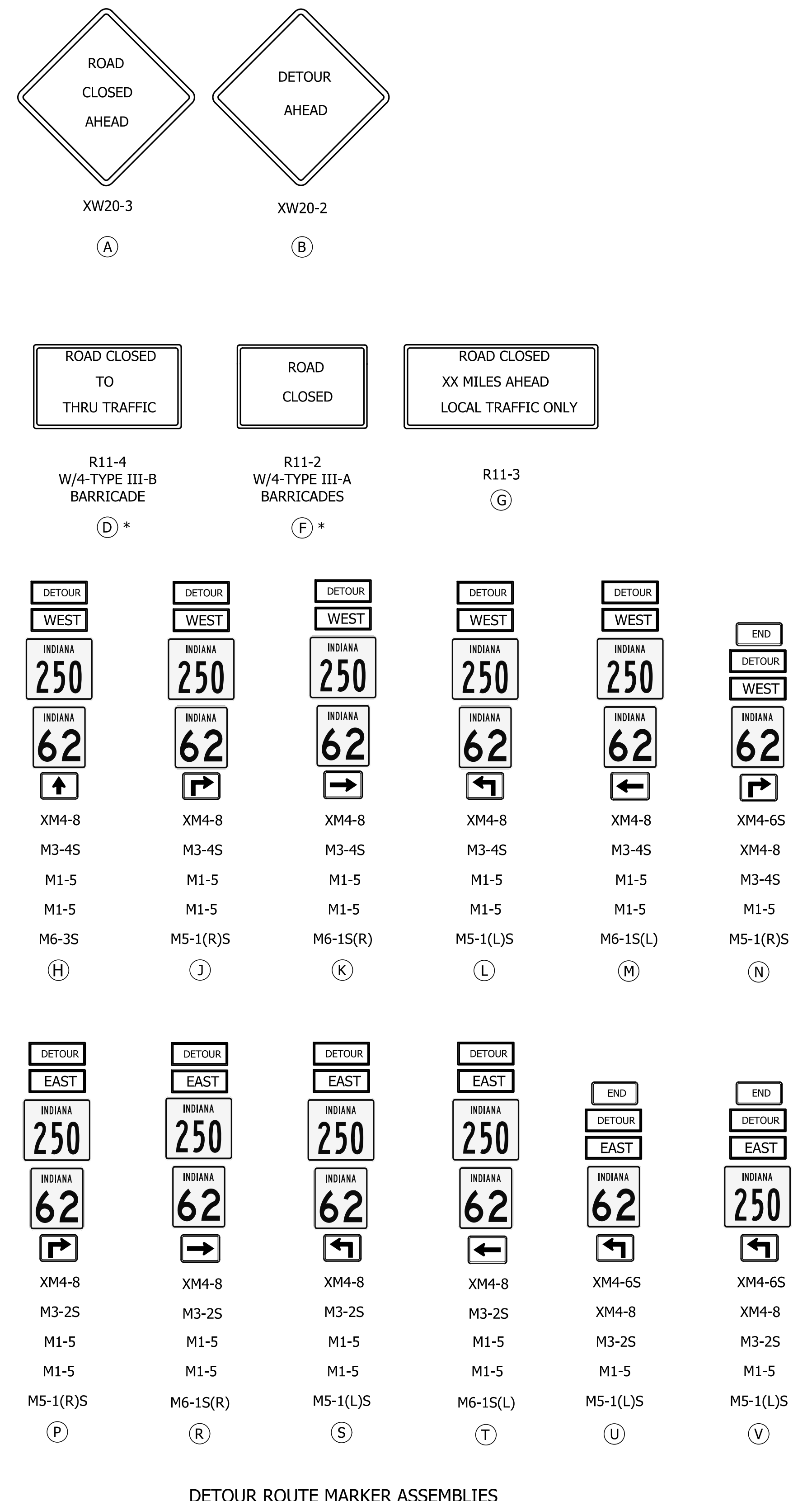
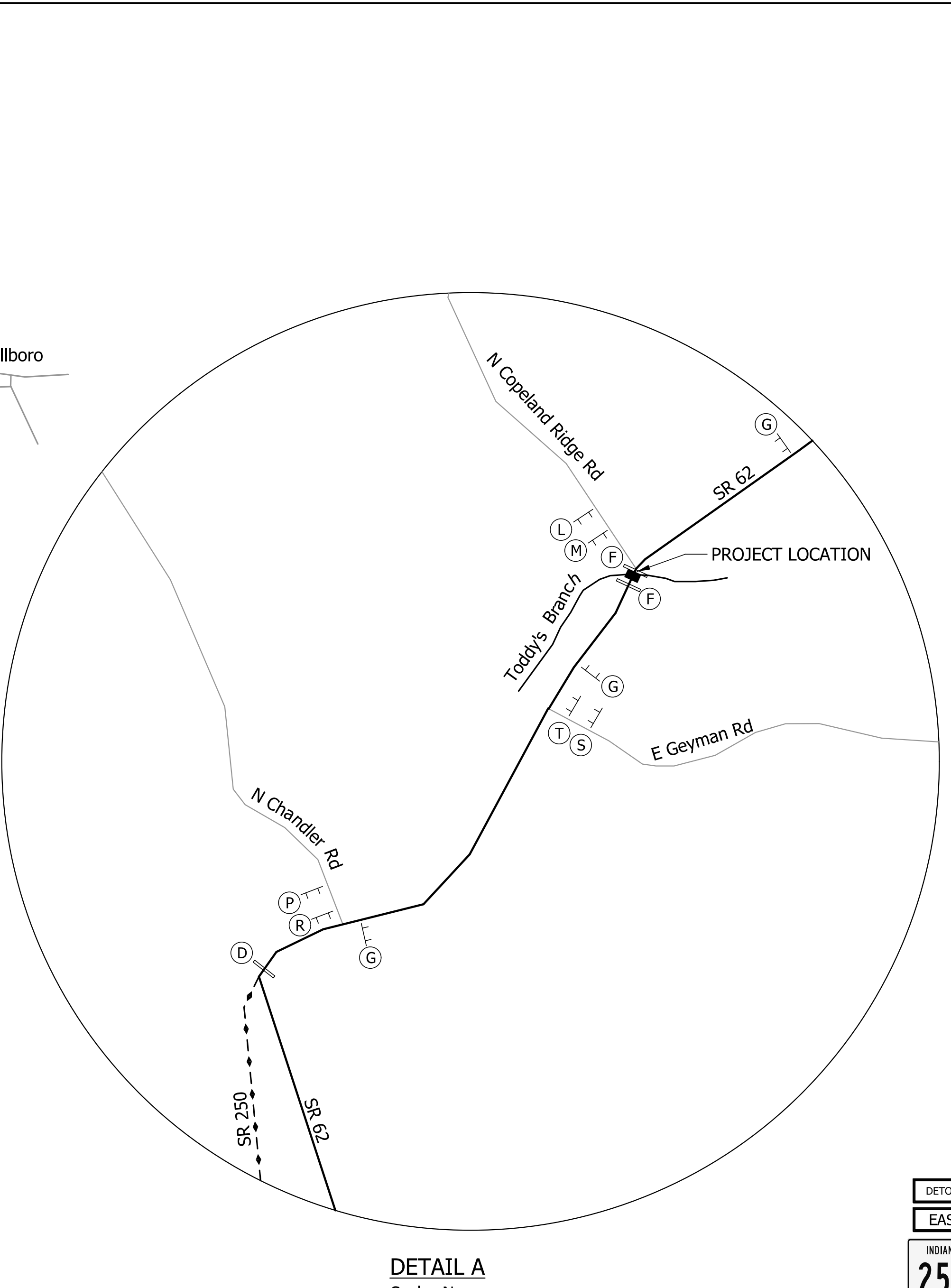
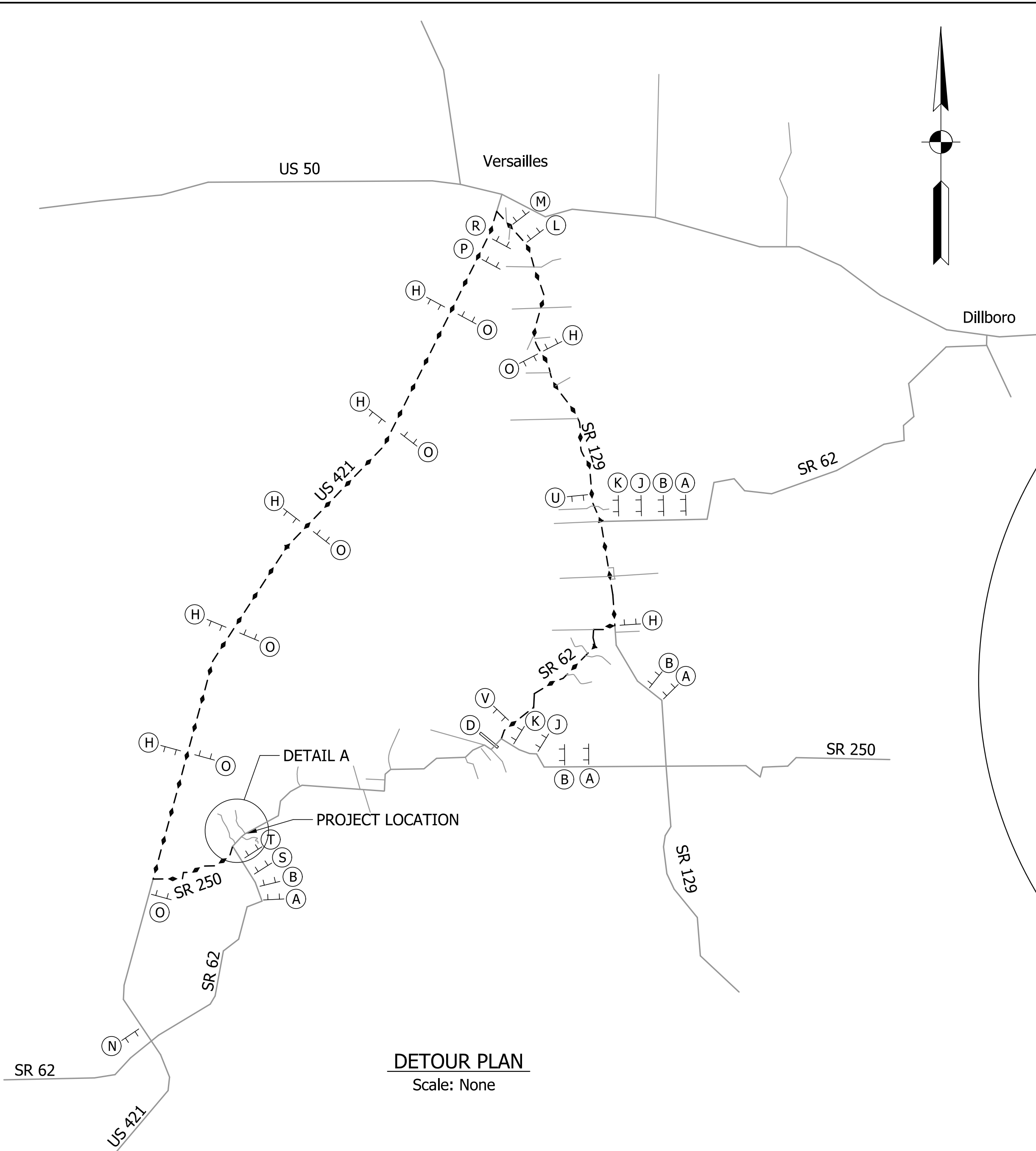


TYPICAL SECTION - N COPELAND RIDGE ROAD

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RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER DATE	INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE		BRIDGE FILE		
			As Noted		062-39-10270		
DESIGNED: MAK	DRAWN: JDG	SR 62 OVER TODDY'S BRANCH TYPICAL SECTIONS		VERTICAL SCALE		DESIGNATION	
CHECKED: SJA	CHECKED: SJA			As Noted		1701457	
				SURVEY BOOK		DWG. NO.	
						TS-02	
				CONTRACT		PROJECT	
				B-40421		1701457	



NOTES:
 See Indiana Manual on Uniform Traffic Control Devices and Standard Drawings for sign details and spacings.
 Contractor shall give residence and lessors on the closed road 24 hour access to drives and field entrances.
 *-Signs "D" & "F" to be paid for as Road Closure Sign Assemblies.

SIGN SUMMARY

** CONSTRUCTION SIGN TYPE "A"	13 EA.
DETOUR ROUTE MARKER ASSEMBLY	34 EA.
ROAD CLOSURE SIGN ASSEMBLY	4 EA.
BARRICADE, TYPE III-A	96 LFT.
BARRICADE, TYPE III-B	96 LFT.

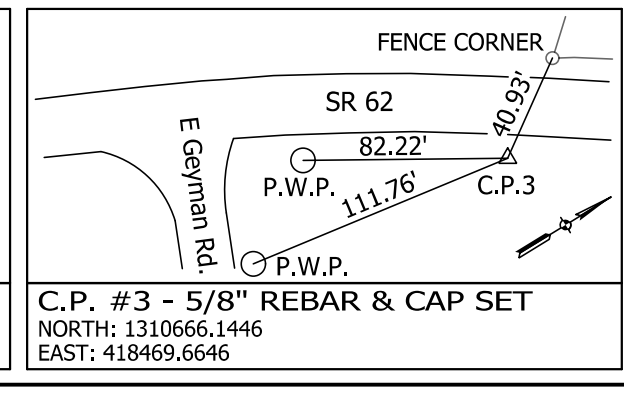
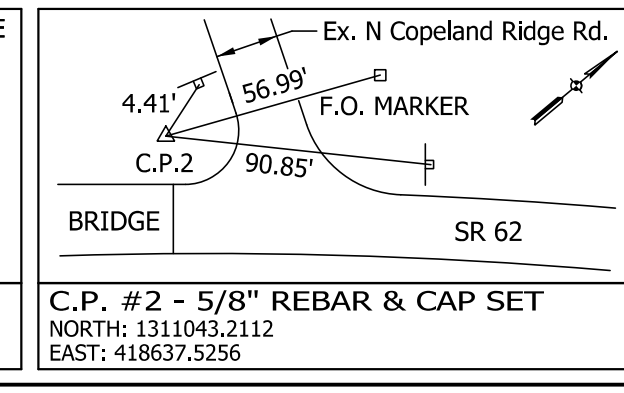
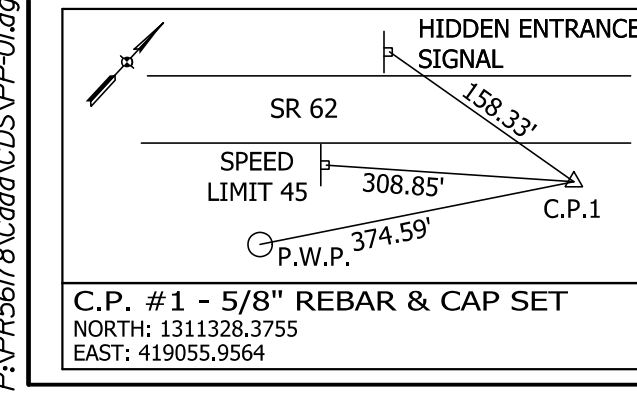
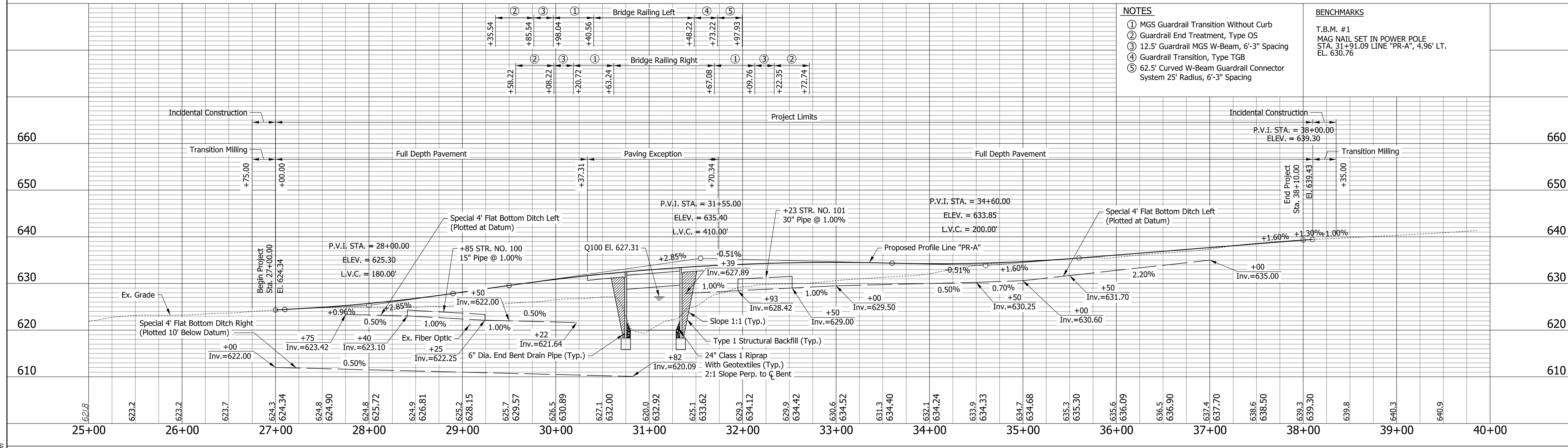
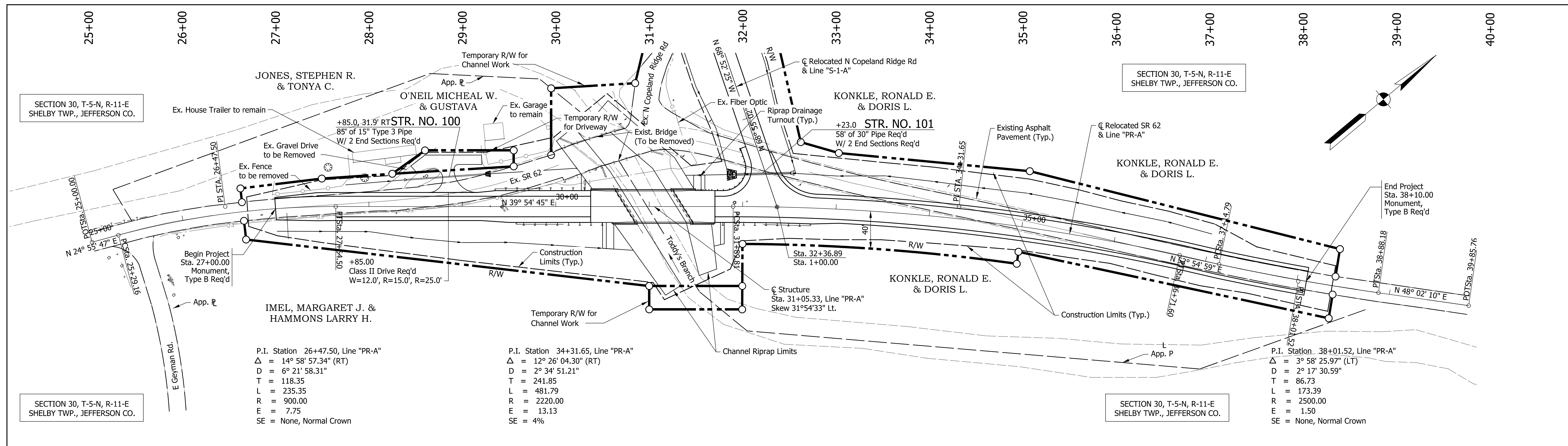
** INCLUDES 2 XG20-5 ROUTE CLOSURE NOTICE SIGNS (LOCATION TO BE DETERMINED BY PROJECT ENGINEER IN THE FIELD)

LEGEND:

- █ Closed Bridge Location
- Barricade
- TT Sign Assembly
- - - - Detour Route

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RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER DATE	INDIANA DEPARTMENT OF TRANSPORTATION SR 62 OVER TODDY'S BRANCH DETOUR PLAN	HORIZONTAL SCALE As Noted	BRIDGE FILE 062-39-10270
DESIGNED: SJA DRAWN: JDG		VERTICAL SCALE As Noted	DESIGNATION 1701457
CHECKED: XAC CHECKED: STB		SURVEY BOOK	DWG. NO. MT-01
		CONTRACT B-40421	PROJECT 1701457



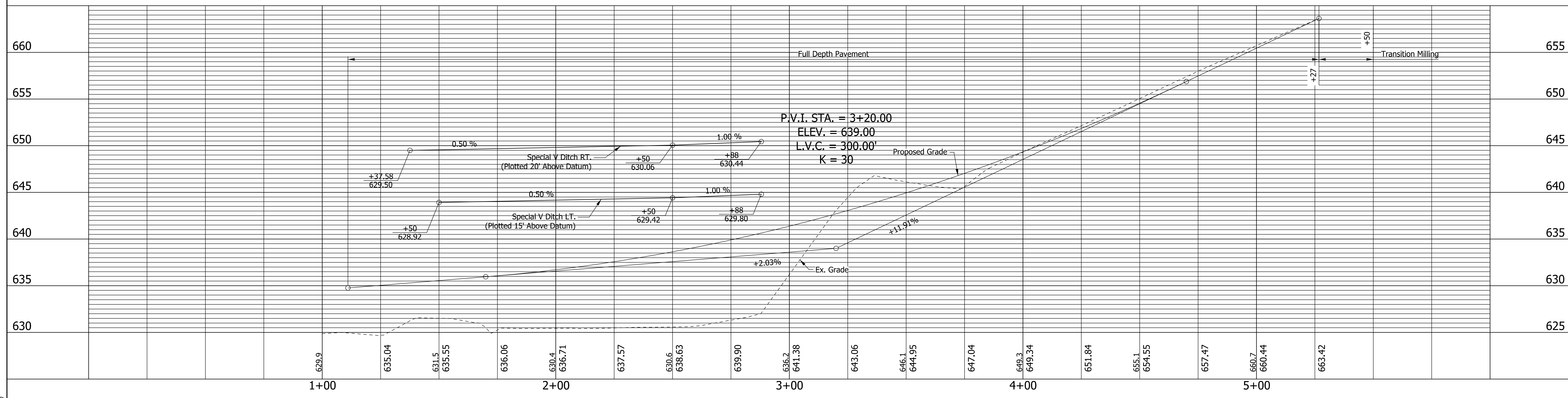
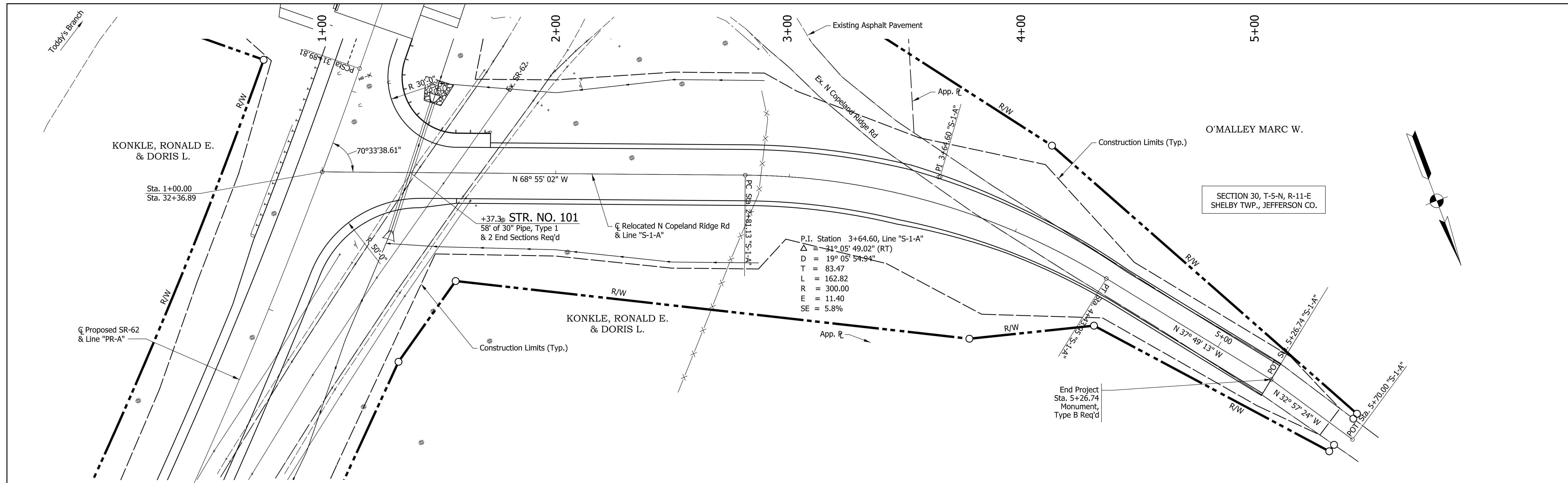
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: SJA	DRAWN: JDG	
CHECKED: XAC	CHECKED: SJA	

INDIANA DEPARTMENT OF TRANSPORTATION

SR 62 OVER TODDY'S BRANCH PLAN AND PROFILE - 1

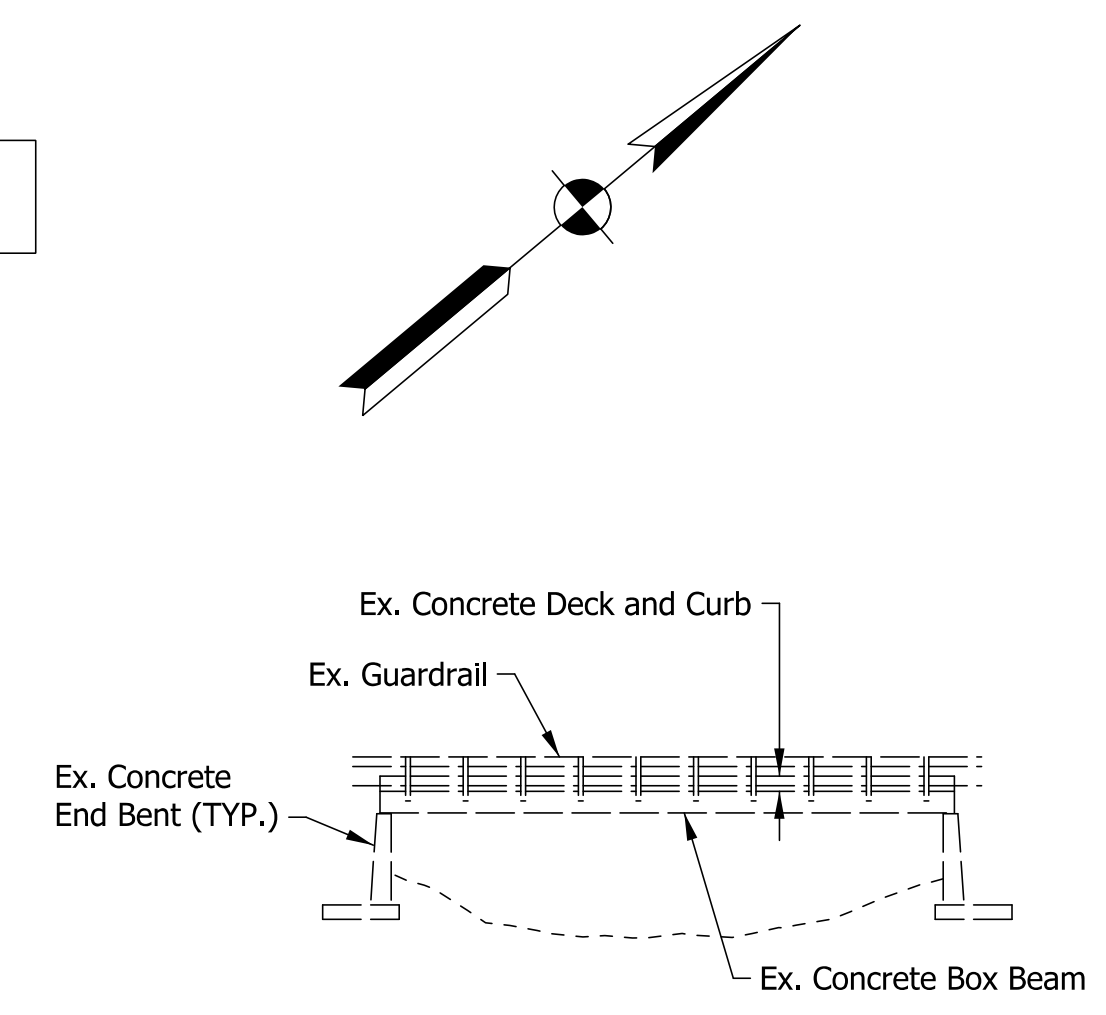
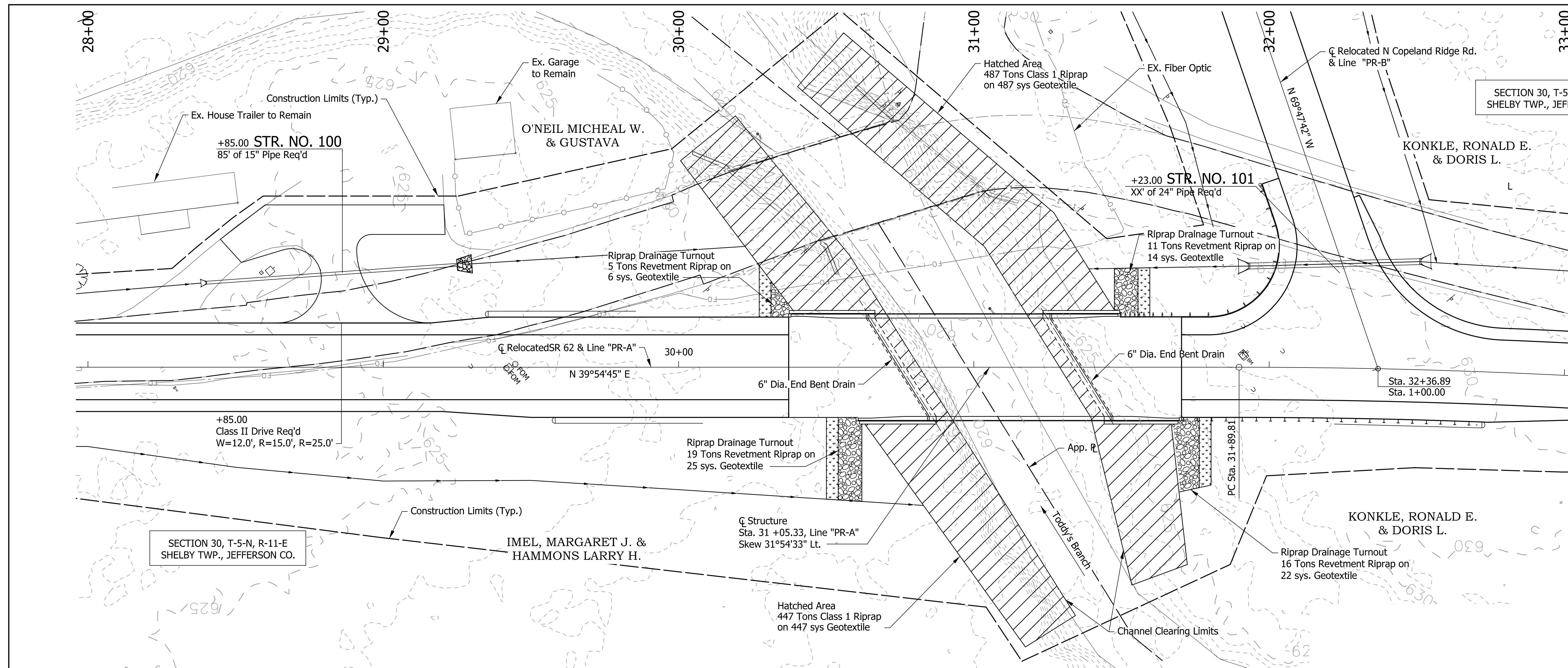
HORIZONTAL SCALE 1" = 50'	BRIDGE FILE 062-39-10270
VERTICAL SCALE 1" = 10'	DESIGNATION 1701457
SURVEY BOOK	DWG. NO. SHEET NO. PP-01 9 of 29
CONTRACT B-40421	PROJECT 1701457

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RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER DATE	INDIANA DEPARTMENT OF TRANSPORTATION	HORIZONTAL SCALE 1" = 20' VERTICAL SCALE 1" = 5'	BRIDGE FILE 062-39-10270 DESIGNATION 1701457
DESIGNED: MAK DRAWN: MAK CHECKED: JLD CHECKED: JLD	SR 62 OVER TODDY'S BRANCH PLAN AND PROFILE - 2	SURVEY BOOK CONTRACT B-40421	DWG. NO. SHEET NO. PP-02 10 of 29 PROJECT 1701457



EXISTING STRUCTURE

The existing structure is a single span concrete box beam bridge with a reinforced concrete deck; built in 1968;
 Span: 58'-6"
 Clear roadway: 31'-9"
 Substructures: End Bents have concrete spread footings.
 Existing structure to be removed.

HYDRAULIC DATA

Drainage Area	4.05 sq. mile
Q100 Discharge	2,600 cfs
Q100 Elevation	627.31 ft
Backwater at Q100	-0.69 ft
Velocity at Q100	5.90 ft/sec
Waterway Opening Required	413.85 sft
Waterway Opening provided	440.80 sft
Min. Low Structure Elevation Provided	628.29 ft
Skew	31°54'33"
Q100 Scour Elevation	617.74 ft
Q500 Scour Elevation	617.74 ft
Road Overflow Waterway Area	0 sft
Existing Road Overflow Waterway Area	0 sft
Existing Waterway Opening	341.70 sft
Existing Low Structure Elevation	628.77 ft
Existing Backwater	0.03 ft
Existing Road Overflow Area	0 sft
Flow Line Elevation	620.00 ft

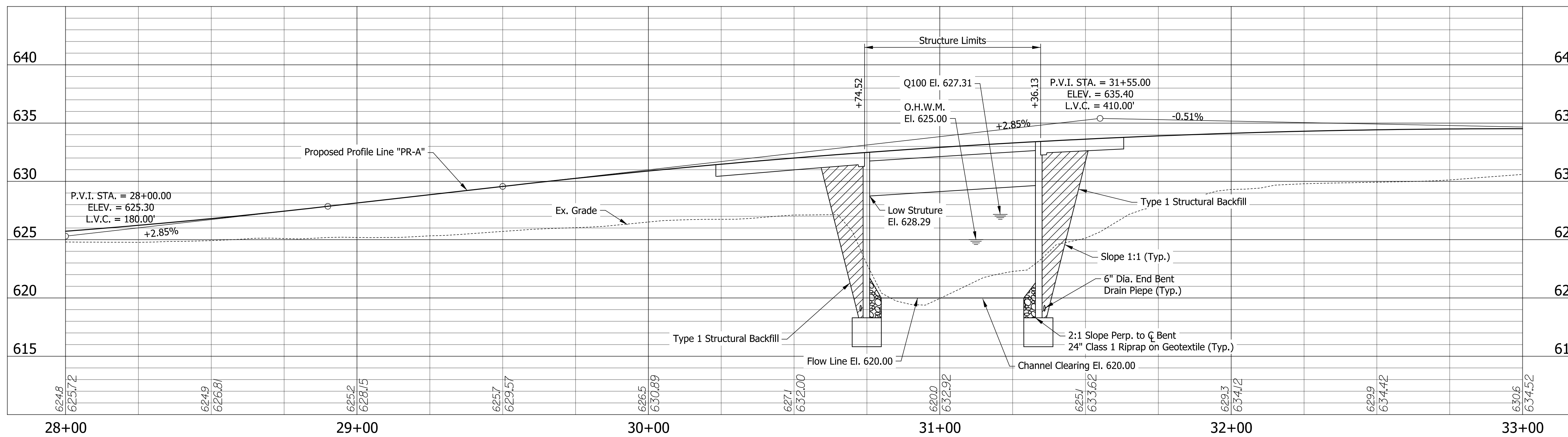
HYDRAULIC SCOUR DATA

Q100 Discharge	2,600 cfs
Max Velocity at Q100	7.60 ft/s
Q100 Scour Depth (Contraction)	0.0 ft
Q100 Scour Depth (Total)	0.0 ft
Q100 Low Scour Elevation	617.74 ft
Q500 Discharge	3,640 cfs
Max Velocity at Q500	9.52 ft/sec
Q500 Scour Depth (Contraction)	0.0 ft
Q500 Scour Depth (Total)	0.0 ft
Q500 Low Scour Elevation	617.74 ft

EARTHWORK TABULATION

Earthwork will be included with Stage 3

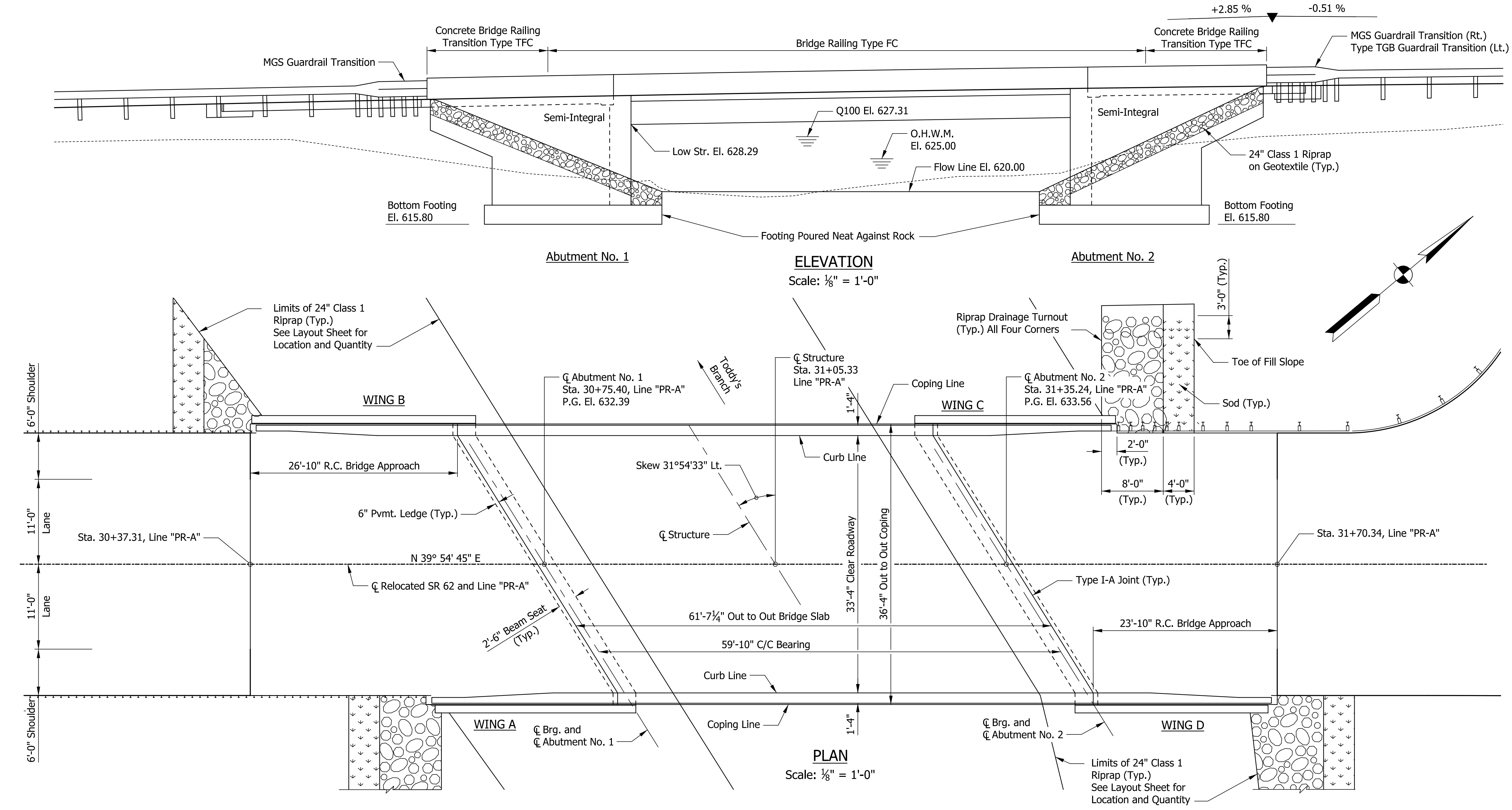
**PRESTRESSED CONCRETE
 I-BEAM TYPE II BRIDGE
 SPAN: 59'-10"
 SKEW: 31° 54' 33" LEFT
 33'-4" CLEAR ROADWAY
 SR 62 OVER TODDY'S BRANCH
 JEFFERSON COUNTY**



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RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE	INDIANA DEPARTMENT OF TRANSPORTATION	HORIZONTAL SCALE	BRIDGE FILE
DESIGNED: SJA	DRAWN: JDG			1" = 20'	062-39-10270
CHECKED: XAC	CHECKED: XAC		SR 62 OVER TODDY'S BRANCH LAYOUT	VERTICAL SCALE	DESIGNATION
				1" = 5'	1701457
				SURVEY BOOK	DWG. NO.
				LY-01	12 of 29
				CONTRACT	PROJECT
				B-40421	1701457

STRUCTURE TO BE BUILT ON A 410' VERTICAL CURVE



GENERAL NOTES:

- Plans for existing structure are on file in the Central Office as Structure No. 62-39-05946 and are available upon request.
- Reinforcing steel cover shall be 2 1/2" in top and 1" minimum in bottom of floor slabs, 3" in footings except 4" in bottom of footings and 2" in all other parts, unless noted.
- The following locations shall be Surface Sealed: Top of Bridge Deck, Copings, all exposed surfaces of Concrete Railings, Concrete Bridge Deck, and RCBA's.

DESIGN DATA

Designed to HL-93 loadings, in accordance with AASHTO LRFD Bridge Design Specifications, 8th edition, 2017.

DEAD LOAD

Actual weight plus 35 P.S.F. (Composite) for future wearing surface and 15 P.S.F. (Non-Composite) for permanent metal deck forms.

FLOOR SLAB

Designed with a 7 1/2" structural depth plus 1/2" sacrificial wearing surface.

DESIGN STRESSES

CONCRETE

Class C	fc = 4,000 PSI
Class B	fc = 3,000 PSI
Class A	fc = 3,500 PSI

REINFORCING STEEL

Grade 60	fy = 60,000 PSI
----------	-----------------

CONSTRUCTION LOADING

The exterior beam has been checked for strength, deflection, and overturning using the construction loads shown below. Cantilever overhang brackets were assumed for support of the deck overhang past the edge of the exterior beam. The finishing machine was assumed to be supported 6 in. outside the vertical coping form. The top overhang brackets were assumed to be located 6 in. past the edge of the vertical coping form. The bottom overhang brackets were assumed to be braced against the intersection of the beam bottom flange and web.

DECK FALSEWORK LOADS

Designed for 15 lbs./sq.ft. for permanent metal stay-in-place deck forms, removable deck forms and 2 ft. exterior walkway.

CONSTRUCTION LIVE LOADS

Designed for 20 lbs./sq.ft. extending 2 ft. past the edge of coping and 75 lbs./sq.ft. vertical force applied at a distance of 6" outside the face of coping over a 30 ft. length of the deck centered with the finishing machine.

FINISHING-MACHINE LOAD

4500 lbs. distributed over 10 ft. along the coping.

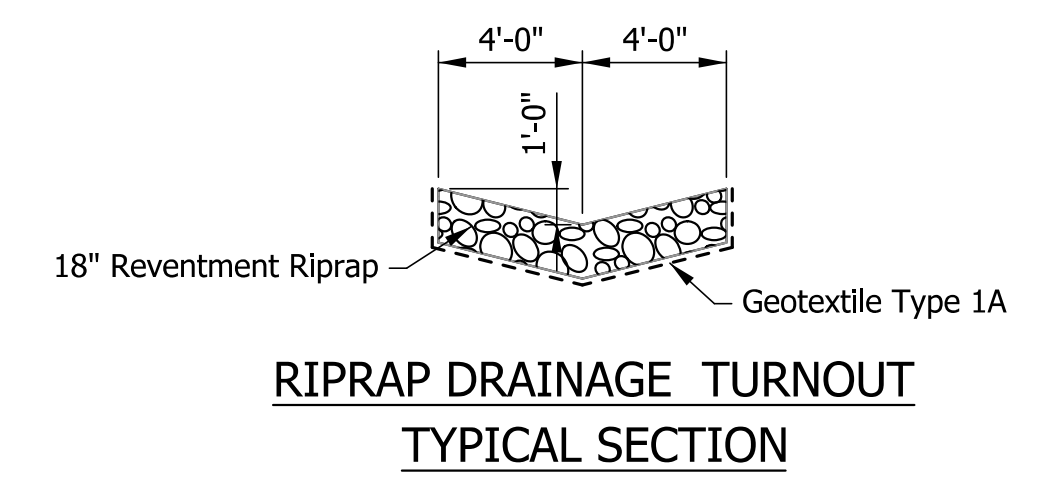
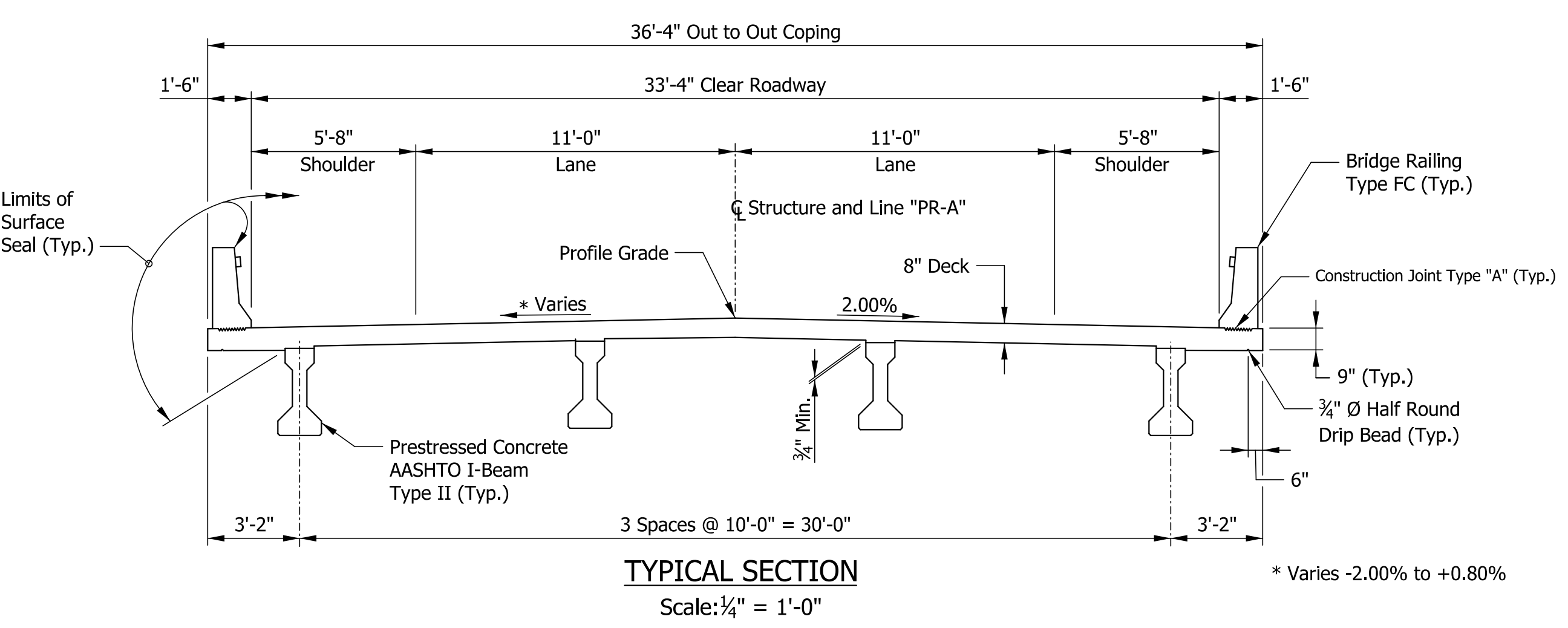
WIND LOAD

Structure designed for 70mph horizontal wind loading in accordance with LRFD 3.8.1.

SEISMIC DESIGN LOAD

Seismic Performance Zone	Zone 1
Acceleration Coefficient	0.059
Seismic Soil Profile Type	Class B

PRESTRESSED CONCRETE I-BEAM TYPE II BRIDGE
 SPAN: 59'-10"
 SKEW: 31° 54' 33" LEFT
 33'-4" CLEAR ROADWAY
 SR 62 OVER TODDY'S BRANCH
 JEFFERSON COUNTY



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RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE		INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE		BRIDGE FILE	
DESIGNED: SJA		DRAWN: JDG		CHECKED: XAC				As Noted		As Noted	
CHECKED: XAC		CHECKED: SJA		SR 62 OVER TODDY'S BRANCH		GENERAL PLAN		SURVEY BOOK		DWG. NO.	
								B-40421		13 of 29	
								PROJECT		1701457	

Appendix C

Early Coordination

<<Address>>

Re: Des. Nos. 1701457, 1602259, 1701455, and 1700005
 Bridge Nos. 062-39-05946 C, 062-39-05947 C,
 062-39-05859 B, and 062-39-06045 C
 Bridge Rehabilitation/Reconstruction SR 62
 Jefferson County, Indiana

June 12, 2018

Dear _____:

The Indiana Department of Transportation (INDOT) and Federal Highway Administration (FHWA) intends to proceed with a project involving the rehabilitation and reconstruction of four bridges along State Route (SR) 62. This letter is part of the early coordination phase of the environmental review process. Burgess & Niple, Inc. (B&N) is requesting comments from your area of expertise regarding any possible environmental effects associated with this project. **Please use the above designation numbers and description in your reply.** We will incorporate your comments into a study of the project's environmental impacts.

All four bridges are located on SR 62, a two-lane roadway classified as a Rural Major Collector with rolling terrain.

Designation No. 1701457 is a bridge replacement project over Toddy's Branch, located 0.35 mile east of SR 250. The existing beams, wingwalls, and abutments exhibit cracking, spalling, and exposed rebar. The current recommendation is to replace the bridge with concrete beam superstructure on a new alignment and profile.

Designation No. 1602259 is a bridge rehabilitation project over Toddy's Branch, 0.77 mile east of SR 250 (RPO 24+94). The existing structure exhibits cracks and spalling on the superstructure. The joints are missing material, and the steel railings have visible impacts leading to damaged copings, wingwalls, and abutments. The superstructure will be replaced and widened for new concrete rails. The existing box beams will be replaced with AASHTO Type II Concrete Beams. The existing abutments will be modified, cleaned, and patched.

Designation No. 1701455 is a bridge replacement project over Indian-Kentuck Creek. It is located 0.59 mile west of the intersection with SR 250 (RPO 30+80). The current structure has map cracking across the wearing surface and longitudinal cracks on the beams. Spalling and cracking are present on both abutments, and there is a large scour hole at one of the piers. The recommendation is to replace the bridge on a new alignment and profile with a concrete beam superstructure and add scour protection.

Designation No. 1700005 is a bridge rehabilitation project located over Salem Branch, 1.46 miles west of SR 129 (RPO 34+35). The structure currently has longitudinal and map cracks along the superstructure, and missing joint material in some locations. The bridge rails show impacts that lead to damaged copings, wingwalls, and abutments. The existing superstructure will be removed down to the bridge seat and replaced with a slab bridge. The superstructure will be widened to provide additional width for the concrete bridge rails. Patching will occur on the end bents and wingwalls, as well as the concrete deck area.

June 12, 2018
Page 2

The proposed projects will require closure of SR 62. A temporary detour will be put in place that utilizes SR 250, SR 421, and SR 129. Construction is currently scheduled to take place in summer of 2022.

The location of these bridges is primarily rural, the majority of the land being forested with occasional residential and agricultural properties. Work in the waterways is required, thus one Waters of the U.S. (WOTUS) report is to be completed for all four projects. Coordination with the INDOT Environmental Services-Ecological and Waterway Permitting will occur. A Section 404 Waterway Permit from the U.S. Army Corps of Engineers (USACE) and an Indiana Section 401 Water Quality Certification is anticipated. An Indiana Department of Environmental Management (IDEM) Rule 5 Permit is anticipated for the Designation No. 1701457 project, as the project will likely create more than 1 acre of disturbance. The projects qualify for the application of the U.S. Fish & Wildlife Service (USFWS) range-wide programmatic informal consultation for the Indiana bat and Northern long-eared bat. The project information form will be provided to USFWS for review separately. INDOT has indicated that the project will most likely be cleared with a Categorical Exclusion Level 1 document.

Should we not receive your response **within thirty (30) calendar days** from the date of this letter, it will be assumed that your agency feels that there will be no adverse effects incurred as a result of the proposed project. However, if you find that an extension to the response time is necessary, a reasonable amount may be granted upon request.

If you have any questions regarding this matter, please feel free to contact me at the address and phone number on the letterhead above or by email at liz.neudeck@burgessniple.com. Thank you in advance for your input.

Sincerely,

BURGESS & NIPLE, INC.



Liz Neudeck
Environmental Engineering Intern

RGF:cmc
Attachments
Maps, Photographs, Agency Form (if required)
List of ECL Recipients

The following agencies received Early Coordination Letters:

Regional Environmental Coordinator
Midwest Regional Office
National Park Service
601 Riverfront Drive
Omaha, NE 68102

Ms. Robin McWilliams
Field Supervisor
U.S. Fish & Wildlife Service
Bloomington Indiana Field Office
620 South Walker Street
Bloomington, IN 47403-2121

Field Environmental Officer
Chicago Regional Office
U.S. Department of Housing & Urban Development
Metcalf Federal Building
77 West Jackson Boulevard, Room 2401
Chicago, IL 60604

Mr. Antonio Johnson
Federal Highway Administration
Room 254, Federal Office Building
575 North Pennsylvania Street
Indianapolis, IN 47250

Mr. Rick Neilson
State Conservationist
Natural Resources Conservation Service
6013 Lakeside Blvd.
Indianapolis, IN 46278

Mr. Norbert Schafer
Jefferson County Commissioner, Upper District
300 E. Main Street
Room 103
Madison, IN 47250

Mr. Robert Phillips, Superintendent
Jefferson County Highway Department
300 E. Main Street, Room 103
Madison, IN 47250

Council Members
Jefferson County Council
300 E. Main Street, Room 103
Madison, IN 47250

Mr. Gregory McKay
U.S. Army Corps of Engineers
Louisville District
ATTN: CELRL-RDN
PO Box 59
Louisville, KY 40201-0059

Environmental Coordinator
Indiana Department of Natural Resources
Division of Fish and Wildlife
Room W264, IGC South
402 W. Washington Street
Indianapolis, IN 46204-2641

Indiana Geological Survey
611 North Walnut Grove
Bloomington, IN 47405

INDOT – Office of Public Involvement
Public Hearings Manager
100 N. Senate Avenue, Rm 642
Indianapolis, IN 46204



Indiana Department of Environmental Management

We Protect Hoosiers and Our Environment.

100 North Senate Avenue - Indianapolis, IN 46204
(800) 451-6027 - (317) 232-8603 - www.idem.IN.gov

Indiana Department of Transportation

5333 Hatfield Road
Fort Wayne , IN 46808
Date

Elizabeth Neudeck
Jeff Drake
251 North Illinois Street
Indianapolis , IN 46204

To Engineers and Consultants Proposing Roadway Construction Projects:

RE: All four bridges are located on SR 62, a two-lane roadway classified as a Rural Major Collector with rolling terrain. Designation No. 1701457 is a bridge replacement project over Toddy's Branch, located 0.35 mile east of SR 250. The existing beams, wingwalls, and abutments exhibit cracking, spalling, and exposed rebar. The current recommendation is to replace the bridge with concrete beam superstructure on a new alignment and profile. Designation No. 1602259 is a bridge rehabilitation project over Toddy's Branch, 0.77 mile east of SR 250 (RPO 24+94). The existing structure exhibits cracks and spalling on the superstructure. The superstructure will be replaced and widened for new concrete rails. The existing box beams will be replaced with AASHTO Type II Concrete Beams. The existing abutments will be modified, cleaned, and patched. Designation No. 1701455 is a bridge replacement project over Indian-Kentuck Creek. It is located 0.59 mile west of the intersection with SR 250 (RPO 30+80). The recommendation is to replace the bridge on a new alignment and profile with a concrete beam superstructure and add scour protection. Designation No. 1700005 is a bridge rehabilitation project located over Salem Branch, 1.46 miles west of SR 129 (RPO 34+35). The existing superstructure will be removed down to the bridge seat and replaced with a slab bridge. The superstructure will be widened to provide additional width for the concrete bridge rails. Patching will occur on the end bents and wingwalls, as well as the concrete deck area.

This letter from the Indiana Department of Environmental Management (IDEM) serves as a standardized response to enquiries inviting IDEM comments on roadway construction, reconstruction, or other improvement projects within existing roadway corridors when the proposed scope of the project is beneath the threshold requiring a formal National Environmental Policy Act-mandated Environmental Assessment or Environmental Impact Statement. As the letter attempts to address all roadway-related environmental topics of potential concern, it is possible that not every topic addressed in the letter will be applicable to your particular roadway project.

For additional information on specific roadway-related topics of interest, please visit the appropriate Web pages cited below, many of which provide contact information for persons within the various program areas who can answer questions not fully addressed in this letter. Also please be mindful that some environmental requirements may be subject to change and so each person intending to include a copy of this letter in their project documentation packet is advised to download the most recently revised version of the letter; found at: <http://www.in.gov/idem/5283.htm> (<http://www.in.gov/idem/5283.htm>).

To ensure that all environmentally-related issues are adequately addressed, IDEM recommends that you read this letter in its entirety, and consider each of the following issues as you move forward with the planning of your proposed roadway construction, reconstruction, or improvement project:

WATER AND BIOTIC QUALITY

1. Section 404 of the Clean Water Act requires that you obtain a permit from the U.S. Army Corps of Engineers (USACE) before discharging dredged or fill materials into any wetlands or other waters, such as rivers, lakes, streams, and ditches. Other activities regulated include the relocation, channelization, widening, or other such alteration of a stream, and the mechanical clearing (use of heavy construction equipment) of wetlands. Thus, as a project owner or sponsor, it is your responsibility to ensure that no wetlands are disturbed without the proper permit. Although you may initially refer to the U.S. Fish and Wildlife Service National Wetland Inventory maps as a means of identifying potential areas of concern, please be mindful that those maps do not depict jurisdictional wetlands regulated by the USACE or the Department of Environmental Management. A valid jurisdictional wetlands determination can only be made by the USACE, using the 1987 Wetland Delineation Manual.

USACE recommends that you have a consultant check to determine whether your project will abut, or lie within, a wetland area. To view a list of consultants that have requested to be included on a list posted by the USACE on their Web site, see USACE Permits and Public Notices (<http://www.lrl.usace.army.mil/orf/default.asp>) (<http://www.lrl.usace.army.mil/orf/default.asp>) and then click on "Information" from the menu on the right-hand side of that page. Their "Consultant List" is the fourth entry down on the "Information" page. Please note that the USACE posts all consultants that request to appear on the list, and that inclusion of any particular consultant on the list does not represent an endorsement of that consultant by the USACE, or by IDEM.

Much of northern Indiana (Newton, Lake, Porter, LaPorte, St. Joseph, Elkhart, LaGrange, Steuben, and Dekalb counties; large portions of Jasper, Starke, Marshall, Noble, Allen, and Adams counties; and lesser portions of Benton, White, Pulaski, Kosciusko, and Wells counties) is served by the USACE District Office in Detroit (313-226-6812). The central and southern portions of the state (large portions of Benton, White, Pulaski, Kosciusko, and Wells counties; smaller portions of Jasper, Starke, Marshall, Noble, Allen, and Adams counties; and all other Indiana counties located in north-central, central, and southern Indiana) are served by the USACE Louisville District Office (502-315-6733).

Additional information on contacting these U.S. Army Corps of Engineers (USACE) District Offices, government agencies with jurisdiction over wetlands, and other water quality issues, can be found at <http://www.in.gov/idem/4396.htm> (<http://www.in.gov/idem/4396.htm>). IDEM recommends that impacts to wetlands and other water resources be avoided to the fullest extent.

2. In the event a Section 404 wetlands permit is required from the USACE, you also must obtain a Section 401 Water Quality Certification from the IDEM Office of Water Quality Wetlands Program. To learn more about the Wetlands Program, visit: <http://www.in.gov/idem/4384.htm> (<http://www.in.gov/idem/4384.htm>).
3. If the USACE determines that a wetland or other water body is isolated and not subject to Clean Water Act regulation, it is still regulated by the state of Indiana . A State Isolated Wetland permit from IDEM's Office of Water Quality (OWQ) is required for any activity that results in the discharge of dredged or fill materials into isolated wetlands. To learn more about isolated wetlands, contact the OWQ Wetlands Program at 317-233-8488.

4. If your project will involve over a 0.5 acre of wetland impact, stream relocation, or other large-scale alterations to water bodies such as the creation of a dam or a water diversion, you should seek additional input from the OWQ Wetlands Program staff. Consult the Web at: <http://www.in.gov/idem/4384.htm> (<http://www.in.gov/idem/4384.htm>) for the appropriate staff contact to further discuss your project.
5. Work within the one-hundred year floodway of a given water body is regulated by the Department of Natural Resources, Division of Water. The Division issues permits for activities regulated under the follow statutes:
 - IC 14-26-2 Lakes Preservation Act 312 IAC 11
 - IC 14-26-5 Lowering of Ten Acre Lakes Act No related code
 - IC 14-28-1 Flood Control Act 310 IAC 6-1
 - IC 14-29-1 Navigable Waterways Act 312 IAC 6
 - IC 14-29-3 Sand and Gravel Permits Act 312 IAC 6
 - IC 14-29-4 Construction of Channels Act No related code

For information on these Indiana (statutory) Code and Indiana Administrative Code citations, see the DNR Web site at: <http://www.in.gov/dnr/water/9451.htm> (<http://www.in.gov/dnr/water/9451.htm>) . Contact the DNR Division of Water at 317-232-4160 for further information.

The physical disturbance of the stream and riparian vegetation, especially large trees overhanging any affected water bodies should be limited to only that which is absolutely necessary to complete the project. The shade provided by the large overhanging trees helps maintain proper stream temperatures and dissolved oxygen for aquatic life.

6. For projects involving construction activity (which includes clearing, grading, excavation and other land disturbing activities) that result in the disturbance of one (1), or more, acres of total land area, contact the Office of Water Quality – Watershed Planning Branch (317/233-1864) regarding the need for of a Rule 5 Storm Water Runoff Permit. Visit the following Web page
 - <http://www.in.gov/idem/4902.htm> (<http://www.in.gov/idem/4902.htm>)

To obtain, and operate under, a Rule 5 permit you will first need to develop a Construction Plan (<http://www.in.gov/idem/4917.htm#constreq> (<http://www.in.gov/idem/4917.htm#constreq>)), and as described in 327 IAC 15-5-6.5 (<http://www.in.gov/legislative/iac/T03270/A00150> [PDF] (<http://www.in.gov/legislative/iac/T03270/A00150.PDF>), pages 16 through 19). Before you may apply for a Rule 5 Permit, or begin construction, you must submit your Construction Plan to your county Soil and Water Conservation District (SWCD) (<http://www.in.gov/isda/soil/contacts/map.html> (<http://www.in.gov/isda/soil/contacts/map.html>)).

Upon receipt of the construction plan, personnel of the SWCD or the Indiana Department of Environmental Management will review the plan to determine if it meets the requirements of 327 IAC 15-5. Plans that are deemed deficient will require re-submittal. If the plan is sufficient you will be notified and instructed to submit the verification to IDEM as part of the Rule 5 Notice of Intent (NOI) submittal. Once construction begins, staff of the SWCD or Indiana Department of Environmental Management will perform inspections of activities at the site for compliance with the regulation.

Please be mindful that approximately 149 Municipal Separate Storm Sewer System (MS4) areas are now being established by various local governmental entities throughout the state as part of the implementation of Phase II federal storm water requirements. All of these MS4 areas will eventually take responsibility for Construction Plan review, inspection, and enforcement. As these MS4 areas

obtain program approval from IDEM, they will be added to a list of MS4 areas posted on the IDEM Website at: <http://www.in.gov/idem/4900.htm> (<http://www.in.gov/idem/4900.htm>).

If your project is located in an IDEM-approved MS4 area, please contact the local MS4 program about meeting their storm water requirements. Once the MS4 approves the plan, the NOI can be submitted to IDEM.

Regardless of the size of your project, or which agency you work with to meet storm water requirements, IDEM recommends that appropriate structures and techniques be utilized both during the construction phase, and after completion of the project, to minimize the impacts associated with storm water runoff. The use of appropriate planning and site development and appropriate storm water quality measures are recommended to prevent soil from leaving the construction site during active land disturbance and for post construction water quality concerns. Information and assistance regarding storm water related to construction activities are available from the Soil and Water Conservation District (SWCD) offices in each county or from IDEM.

7. For projects involving impacts to fish and botanical resources, contact the Department of Natural Resources - Division of Fish and Wildlife (317/232-4080) for addition project input.
8. For projects involving water main construction, water main extensions, and new public water supplies, contact the Office of Water Quality - Drinking Water Branch (317-308-3299) regarding the need for permits.
9. For projects involving effluent discharges to waters of the State of Indiana , contact the Office of Water Quality - Permits Branch (317-233-0468) regarding the need for a National Pollutant Discharge Elimination System (NPDES) permit.
10. For projects involving the construction of wastewater facilities and sewer lines, contact the Office of Water Quality - Permits Branch (317-232-8675) regarding the need for permits.

AIR QUALITY

The above-noted project should be designed to minimize any impact on ambient air quality in, or near, the project area. The project must comply with all federal and state air pollution regulations. Consideration should be given to the following:

1. Regarding open burning, and disposing of organic debris generated by land clearing activities; some types of open burning are allowed (<http://www.in.gov/idem/4148.htm> (<http://www.in.gov/idem/4148.htm>)) under specific conditions. You also can seek an open burning variance from IDEM.

However, IDEM generally recommends that you take vegetative wastes to a registered yard waste composting facility or that the waste be chipped or shredded with composting on site (you must register with IDEM if more than 2,000 pounds is to be composted; contact 317/232-0066). The finished compost can then be used as a mulch or soil amendment. You also may bury any vegetative wastes (such as leaves, twigs, branches, limbs, tree trunks and stumps) onsite, although burying large quantities of such material can lead to subsidence problems, later on.

Reasonable precautions must be taken to minimize fugitive dust emissions from construction and demolition activities. For example, wetting the area with water, constructing wind barriers, or treating dusty areas with chemical stabilizers (such as calcium chloride or several other commercial products). Dirt tracked onto paved roads from unpaved areas should be minimized.

Additionally, if construction or demolition is conducted in a wooded area where blackbirds have roosted or abandoned buildings or building sections in which pigeons or bats have roosted for 3-5 years precautionary measures should be taken to avoid an outbreak of histoplasmosis. This disease is caused by the fungus *Histoplasma capsulatum*, which stems from bird or bat droppings that have accumulated in one area for 3-5 years. The spores from this fungus become airborne when the area is disturbed and can cause infections over an entire community downwind of the site. The area should be wetted down prior to cleanup or demolition of the project site. For more detailed information on histoplasmosis prevention and control, please contact the Acute Disease Control Division of the Indiana State Department of Health at (317) 233-7272.

2. The U.S. EPA and the Surgeon General recommend that people not have long-term exposure to radon at levels above 4 pCi/L. (For a county-by-county map of predicted radon levels in Indiana, visit: <http://www.in.gov/idem/4145.htm> (<http://www.in.gov/idem/4145.htm>).

The U.S. EPA further recommends that all homes (and apartments within three stories of ground level) be tested for radon. If in-home radon levels are determined to be 4 pCi/L, or higher, EPA recommends a follow-up test. If the second test confirms that radon levels are 4 pCi/L, or higher, EPA recommends the installation of radon-reduction measures. (For a list of qualified radon testers and radon mitigation (or reduction) specialists visit:

http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon_testers_mitigators_list.pdf

(http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon_testers_mitigators_list.pdf.) It also is recommended that radon reduction measures be built into all new homes, particularly in areas like Indiana that have moderate to high predicted radon levels.

To learn more about radon, radon risks, and ways to reduce exposure visit:

<http://www.in.gov/isdh/regsvcs/radhealth/radon.htm>

(<http://www.in.gov/isdh/regsvcs/radhealth/radon.htm>), <http://www.in.gov/idem/4145.htm>

(<http://www.in.gov/idem/4145.htm>), or <http://www.epa.gov/radon/index.html>

(<http://www.epa.gov/radon/index.html>).

3. With respect to asbestos removal: all facilities slated for renovation or demolition (except residential buildings that have (4) four or fewer dwelling units and which will not be used for commercial purposes) must be inspected by an Indiana-licensed asbestos inspector prior to the commencement of any renovation or demolition activities. If regulated asbestos-containing material (RACM) that may become airborne is found, any subsequent demolition, renovation, or asbestos removal activities must be performed in accordance with the proper notification and emission control requirements.

If no asbestos is found where a renovation activity will occur, or if the renovation involves removal of less than 260 linear feet of RACM off of pipes, less than 160 square feet of RACM off of other facility components, or less than 35 cubic feet of RACM off of all facility components, the owner or operator of the project does not need to notify IDEM before beginning the renovation activity.

For questions on asbestos demolition and renovation activities, you can also call IDEM's Lead/Asbestos section at 1-888-574-8150.

However, in all cases where a demolition activity will occur (even if no asbestos is found), the owner or operator must still notify IDEM 10 working days prior to the demolition, using the form found at <http://www.in.gov/icpr/webfile/formsdiv/44593.pdf> (<http://www.in.gov/icpr/webfile/formsdiv/44593.pdf>).

Anyone submitting a renovation/demolition notification form will be billed a notification fee based upon the amount of friable asbestos containing material to be removed or demolished. Projects that involve the removal of more than 2,600 linear feet of friable asbestos containing materials on pipes, or 1,600 square feet or 400 cubic feet of friable asbestos containing material on other facility components, will be billed a fee of \$150 per project; projects below these amounts will be billed a fee of \$50 per project. All notification remitters will be billed on a quarterly basis.

For more information about IDEM policy regarding asbestos removal and disposal, visit:

<http://www.in.gov/idem/4983.htm> (<http://www.in.gov/idem/4983.htm>).

4. With respect to lead-based paint removal: IDEM encourages all efforts to minimize human exposure to lead-based paint chips and dust. IDEM is particularly concerned that young children exposed to lead can suffer from learning disabilities. Although lead-based paint abatement efforts are not mandatory, any abatement that is conducted within housing built before January 1, 1978, or a child-occupied facility is required to comply with all lead-based paint work practice standards, licensing and notification requirements. For more information about lead-based paint removal visit: <http://www.in.gov/isdh/19131.htm> (<http://www.in.gov/isdh/19131.htm>).
5. Ensure that asphalt paving plants are permitted and operate properly. The use of cutback asphalt, or asphalt emulsion containing more than seven percent (7%) oil distillate, is prohibited during the months April through October. See 326 IAC 8-5-2, Asphalt Paving Rule (<http://www.ai.org/legislative/iac/T03260/A00080.PDF> (<http://www.ai.org/legislative/iac/T03260/A00080.PDF>)).
6. If your project involves the construction of a new source of air emissions or the modification of an existing source of air emissions or air pollution control equipment, it will need to be reviewed by the IDEM Office of Air Quality (OAQ). A registration or permit may be required under 326 IAC 2 (View at: www.ai.org/legislative/iac/t03260/a00020.pdf (<http://www.ai.org/legislative/iac/t03260/a00020.pdf>)). New sources that use or emit hazardous air pollutants may be subject to Section 112 of the Clean Air Act and corresponding state air regulations governing hazardous air pollutants.
7. For more information on air permits visit: <http://www.in.gov/idem/4223.htm> (<http://www.in.gov/idem/4223.htm>), or to initiate the IDEM air permitting process, please contact the Office of Air Quality Permit Reviewer of the Day at (317) 233-0178 or OAMPROD atdem.state.in.us.

LAND QUALITY

In order to maintain compliance with all applicable laws regarding contamination and/or proper waste disposal, IDEM recommends that:

1. If the site is found to contain any areas used to dispose of solid or hazardous waste, you need to contact the Office of Land Quality (OLQ) at 317-308-3103.
2. All solid wastes generated by the project, or removed from the project site, need to be taken to a properly permitted solid waste processing or disposal facility. For more information, visit <http://www.in.gov/idem/4998.htm> (<http://www.in.gov/idem/4998.htm>).
3. If any contaminated soils are discovered during this project, they may be subject to disposal as hazardous waste. Please contact the OLQ at 317-308-3103 to obtain information on proper disposal procedures.

4. If PCBs are found at this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding management of any PCB wastes from this site.
5. If there are any asbestos disposal issues related to this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding the management of asbestos wastes (Asbestos removal is addressed above, under Air Quality).
6. If the project involves the installation or removal of an underground storage tank, or involves contamination from an underground storage tank, you must contact the IDEM Underground Storage Tank program at 317/308-3039. See: <http://www.in.gov/idem/4999.htm> (<http://www.in.gov/idem/4999.htm>).

FINAL REMARKS

Should you need to obtain any environmental permits in association with this proposed project, please be mindful that IC 13-15-8 requires that you notify all adjoining property owners and/or occupants within ten days your submittal of each permit application. However, if you are seeking multiple permits, you can still meet the notification requirement with a single notice if all required permit applications are submitted with the same ten day period.

Should the scope of the proposed project be expanded to the extent that a National Environmental Policy Act Environmental Assessment (EA) or Environmental Impact Statement (EIS) is required, IDEM will actively participate in any early interagency coordination review of the project.

Meanwhile, please note that this letter does not constitute a permit, license, endorsement or any other form of approval on the part of the Indiana Department of Environmental Management regarding any project for which a copy of this letter is used. Also note that it is the responsibility of the project engineer or consultant using this letter to ensure that the most current draft of this document, which is located at <http://www.in.gov/idem/5284.htm> (<http://www.in.gov/idem/5284.htm>), is used.

Signature(s) of the Applicant

I acknowledge that the following proposed roadway project will be financed in part, or in whole, by public monies.

Project Description

All four bridges are located on SR 62, a two-lane roadway classified as a Rural Major Collector with rolling terrain. Designation No. 1701457 is a bridge replacement project over Toddy's Branch, located 0.35 mile east of SR 250. The existing beams, wingwalls, and abutments exhibit cracking, spalling, and exposed rebar. The current recommendation is to replace the bridge with concrete beam superstructure on a new alignment and profile. Designation No. 1602259 is a bridge rehabilitation project over Toddy's Branch, 0.77 mile east of SR 250 (RPO 24+94). The existing structure exhibits cracks and spalling on the superstructure. The superstructure will be replaced and widened for new concrete rails. The existing box beams will be replaced with AASHTO Type II Concrete Beams. The existing abutments will be modified, cleaned, and patched. Designation No. 1701455 is a bridge replacement project over Indian-Kentuck Creek. It is located 0.59 mile west of the intersection with SR 250 (RPO 30+80). The recommendation is to replace the bridge on a new alignment and profile with a concrete beam superstructure and add scour protection. Designation No. 1700005 is a bridge rehabilitation project located over Salem Branch, 1.46 miles west of SR 129 (RPO 34+35).

The existing superstructure will be removed down to the bridge seat and replaced with a slab bridge. The superstructure will be widened to provide additional width for the concrete bridge rails. Patching will occur on the end bents and wingwalls, as well as the concrete deck area.

With my signature, I do hereby affirm that I have read the letter from the Indiana Department of Environment that appears directly above. In addition, I understand that in order to complete that project in which I am interested, with a minimum of impact to the environment, I must consider all the issues addressed in the aforementioned letter, and further, that I must obtain any required permits.

Date: 8/21/2020

Signature of the INDOT Project Engineer or Other Responsible Agent Terry M. Summers

Date: 6/15/2018

Signature of the For Hire Consultant Jeff Drake

Jeff Drake

State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

DNR #: ER-20638

Request Received: June 13, 2018

Requestor: Burgess and Niple Inc
Liz Neudeck
5085 Reed Road
Columbus, OH 43220

Project: SR 62 bridge projects:
1) Des #1701457: replacement over Toddy's Branch, 0.35 mile east of SR 250
2) Des #1602259: rehabilitation over Toddy's Branch, 0.77 mile east of SR 250
3) Des #1701455: replacement over Indian-Kentuck Creek, 0.59 mile west of SR 250
4) Des #1700005: rehabilitation over Salem Branch, 1.46 miles west of SR 129

County/Site info: Jefferson

The Indiana Department of Natural Resources has reviewed the above referenced project per your request. Our agency offers the following comments for your information and in accordance with the National Environmental Policy Act of 1969.

If our agency has regulatory jurisdiction over the project, the recommendations contained in this letter may become requirements of any permit issued. If we do not have permitting authority, all recommendations are voluntary.

Regulatory Assessment: Des #s 1701457, 1602259, and 1701455 will require the formal approval of our agency for construction in a floodway pursuant to the Flood Control Act (IC 14-28-1), unless they qualify for a bridge exemption (see enclosure). Please include a copy of this letter with the permit application(s) if they do not meet the bridge exemption criteria.

Formal approval by the Department of Natural Resources under the regulatory programs administered by the Division of Water is not required for Des #1700005 over Salem Branch.

Natural Heritage Database: The Natural Heritage Program's data have been checked. To date, no plant or animal species listed as state or federally threatened, endangered, or rare have been reported to occur in the project vicinity.

Fish & Wildlife Comments: Avoid and minimize impacts to fish, wildlife, and botanical resources to the greatest extent possible, and compensate for impacts. The following are recommendations that address potential impacts identified in the proposed project area:

1) Crossing Structures:

For purposes of maintaining fish and wildlife passage through a crossing structure, the Environmental Unit recommends bridges rather than culverts and bottomless culverts rather than box or pipe culverts. Wide culverts are better than narrow culverts, and culverts with shorter through lengths are better than culverts with longer through lengths. If box or pipe culverts are used, the bottoms should be buried a minimum of 6" (or 20% of the culvert height/pipe diameter, whichever is greater up to a maximum of 2') below the stream bed elevation to allow a natural streambed to form within or under the crossing structure. Crossings should: span the entire channel width (a minimum of 1.2 times the bankful width); maintain the natural stream substrate within the structure; have a minimum openness ratio (height x width / length) of 0.25; and have stream depth and water velocities during low-flow conditions that are approximate to those in the natural stream channel.

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2) Fish & Wildlife Passage:

The new, replacement, or rehabbed structure, and any bank stabilization under the structure, should not create conditions that are less favorable for wildlife passage under the structure compared to the current conditions. Riprap must not be placed in the active thalweg channel or placed in the streambed in a manner that precludes fish or aquatic organism passage (riprap must not be placed above the existing streambed elevation). Riprap may be used only at the toe of the sideslopes up to the ordinary high water mark (OHWM). The banks above the OHWM must be restored, stabilized, and revegetated using geotextiles and a mixture of grasses, sedges, wildflowers, shrubs, and trees native to Southern Indiana and specifically for stream bank/floodway stabilization purposes as soon as possible upon completion.

Where hard armoring is needed, wildlife passage can be facilitated by using a smooth-surfaced material instead of riprap, such as articulated concrete block mats, fabric-formed concrete mats or other similar smooth-surfaced materials as these materials will not impair wildlife movement.

Information about bioengineering techniques can be found at <http://www.in.gov/legislative/iac/20120404-IR-312120154NRA.xml.pdf>. Also, the following is a USDA/NRCS document that outlines many different bioengineering and other bank stabilization techniques: <http://directives.sc.gov.usda.gov/17553.wba>.

3) Riparian Habitat:

We recommend a mitigation plan be developed (and submitted with the permit application, if required) for any unavoidable habitat impacts that will occur. The DNR's Floodway Habitat Mitigation guidelines (and plant lists) can be found online at: <http://www.in.gov/legislative/iac/20140806-IR-312140295NRA.xml.pdf>.

Impacts to non-wetland forest of one (1) acre or more should be mitigated at a minimum 2:1 ratio. If less than one acre of non-wetland forest is removed in a rural setting, replacement should be at a 1:1 ratio based on area. Impacts to non-wetland forest under one (1) acre in an urban setting should be mitigated by planting five trees, at least 2 inches in diameter-at-breast height (dbh), for each tree which is removed that is 10" dbh or greater (5:1 mitigation based on the number of large trees). Impacts to wetland habitat should be mitigated at the appropriate ratio according to the 1991 INDOT/IDNR/USFWS Memorandum of Understanding.

The mitigation site should be located in the floodway, downstream of the one (1) square mile drainage area of that stream (or another stream within the 8-digit HUC, preferably as close to the impact site as possible) and adjacent to existing forested riparian habitat.

The additional measures listed below should be implemented to avoid, minimize, or compensate for impacts to fish, wildlife, and botanical resources:

1. Revegetate all bare and disturbed areas in the floodway with a mixture of native grasses, sedges, wildflowers as soon as possible upon completion. Do not use any varieties of Tall Fescue or other non-native plants (e.g. crown-vetch).
2. Minimize and contain within the project limits inchannel disturbance and the clearing of trees and brush.
3. Do not work in the waterway from April 1 through June 30 without the prior written approval of the Division of Fish and Wildlife.
4. Do not cut any trees suitable for Indiana bat or Northern Long-eared bat roosting (greater than 3 inches dbh, living or dead, with loose hanging bark, or with cracks, crevices, or cavities) from April 1 through September 30.
5. Do not excavate in the low flow area except for the placement of piers, foundations, and riprap, or removal of the old structure.

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6. Do not construct any temporary runarounds, access bridges, causeways, cofferdams, diversions, or pumparounds.
7. Use minimum average 6 inch graded riprap stone extended below the normal water level to provide habitat for aquatic organisms in the voids.
8. Plant native hardwood trees along the top of the bank and right-of-way to replace the vegetation destroyed during construction.
9. Post "Do Not Mow or Spray" signs along the right-of-way.
10. Appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from entering the stream or leaving the construction site; maintain these measures until construction is complete and all disturbed areas are stabilized.
11. Seed and protect disturbed streambanks that are 3:1 or steeper with erosion control blankets that are heavy-duty, biodegradable, and net-free or that use loose-woven / Leno-woven netting to minimize the entrapment and snaring of small-bodied wildlife such as snakes and turtles (follow manufacturer's recommendations for selection and installation); seed and mulch all disturbed areas not protected by other methods.

Contact Staff:

Christie L. Stanifer, Environ. Coordinator, Fish & Wildlife
Our agency appreciates this opportunity to be of service. Please contact the above staff member at (317) 232-4080 if we can be of further assistance.



Date: July 12, 2018

Christie L. Stanifer
Environ. Coordinator
Division of Fish and Wildlife

The Flood Control Act (IC 14-28-1) contains a provision (Section 22), which exempts certain bridge projects from its permitting requirement. Specifically, the Act states:

A permit is not required for "a construction or reconstruction project on a state or county highway bridge in a rural area that crosses a stream having an upstream drainage area of not more than fifty (50) square miles..."

Therefore, in order for a bridge project to be exempt, it must:

- be a state or county highway department project;
- be a bridge;
- be located in a rural area; and
- cross a stream having an upstream drainage area of less than 50 square miles.

The initial criterion is very specific - the structure must be a state or county highway department project.

The second requirement mandates that the project be a bridge (for this provision, the Department of Natural Resources considers a culvert to be a bridge). Projects such as bank protection, spoil disposal, borrow pits, etc. are not automatically exempt. Anyone proposing to undertake a non-bridge related activity should consult with the Division of Water's Technical Services Section staff at 317-232-4160 (or toll free at 1-877-928-3755) regarding the applicability of the exemption prior to initiating work.

The third criterion states that the project must be located in a rural area. The phrase "rural area" is defined as an area:

- where the lowest floor elevation, including a basement, of any residential, commercial, or industrial building impacted by the project is at least 2 feet above the 100 year flood elevation with the project in place;
- located outside the corporate boundaries of a consolidated or an incorporated city or town; and
- located outside of the territorial authority for comprehensive planning (generally, a 2 mile planning buffer around a city or town).

The final criterion limits the exemption to a project crossing a stream having an upstream drainage area of less than 50 square miles. The drainage area includes all land area contributing to runoff above the project site and is determined from the United States Geological Survey 7½ minute series quadrangle maps. The Department of Natural Resources will determine the drainage area upon written request.

This exemption has been grossly misunderstood and liberally applied in the past. As a result, the Department of Natural Resources is taking a firm stance on future violations. If challenged, it will be the responsibility of the person claiming the exemption to prove to the Department that all 4 criteria have been satisfied. Failure to do so will result in the Department initiating litigation with the potential for the imposition of fines in amounts up to \$10,000 per day.

Note: This exemption only applies to the Flood Control Act. If a bridge is to be constructed over a navigable waterway, or over or near a public freshwater lake, a permit will be required.

Matthew Kestner

From: McWilliams, Robin <robin_mcwilliams@fws.gov>
Sent: Wednesday, June 13, 2018 3:33 PM
To: Neudeck, Liz
Subject: Re: [EXTERNAL] ECL State Route 62 Bridge Projects (Des 1701457, 1602259, 1701455, and 1700005)

Dear Liz,

This responds to your recent letter, requesting our comments on the aforementioned project.

These comments have been prepared under the authority of the Fish and Wildlife Coordination Act (16 U.S.C. 661 et. seq.) and are consistent with the intent of the National Environmental Policy Act of 1969, the Endangered Species Act of 1973, and the U. S. Fish and Wildlife Service's Mitigation Policy.

The projects are within the range of the Indiana bat (*Myotis sodalis*) and northern long-eared bat (*Myotis septentrionalis*) and should follow the new Indiana bat/northern long-eared bat programmatic consultation process, if applicable (*i.e.* a federal transportation nexus is established). We will review that information once it is received.

The projects are also within the range of the sheepsnose mussel (*Plethobasus cyphus*), but no impacts are anticipated to the mussel as a result of the projects.

Based on a review of the information you provided, the U.S. Fish and Wildlife Service has no objections to the projects as currently proposed. However, should new information arise pertaining to project plans or a revised species list be published, it will be necessary for the Federal agency to reinitiate consultation. Standard recommendations are provided below.

We appreciate the opportunity to comment at this early stage of project planning. If project plans change such that fish and wildlife habitat may be affected, please re-coordinate with our office as soon as possible. If you have any questions about our recommendations, please call (812) 334-4261 x. 207.

Sincerely,
 Robin McWilliams Munson

Standard Recommendations:

1. Do not clear trees or understory vegetation outside the construction zone boundaries. **(This restriction is not related to the "tree clearing" restriction for potential Indiana Bat habitat.)**
2. Restrict below low-water work in streams to placement of culverts, piers, pilings and/or footings, shaping of the spill slopes around the bridge abutments, and placement of riprap.

Culverts should span the active stream channel, should be either embedded or a 3-sided or open-arch culvert, and be installed where practicable on an essentially flat slope. When an open-bottomed culvert or arch is used in a stream, which has a good natural bottom substrate, such as gravel, cobbles and boulders, the existing substrate should be left undisturbed beneath the culvert to provide natural habitat for the aquatic community.

3. Restrict channel work and vegetation clearing to the minimum necessary for installation of the stream crossing structure.
4. Minimize the extent of hard armor (riprap) in bank stabilization by using bioengineering techniques whenever possible. If rip rap is utilized for bank stabilization, extend it below low-water elevation to provide aquatic habitat.
5. Implement temporary erosion and sediment control methods within areas of disturbed soil. All disturbed soil areas upon project completion will be vegetated following INDOT's standard specifications.
6. Avoid all work within the inundated part of the stream channel (in perennial streams and larger intermittent streams) during the fish spawning season (April 1 through June 30), except for work within sealed structures such as caissons or cofferdams that were installed prior to the spawning season. No equipment shall be operated below Ordinary High Water Mark during this time unless the machinery is within the caissons or on the cofferdams.
7. Evaluate wildlife crossings under bridge/culverts projects in appropriate situations. Suitable crossings include flat areas below bridge abutments with suitable ground cover, high water shelves in culverts, amphibian tunnels and diversion fencing.

Robin McWilliams Munson

U.S. Fish and Wildlife Service
620 South Walker Street
Bloomington, Indiana 46403
812-334-4261 x. 207 Fax: 812-334-4273

Monday, Tuesday - 7:30a-3:00p
Wednesday, Thursday - telework 8:30a-3:00p

On Wed, Jun 13, 2018 at 3:06 PM, Neudeck, Liz <Elizabeth.Neudeck@burgessniple.com> wrote:

Hello,

Attached is the Early Coordination Letter for 4 bridge rehabilitation/reconstruction projects along State Route 62, Des no. 1701457, 1602259, 1701455, and 1700005. Please do not hesitate to reach out with questions or concerns regarding the projects.

Sincerely,

Liz Neudeck

Burgess & Niple

Organization and Project Information

Project ID:
Des. ID: 1701457
Project Title: Des. 1701457 SR 62 over Toddy's Branch
Name of Organization: Burgess & Niple
Requested by: Matthew Kestner

Environmental Assessment Report

1. Geological Hazards:
 - Potential Karst
 - 1% Annual Chance Flood Hazard
2. Mineral Resources:
 - Bedrock Resource: Low Potential
 - Sand and Gravel Resource: None documented in the area
3. Active or abandoned mineral resources extraction sites:
 - None documented in the area

*All map layers from Indiana Map (maps.indiana.edu)

DISCLAIMER:

This document was compiled by Indiana University, Indiana Geological Survey, using data believed to be accurate; however, a degree of error is inherent in all data. This product is distributed "AS-IS" without warranties of any kind, either expressed or implied, including but not limited to warranties of suitability to a particular purpose or use. No attempt has been made in either the design or production of these data and document to define the limits or jurisdiction of any federal, state, or local government. The data used to assemble this document are intended for use only at the published scale of the source data or smaller (see the metadata links below) and are for reference purposes only. They are not to be construed as a legal document or survey instrument. A detailed on-the-ground survey and historical analysis of a single site may differ from these data and this document.

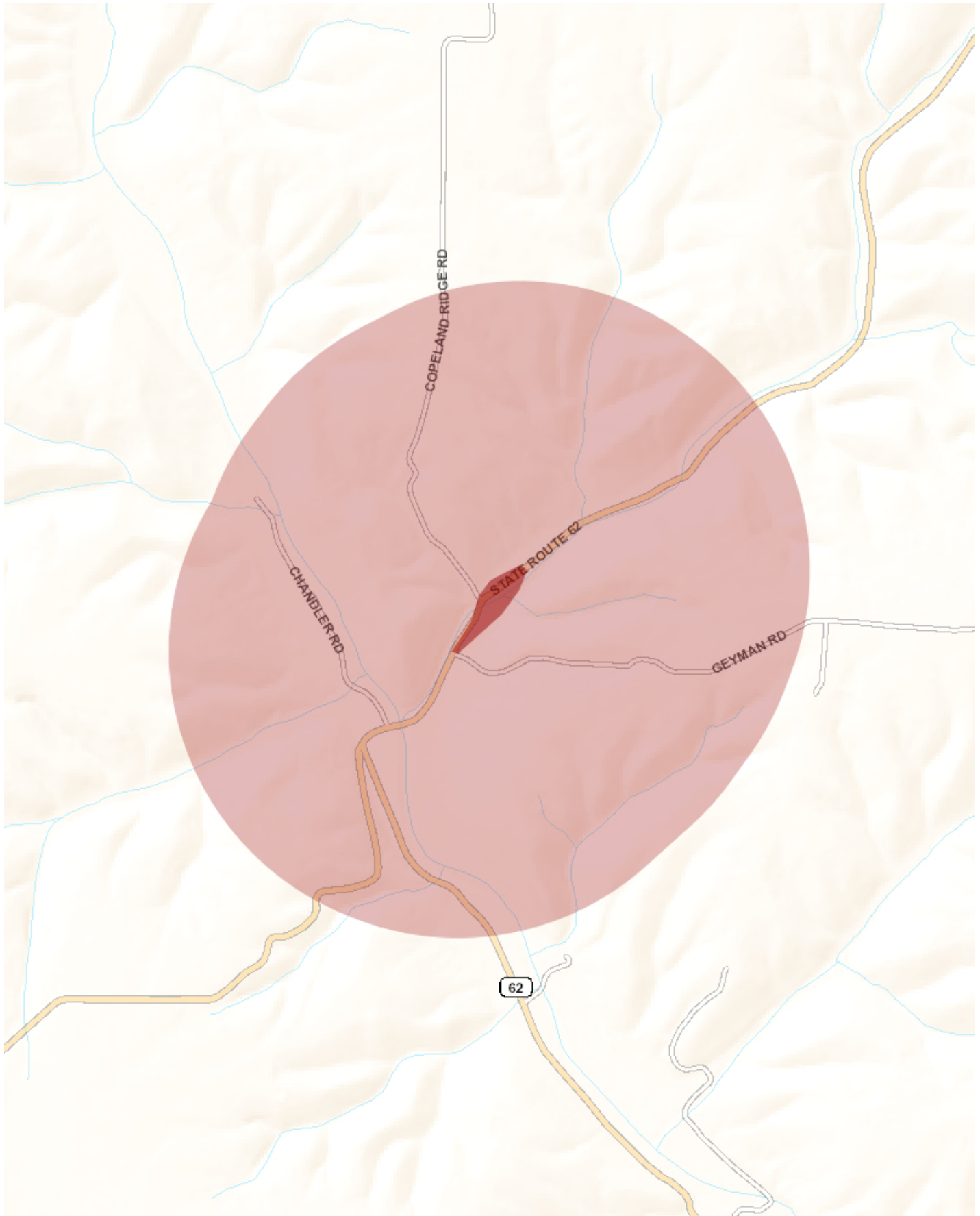
This information was furnished by Indiana Geological Survey

Address: 420 N. Walnut St., Bloomington, IN 47404

Email: IGSEnvir@indiana.edu

Phone: 812 855-7428

Date: August 28, 2020



Metadata:

- https://maps.indiana.edu/metadata/Hydrology/Karst_Cave_Density.html
- https://maps.indiana.edu/metadata/Hydrology/Floodplains_FIRM.html
- https://maps.indiana.edu/metadata/Geology/Bedrock_Geology.html



United States Department of the Interior



FISH AND WILDLIFE SERVICE

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

Phone: (812) 334-4261 Fax: (812) 334-4273

<http://www.fws.gov/midwest/Endangered/section7/s7process/step1.html>

In Reply Refer To:

September 18, 2019

Consultation Code: 03E12000-2019-I-1784

Event Code: 03E12000-2019-E-07683

Project Name: DES 1701457 - SR 62 over Toddy's Branch - Bridge Replacement

Subject: Concurrence verification letter for the 'DES 1701457 - SR 62 over Toddy's Branch - Bridge Replacement' project under the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

To whom it may concern:

The U.S. Fish and Wildlife Service (Service) has received your request dated to verify that the **DES 1701457 - SR 62 over Toddy's Branch - Bridge Replacement** (Proposed Action) may rely on the concurrence provided in the February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat (PBO) to satisfy requirements under Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended; 16 U.S.C 1531 *et seq.*).

Based on the information you provided (Project Description shown below), you have determined that the Proposed Action is within the scope and adheres to the criteria of the PBO, including the adoption of applicable avoidance and minimization measures, may affect, but is not likely to adversely affect (NLAA) the endangered Indiana bat (*Myotis sodalis*) and/or the threatened Northern long-eared bat (*Myotis septentrionalis*).

The Service has 14 calendar days to notify the lead Federal action agency or designated non-federal representative if we determine that the Proposed Action does not meet the criteria for a NLAA determination under the PBO. If we do not notify the lead Federal action agency or designated non-federal representative within that timeframe, you may proceed with the Proposed Action under the terms of the NLAA concurrence provided in the PBO. This verification period allows Service Field Offices to apply local knowledge to implementation of the PBO, as we may identify a small subset of actions having impacts that were unanticipated. In such instances, Service Field Offices may request additional information that is necessary to verify inclusion of the proposed action under the PBO.

For Proposed Actions that include bridge/structure removal, replacement, and/or maintenance activities: If your initial bridge/structure assessments failed to detect Indiana bats, but you later detect bats during construction, please submit the Post Assessment Discovery of Bats at Bridge/Structure Form (User Guide Appendix E) to this Service Office. In these instances, potential incidental take of Indiana bats may be exempted provided that the take is reported to the Service.

If the Proposed Action is modified, or new information reveals that it may affect the Indiana bat and/or Northern long-eared bat in a manner or to an extent not considered in the PBO, further review to conclude the requirements of ESA Section 7(a)(2) may be required. If the Proposed Action may affect any other federally-listed or proposed species, and/or any designated critical habitat, additional consultation is required. If the proposed action has the potential to take bald or golden eagles, additional coordination with the Service under the Bald and Golden Eagle Protection Act may also be required. In either of these circumstances, please contact this Service Office.

Project Description

The following project name and description was collected in IPaC as part of the endangered species review process.

Name

DES 1701457 - SR 62 over Toddy's Branch - Bridge Replacement

Description

The bridge (#062-039-05946 B) is located on SR 62 over Toddy's Branch, 0.35 Miles east of the intersection with SR 250, West Junction. The existing structure is a Composite Prestressed Concrete Box Beam Bridge with 1 span. This bridge was built in 1968. The NBI # for the structure is 22460. There is some leaking and efflorescence between the box beams. Beam #7 has one hairline crack and Beam #1 has one crack with delamination. There is vertical cracking with some spalls with exposed rebar in the abutments. There are cracks with efflorescence in the wingwalls. There is a spall at the top of the southeast wingwall.

The proposed roadway section will consist of two 12' lanes and 3'-4" shoulders paved to the face of new MGS guardrail. N. Copeland Ridge Road will be relocated to fit the newly aligned SR 62. With the new vertical and horizontal alignment, SR 62 within this projects limits will comply with all level 1 design criteria, requiring no design exceptions. The preferred alternative involves replacing the bridge on a new alignment and profile. Placing the bridge on a new alignment will remove all deficiencies present in the current structure. The proposed profile will provide the desired 2' of freeboard above the Q100 water surface elevation.

Suitable summer bat habitat exists surrounding the bridge. The terrain is primarily steep, forested hillside with a narrow hollow consisting primarily of agricultural fields. Approximately 0.5 acre of new right-of-way will be required for this project. Permanent stream impacts will occur due to the placement of riprap and temporary stream impacts for scaffolding and cofferdams. There will be no permanent lighting associated with this project. Temporary lighting may be necessary and will be directed away from wooded areas. Approximately 10 trees will be removed as a result of this project. No evidence of bats or bird nests were seen or heard under the bridge during an assessment on 7/9/2019. During construction, the road will be closed and a detour will be utilized. The project letting date is scheduled for 1/12/2022.

A review of the USFWS database by INDOT - Seymour District for Des 1701457 on 11/26/2018 did not indicate the presence of endangered bat species in or within 0.5 mile of the project area. The Red Flag Investigation was submitted to INDOT - Site Assessment and Management on 9/17/2019.

Determination Key Result

Based on your answers provided, this project(s) may affect, but is not likely to adversely affect the endangered Indiana bat and/or the threatened Northern long-eared bat. Therefore, consultation with the U.S. Fish and Wildlife Service pursuant to Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended 16 U.S.C. 1531 *et seq.*) is required. However, also based on your answers provided, this project may rely on the concurrence provided in the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

Qualification Interview

1. Is the project within the range of the Indiana bat^[1]?

[1] See [Indiana bat species profile](#)

Automatically answered

Yes

2. Is the project within the range of the Northern long-eared bat^[1]?

[1] See [Northern long-eared bat species profile](#)

Automatically answered

Yes

3. Which Federal Agency is the lead for the action?

A) Federal Highway Administration (FHWA)

4. Are *all* project activities limited to non-construction^[1] activities only? (examples of non-construction activities include: bridge/abandoned structure assessments, surveys, planning and technical studies, property inspections, and property sales)

[1] Construction refers to activities involving ground disturbance, percussive noise, and/or lighting.

No

5. Does the project include *any* activities that are **greater than** 300 feet from existing road/rail surfaces^[1]?

[1] Road surface is defined as the actively used [e.g. motorized vehicles] driving surface and shoulders [may be pavement, gravel, etc.] and rail surface is defined as the edge of the actively used rail ballast.

No

6. Does the project include *any* activities **within** 0.5 miles of a known Indiana bat and/or NLEB hibernaculum^[1]?

[1] For the purpose of this consultation, a hibernaculum is a site, most often a cave or mine, where bats hibernate during the winter (see suitable habitat), but could also include bridges and structures if bats are found to be hibernating there during the winter.

No

7. Is the project located **within** a karst area?

No

8. Is there *any* suitable^[1] summer habitat for Indiana Bat or NLEB **within** the project action area^[2]? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] The action area is defined as all areas to be affected directly or indirectly by the Federal action and not merely the immediate area involved in the action (50 CFR Section 402.02). Further clarification is provided by the [national consultation FAQs](#).

Yes

9. Will the project remove *any* suitable summer habitat^[1] and/or remove/trim any existing trees **within** suitable summer habitat?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

Yes

10. Will the project clear more than 20 acres of suitable habitat per 5-mile section of road/rail?

No

11. Have presence/probable absence (P/A) summer surveys^{[1][2]} been conducted^{[3][4]} **within** the suitable habitat located within your project action area?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] Presence/probable absence summer surveys conducted within the fall swarming/spring emergence home range of a documented Indiana bat hibernaculum (contact local Service Field Office for appropriate distance from hibernacula) that result in a negative finding requires additional consultation with the local Service Field Office to determine if clearing of forested habitat is appropriate and/or if seasonal clearing restrictions are needed to avoid and minimize potential adverse effects on fall swarming and spring emerging Indiana bats.

[3] For projects within the range of either the Indiana bat or NLEB in which suitable habitat is present, and no bat surveys have been conducted, the transportation agency will assume presence of the appropriate species. This assumption of presence should be based upon the presence of suitable habitat and the capability of bats to occupy it because of their mobility.

[4] Negative presence/probable absence survey results obtained using the [summer survey guidance](#) are valid for a minimum of two years from the completion of the survey unless new information (e.g., other nearby surveys) suggest otherwise.

No

12. Does the project include activities **within documented Indiana bat habitat**^{[1][2]}?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry biangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

No

13. Will the removal or trimming of habitat or trees occur **within** suitable but **undocumented Indiana bat** roosting/foraging habitat or travel corridors?

Yes

14. What time of year will the removal or trimming of habitat or trees **within** suitable but **undocumented Indiana bat** roosting/foraging habitat or travel corridors occur^[1]?

[1] Coordinate with the local Service Field Office for appropriate dates.

B) During the inactive season

15. Does the project include activities **within documented NLEB habitat**^{[1][2]}?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry triangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

No

16. Will the removal or trimming of habitat or trees occur **within** suitable but **undocumented NLEB** roosting/foraging habitat or travel corridors?

Yes

17. What time of year will the removal or trimming of habitat or trees **within** suitable but **undocumented NLEB** roosting/foraging habitat or travel corridors occur?

B) During the inactive season

18. Will *any* tree trimming or removal occur **within** 100 feet of existing road/rail surfaces?

Yes

19. Will the tree removal alter *any* **documented** Indiana bat or NLEB roosts and/or alter any surrounding summer habitat **within** 0.25 mile of a documented roost?

No

20. Will *any* tree trimming or removal occur **between** 100-300 feet of existing road/rail surfaces?

No

21. Are *all* trees that are being removed clearly demarcated?

Yes

22. Will the removal of habitat or the removal/trimming of trees include installing new or replacing existing **permanent** lighting?

No

23. Does the project include wetland or stream protection activities associated with compensatory wetland mitigation?

No

24. Does the project include slash pile burning?

No

25. Does the project include *any* bridge removal, replacement, and/or maintenance activities (e.g., any bridge repair, retrofit, maintenance, and/or rehabilitation work)?

Yes

26. Is there *any* suitable habitat^[1] for Indiana bat or NLEB **within** 1,000 feet of the bridge? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's current [summer survey guidance](#) for our current definitions of suitable habitat.

Yes

27. Has a bridge assessment^[1] been conducted **within** the last 24 months^[2] to determine if the bridge is being used by bats?

[1] See [User Guide Appendix D](#) for bridge/structure assessment guidance

[2] Assessments must be completed no more than 2 years prior to conducting any work below the deck surface on all bridges that meet the physical characteristics described in the Programmatic Consultation, regardless of whether assessments have been conducted in the past. Due to the transitory nature of bat use, a negative result in one year does not guarantee that bats will not use that bridge/structure in subsequent years.

Yes

SUBMITTED DOCUMENTS

- *USFWS - Inspection Form.pdf* <https://ecos.fws.gov/ipac/project/DODFIVZ56RAQXC7I4EW3PU4724/projectDocuments/18296111>

28. Did the bridge assessment detect *any* signs of Indiana bats and/or NLEBs roosting in/under the bridge (bats, guano, etc.)^[1]?

[1] If bridge assessment detects signs of *any* species of bats, coordination with the local FWS office is needed to identify potential threatened or endangered bat species. Additional studies may be undertaken to try to identify which bat species may be utilizing the bridge prior to allowing *any* work to proceed.

Note: There is a small chance bridge assessments for bat occupancy do not detect bats. Should a small number of bats be observed roosting on a bridge just prior to or during construction, such that take is likely to occur or does occur in the form of harassment, injury or death, the PBO requires the action agency to report the take. Report all unanticipated take within 2 working days of the incident to the USFWS. Construction activities may continue without delay provided the take is reported to the USFWS and is limited to 5 bats per project.

No

29. Will the bridge removal, replacement, and/or maintenance activities include installing new or replacing existing **permanent** lighting?

No

30. Does the project include the removal, replacement, and/or maintenance of *any* structure other than a bridge? (e.g., rest areas, offices, sheds, outbuildings, barns, parking garages, etc.)

No

31. Will the project involve the use of **temporary** lighting *during* the active season?

Yes

32. Is there *any* suitable habitat **within** 1,000 feet of the location(s) where **temporary** lighting will be used?

Yes

33. Will the project install new or replace existing **permanent** lighting?

No

34. Does the project include percussives or other activities (**not including tree removal/trimming or bridge/structure work**) that will increase noise levels above existing traffic/background levels?

Yes

35. Will the activities that use percussives (**not including tree removal/trimming or bridge/structure work**) and/or increase noise levels above existing traffic/background levels be conducted *during* the active season^[1]?

[1] Coordinate with the local Service Field Office for appropriate dates.

Yes

36. Will *any* activities that use percussives (**not including tree removal/trimming or bridge/structure work**) and/or increase noise levels above existing traffic/background levels be conducted *during* the inactive season^[1]?

[1] Coordinate with the local Service Field Office for appropriate dates.

Yes

37. Are *all* project activities that are **not associated with** habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives, limited to actions that DO NOT cause any additional stressors to the bat species?

Examples: lining roadways, unlighted signage , rail road crossing signals, signal lighting, and minor road repair such as asphalt fill of potholes, etc.

No

38. Will the project raise the road profile **above the tree canopy**?

No

39. Are the project activities that use percussives (not including tree removal/trimming or bridge/structure work) consistent with a Not Likely to Adversely Affect determination in this key?

Automatically answered

Yes, because the activities are within 300 feet of the existing road/rail surface, greater than 0.5 miles from a hibernacula, conducted during the active season, and are not within documented habitat

40. Are the project activities that use percussives (not including tree removal/trimming or bridge/structure work) and/or increase noise levels above existing traffic/background levels consistent with a No Effect determination in this key?

Automatically answered

Yes, because the activities are within 300 feet of the existing road/rail surface, greater than 0.5 miles from a hibernacula, and conducted during the inactive season

41. Is the habitat removal portion of this project consistent with a Not Likely to Adversely Affect determination in this key?

Automatically answered

Yes, because the tree removal/trimming that occurs outside of the active season occurs greater than 0.5 miles from the nearest hibernaculum, is less than 100 feet from the existing road/rail surface, includes clear demarcation of the trees that are to be removed, and does not alter documented roosts and/or surrounding summer habitat within 0.25 miles of a documented roost

42. Is the habitat removal portion of this project consistent with a Not Likely to Adversely Affect determination in this key?

Automatically answered

Yes, because the tree removal/trimming that occurs outside of the active season occurs greater than 0.5 miles from the nearest hibernaculum, is less than 100 feet from the existing road/rail surface, includes clear demarcation of the trees that are to be removed, and does not alter documented roosts and/or surrounding summer habitat within 0.25 miles of a documented roost

43. Is the bridge removal, replacement, or maintenance activities portion of this project consistent with a No Effect determination in this key?

Automatically answered

Yes, because the bridge has been assessed using the criteria documented in the BA and no signs of bats were detected

44. **General AMM 1**

Will the project ensure *all* operators, employees, and contractors working in areas of known or presumed bat habitat are aware of *all* FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable Avoidance and Minimization Measures?

Yes

45. **Tree Removal AMM 1**

Can *all* phases/aspects of the project (e.g., temporary work areas, alignments) be modified, to the extent practicable, to avoid tree removal^[1] in excess of what is required to implement the project safely?

Note: Tree Removal AMM 1 is a minimization measure, the full implementation of which may not always be practicable. Projects may still be NLAA as long as Tree Removal AMMs 2, 3, and 4 are implemented and LAA as long as Tree Removal AMMs 3, 5, 6, and 7 are implemented.

[1] The word “trees” as used in the AMMs refers to trees that are suitable habitat for each species within their range. See the USFWS’ current summer survey guidance for our latest definitions of suitable habitat.

Yes

46. **Tree Removal AMM 2**

Can *all* tree removal activities be restricted to when Indiana bats are not likely to be present (e.g., the inactive season)^[1]?

[1] Coordinate with the local Service Field Office for appropriate dates.

Automatically answered

Yes

47. **Tree Removal AMM 2**

Can *all* tree removal activities be restricted to when Northern long-eared bats are not likely to be present (e.g., the inactive season)^[1]?

[1] Coordinate with the local Service Field Office for appropriate dates.

Automatically answered

Yes

48. **Tree Removal AMM 3**

Can tree removal be limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits)?

Yes

49. **Tree Removal AMM 4**

Can the project avoid cutting down/removal of *all* (1) **documented**^[1] Indiana bat or NLEB roosts^[2] (that are still suitable for roosting), (2) trees **within** 0.25 miles of roosts, and (3) documented foraging habitat any time of year?

[1] The word documented means habitat where bats have actually been captured and/or tracked.

[2] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry biangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

Yes

50. **Lighting AMM 1**

Will *all* **temporary** lighting used during the removal of suitable habitat and/or the removal/trimming of trees within suitable habitat be directed away from suitable habitat during the active season?

Yes

51. **Lighting AMM 1**

Will *all* **temporary** lighting be directed away from suitable habitat during the active season?

Yes

Project Questionnaire

1. Have you made a No Effect determination for *all* other species indicated on the FWS IPaC generated species list?

Yes

2. Have you made a May Affect determination for *any* other species on the FWS IPaC generated species list?

No

3. How many acres^[1] of trees are proposed for removal between 0-100 feet of the existing road/rail surface?

[1] If described as number of trees, multiply by 0.09 to convert to acreage and enter that number.

0.9

4. Please describe the proposed bridge work:

Construction of a new single span concrete bridge on a new alignment

5. Please state the timing of all proposed bridge work:

Project letting is planned for 1/12/2022. Construction may occur within active or inactive seasons.

6. Please enter the date of the bridge assessment:

7/9/2019

Avoidance And Minimization Measures (AMMs)

This determination key result includes the commitment to implement the following Avoidance and Minimization Measures (AMMs):

GENERAL AMM 1

Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs.

LIGHTING AMM 1

Direct temporary lighting away from suitable habitat during the active season.

TREE REMOVAL AMM 1

Modify all phases/aspects of the project (e.g., temporary work areas, alignments) to avoid tree removal.

TREE REMOVAL AMM 2

Apply time of year restrictions for tree removal when bats are not likely to be present, or limit tree removal to 10 or fewer trees per project at any time of year within 100 feet of existing road/rail surface and **outside of documented** roosting/foraging habitat or travel corridors; visual emergence survey must be conducted with no bats observed.

TREE REMOVAL AMM 3

Ensure tree removal is limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits).

TREE REMOVAL AMM 4

Do not remove **documented** Indiana bat or NLEB roosts that are still suitable for roosting, or trees within 0.25 miles of roosts, or **documented** foraging habitat any time of year.

Determination Key Description: FHWA, FRA, FTA Programmatic Consultation For Transportation Projects Affecting NLEB Or Indiana Bat

This key was last updated in IPaC on March 16, 2018. Keys are subject to periodic revision.

This decision key is intended for projects/activities funded or authorized by the Federal Highway Administration (FHWA), Federal Railroad Administration (FRA), and/or Federal Transit Administration (FTA), which require consultation with the U.S. Fish and Wildlife Service (Service) under Section 7 of the Endangered Species Act (ESA) for the endangered **Indiana bat** (*Myotis sodalis*) and the threatened **Northern long-eared bat** (NLEB) (*Myotis septentrionalis*).

This decision key should only be used to verify project applicability with the Service's [February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects](#). The programmatic biological opinion covers limited transportation activities that may affect either bat species, and addresses situations that are both likely and not likely to adversely affect either bat species. This decision key will assist in identifying the effect of a specific project/activity and applicability of the programmatic consultation. The programmatic biological opinion is not intended to cover all types of transportation actions. Activities outside the scope of the programmatic biological opinion, or that may affect ESA-listed species other than the Indiana bat or NLEB, or any designated critical habitat, may require additional ESA Section 7 consultation.



United States Department of the Interior



FISH AND WILDLIFE SERVICE

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

Phone: (812) 334-4261 Fax: (812) 334-4273

<http://www.fws.gov/midwest/Endangered/section7/s7process/step1.html>

In Reply Refer To:

September 17, 2019

Consultation Code: 03E12000-2019-SLI-1784

Event Code: 03E12000-2019-E-07614

Project Name: DES 1701457 - SR 62 over Toddy's Branch - Bridge Replacement

Subject: Updated list of threatened and endangered species that may occur in your proposed project location, and/or may be affected by your proposed project

To Whom It May Concern:

The attached species list identifies any federally threatened, endangered, proposed and candidate species that may occur within the boundary of your proposed project or may be affected by your proposed project. The list also includes designated critical habitat if present within your proposed project area or affected by your project. This list is provided to you as the initial step of the consultation process required under section 7(c) of the Endangered Species Act, also referred to as Section 7 Consultation.

Section 7 of the Endangered Species Act of 1973 requires that actions authorized, funded, or carried out by Federal agencies not jeopardize federally threatened or endangered species or adversely modify designated critical habitat. To fulfill this mandate, Federal agencies (or their designated non-federal representative) must consult with the Service if they determine their project “may affect” listed species or critical habitat.

Under 50 CFR 402.12(e) (the regulations that implement Section 7 of the Endangered Species Act) the accuracy of this species list should be verified after 90 days. This verification can be completed formally or informally. You may verify the list by visiting the ECOS-IPaC website <http://ecos.fws.gov/ipac/> at regular intervals during project planning and implementation and completing the same process you used to receive the attached list. As an alternative, you may contact this Ecological Services Field Office for updates.

Please use the species list provided and visit the U.S. Fish and Wildlife Service's Region 3 Section 7 Technical Assistance website at - <http://www.fws.gov/midwest/endangered/section7/s7process/index.html>. This website contains step-by-step instructions which will help you

determine if your project will have an adverse effect on listed species and will help lead you through the Section 7 process.

For all **wind energy projects** and **projects that include installing towers that use guy wires or are over 200 feet in height**, please contact this field office directly for assistance, even if no federally listed plants, animals or critical habitat are present within your proposed project or may be affected by your proposed project.

Although no longer protected under the Endangered Species Act, be aware that bald eagles are protected under the Bald and Golden Eagle Protection Act (16 U.S.C. 668 *et seq.*) and Migratory Bird Treaty Act (16 U.S.C. 703 *et seq.*), as are golden eagles. Projects affecting these species may require measures to avoid harming eagles or may require a permit. If your project is near an eagle nest or winter roost area, see our Eagle Permits website at <http://www.fws.gov/midwest/midwestbird/EaglePermits/index.html> to help you determine if you can avoid impacting eagles or if a permit may be necessary.

We appreciate your concern for threatened and endangered species. Please include the Consultation Tracking Number in the header of this letter with any request for consultation or correspondence about your project that you submit to our office.

Attachment(s):

- Official Species List

Official Species List

This list is provided pursuant to Section 7 of the Endangered Species Act, and fulfills the requirement for Federal agencies to "request of the Secretary of the Interior information whether any species which is listed or proposed to be listed may be present in the area of a proposed action".

This species list is provided by:

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

(812) 334-4261

Project Summary

Consultation Code: 03E12000-2019-SLI-1784

Event Code: 03E12000-2019-E-07614

Project Name: DES 1701457 - SR 62 over Toddy's Branch - Bridge Replacement

Project Type: BRIDGE CONSTRUCTION / MAINTENANCE

Project Description: The bridge (#062-039-05946 B) is located on SR 62 over Toddy's Branch, 0.35 Miles east of the intersection with SR 250, West Junction. The existing structure is a Composite Prestressed Concrete Box Beam Bridge with 1 span. This bridge was built in 1968. The NBI # for the structure is 22460. There is some leaking and efflorescence between the box beams. Beam #7 has one hairline crack and Beam #1 has one crack with delamination. There is vertical cracking with some spalls with exposed rebar in the abutments. There are cracks with efflorescence in the wingwalls. There is a spall at the top of the southeast wingwall.

The proposed roadway section will consist of two 12' lanes and 3'-4" shoulders paved to the face of new MGS guardrail. N. Copeland Ridge Road will be relocated to fit the newly aligned SR 62. With the new vertical and horizontal alignment, SR 62 within this projects limits will comply with all level 1 design criteria, requiring no design exceptions. The preferred alternative involves replacing the bridge on a new alignment and profile. Placing the bridge on a new alignment will remove all deficiencies present in the current structure. The proposed profile will provide the desired 2' of freeboard above the Q100 water surface elevation.

Suitable summer bat habitat exists surrounding the bridge. The terrain is primarily steep, forested hillside with a narrow hollow consisting primarily of agricultural fields. Approximately 0.5 acre of new right-of-way will be required for this project. Permanent stream impacts will occur due to the placement of riprap and temporary stream impacts for scaffolding and cofferdams. There will be no permanent lighting associated with this project. Temporary lighting may be necessary and will be directed away from wooded areas. Approximately 10 trees will be removed as a result of this project. No evidence of bats or bird nests were seen or heard under the bridge during an assessment on 7/9/2019. During construction, the road will be closed and a detour will be utilized. The project letting date is scheduled for 1/12/2022.

A review of the USFWS database by INDOT - Seymour District for Des

1701457 on 11/26/2018 did not indicate the presence of endangered bat species in or within 0.5 mile of the project area. The Red Flag Investigation was submitted to INDOT - Site Assessment and Management on 9/17/2019.

Project Location:

Approximate location of the project can be viewed in Google Maps: <https://www.google.com/maps/place/38.84717941187912N85.34847978767675W>



Counties: Jefferson, IN

Endangered Species Act Species

There is a total of 2 threatened, endangered, or candidate species on this species list.

Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species. Note that 1 of these species should be considered only under certain conditions.

IPaC does not display listed species or critical habitats under the sole jurisdiction of NOAA Fisheries¹, as USFWS does not have the authority to speak on behalf of NOAA and the Department of Commerce.

See the "Critical habitats" section below for those critical habitats that lie wholly or partially within your project area under this office's jurisdiction. Please contact the designated FWS office if you have questions.

-
1. [NOAA Fisheries](#), also known as the National Marine Fisheries Service (NMFS), is an office of the National Oceanic and Atmospheric Administration within the Department of Commerce.

Mammals

NAME	STATUS
Indiana Bat <i>Myotis sodalis</i> There is final critical habitat for this species. Your location is outside the critical habitat. Species profile: https://ecos.fws.gov/ecp/species/5949 Species survey guidelines: https://ecos.fws.gov/ipac/guideline/survey/population/1/office/31440.pdf	Endangered
Northern Long-eared Bat <i>Myotis septentrionalis</i> No critical habitat has been designated for this species. This species only needs to be considered under the following conditions: <ul style="list-style-type: none"> ▪ Incidental take of the NLEB is not prohibited here. Federal agencies may consult using the 4(d) rule streamlined process. Transportation projects may consult using the programmatic process. See www.fws.gov/midwest/endangered/mammals/nleb/index.html Species profile: https://ecos.fws.gov/ecp/species/9045	Threatened

Critical habitats

THERE ARE NO CRITICAL HABITATS WITHIN YOUR PROJECT AREA UNDER THIS OFFICE'S JURISDICTION.



Natural Resources Conservation Service
Indiana State Office
6013 Lakeside Boulevard
Indianapolis, IN 46278
317-290-3200

June 29, 2020

Liz Neudeck
Burgess & Niple
251 North Illinois Street, Capital Center Suite 920
Indianapolis, Indiana 46204

Dear Ms. Neudeck:

The proposed project to replace and reconstruct 4 different bridges along State Road 62 in Jefferson County, Indiana, (Des No 1701457, 1602259, 1701455, and 1700005) as referred to in your letter received June 12, 2020, will not cause a conversion of prime farmland.

If you need additional information, please contact John Allen at 317-295-5859.

Sincerely,

RICHARD
NEILSON

Digitally signed by
RICHARD NEILSON
Date: 2020.06.30
08:31:10 -04'00'

RICK NEILSON
State Soil Scientist

Helping People Help the Land.



USDA is an equal opportunity provider, employer and lender.